

AGENDA
OCEANSIDE DEVELOPER'S CONFERENCE

Tuesday, May 13, 2014, 9:00 a.m.
Guajome Room

1. 9:00 a.m. - 10:00 a.m. Discussion of a 15,000 to 25,000 square foot steel building for commercial operation of Energy Storage System located south of Mission Avenue and Hwy. 76 and north of San Luis Rey Elementary School on Caltrans property.

Zoning: Caltrans
Land Use: Caltrans
Neighborhood Area: Loma Alta
Assessor Parcel Number: N/A
Contact Person: Leonard Steinberg
Tel.: 760-363-5505
Email: leonard@creativealliancegroup.com

2. 10:00 a.m. - 11:00 a.m. Discussion to allow for the development of sixteen (16) single-family attached townhome units located on South Cleveland Street.

Zoning: R3 (Medium Density Residential)
Land Use: High Density
Neighborhood Area: Townsite
Assessor Parcel Number: 152-016-06, 08-10, 12
Contact Person: Sean Santa Cruz
Tel.: 858-481-3310, ext. 122
Email: ssantacruz@hallmarkcommunities.com

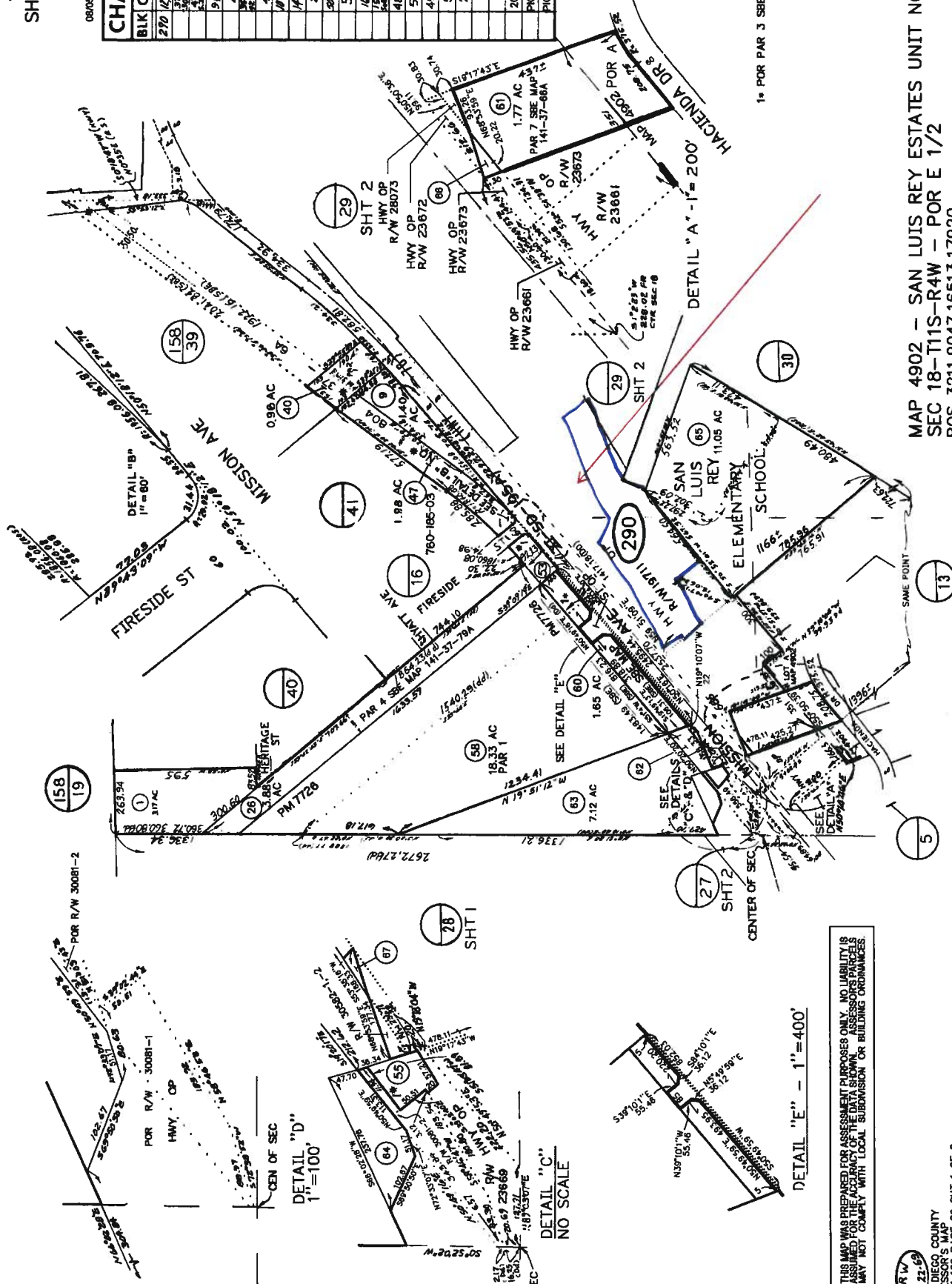
Attachments:

1. Maps
2. Project Description Letter
3. Proposed Elevations

160-29
SHT 1 OF 2
1"=400'
N

08/05/2008 CS

CHANGES		
BLK	OLD NEW YR	CUT
270	12/15/25	4/14/5 7/9 2207
327	1/1/1	4/6 40 2/25/4
333	1/1/1	4/7 8/1 2/24/6
334	1/1/1	4/7 8/1 2/24/6
335	1/1/1	4/7 8/1 2/24/6
336	1/1/1	4/7 8/1 2/24/6
337	1/1/1	4/7 8/1 2/24/6
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398	1/1/1	4/7 8/1 2/24/6
399	1/1/1	4/7 8/1 2/24/6
400	1/1/1	4/7 8/1 2/24/6



1" POR PAR 3 SEE MAP 804-37-6A

ITEM # 1

MAP 4902 - SAN LUIS REY ESTATES UNIT NO 9- LOT A
SEC 18-T11S-R4W - POR E 1/2
ROS 3211,9047,16513,17920

THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY. NO LIABILITY IS ASSUMED FOR THE ACCURACY OF THE DATA SHOWN. ASSESSOR'S PARCELS MAY NOT COMPLY WITH LOCAL SUBDIVISION OR BUILDING ORDINANCES.

ERW
12-23-09
SAN DIEGO COUNTY
ASSESSOR'S MAP
BOOK 180 PAGE 28 SHT 1 OF 2

Green Light Energy Corporation

Project Description:

Green Light Energy Corp. (GLEC) is interested in deploying an energy storage system ("ESS") for commercial operation. The system would house Lithium Ion Batteries with a proven track record of over 20 years. GLEC sites ESS projects that are 20 MW in size with at least 15 minutes of continuous charging and discharging capability at rated capacity and no more than 60 minutes is required. The primary uses of the ESS will be to improve local grid reliability by providing instantaneous frequency regulation and responsive reserves to SDG&E. In essence, this project will help substitute the need for a new power plant in the region. The actual building footprint will be determined as the project moves thru the SDG&E Interconnection and Transmission Impact studies. At this time we are anticipating a steel building in the range of 15,000 to 25,000 square feet. We are siting the structure to be roughly 1 acre from the existing homes and 2 acres from the school. Beyond the grid reliability there would be minimal traffic congestion and a great deal of open space for this commercially zoned parcel.

DOCUMENTARY TRANSFER TAX \$ _____
COMPUTED ON FULL VALUE OF PROPERTY
CONVEYED, OR
COMPUTED ON FULL VALUE LESS LIENS AND
ENCUMBRANCES REMAINING THEREON AT TIME OF
SALE.

Signature of declarant or agent determining tax – firm name _____

Unincorporated Area _____ City of _____

When recorded mail to:

State of California
Department of Transportation
4050 Taylor Street M.S. 310
San Diego, CA 92110

FILE COPY

Space above this line for Recorder's Use
R/W MAP 25001.12 E.A. 003002

DIRECTOR'S DEED

District	County	Route	Post	Number
11	SD	76	P.M. 3.0	DD19711-01-01 (19711-01-02)

The STATE OF CALIFORNIA, acting by and through its Director of Transportation, does hereby grant to

all that real property in the City of Oceanside, County of San Diego, State of California, described as:

SEE EXHIBIT "A"

MAIL TAX
STATEMENTS TO:

EXHIBIT "A"

19711-01-01

THAT PORTION OF THE EAST HALF OF SECTION 18, TOWNSHIP 11 SOUTH, RANGE 4 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING A PORTION OF PARCEL 1A AS CONVEYED TO THE STATE OF CALIFORNIA IN THAT FINAL ORDER OF CONDEMNATION RECORDED MARCH 26, 1970 AS DOCUMENT NO. 70-53236 OF OFFICIAL RECORDS, ON FILE IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, LYING SOUTHERLY AND SOUTHEASTERLY OF THE FOLLOWING DESCRIBED LINE:

BEGINNING AT THE INTERSECTION OF THE WESTERLY LINE OF SAID PARCEL 1A AND COURSE (327) OF THAT PARCEL DESCRIBED ON PAGE 28 OF 29 OF DOCUMENT RECORDED IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY MARCH 2, 2000 AS DOCUMENT NO. 2000-0106756 OF OFFICIAL RECORDS, SAID INTERSECTION BEING DISTANT SOUTH $68^{\circ}53'59''$ WEST, 169.28 FEET FROM THE NORTHEASTERLY TERMINUS OF SAID COURSE (327);

THENCE EASTERLY, SOUTHEASTERLY AND NORTHEASTERLY ALONG SAID PARCEL DESCRIBED ON PAGES 27 AND 28 OF 29 THE FOLLOWING NUMBERED COURSES:

- (1) ALONG SAID COURSE (327) NORTH $68^{\circ}53'59''$ EAST, 169.28 FEET;
- (2) SOUTH $55^{\circ}31'45''$ EAST, 98.80 FEET;
- (3) NORTH $64^{\circ}35'07''$ EAST, 347.45 FEET;
- (4) NORTH $63^{\circ}00'08''$ EAST, 308.63 FEET;
- (5) NORTH $61^{\circ}26'24''$ EAST, 23.87 FEET;
- (6) NORTH $32^{\circ}22'03''$ EAST, 7.06 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 68.00 FEET;
- (7) NORTHEASTERLY ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF $29^{\circ}04'21''$, AN ARC DISTANCE OF 34.50 FEET;
- (8) TANGENT TO SAID CURVE NORTH $61^{\circ}26'24''$ EAST, 72.21 FEET TO ITS INTERSECTION WITH THE SOUTHEASTERLY LINE OF SAID PARCEL 1A DESCRIBED AS " $S.24^{\circ}54'42''W.$ ", 578.81 FEET" AND THE POINT OF **TERMINUS**.

CONTAINING 3.137 ACRES MORE OR LESS.

THERE SHALL BE NO ABUTTER'S RIGHTS, INCLUDING ACCESS RIGHTS APPURTENANT TO THE ABOVE DESCRIBED REAL PROPERTY IN AND TO THE ADJACENT STATE HIGHWAY.

THE BEARINGS AND DISTANCES USED IN THE ABOVE DESCRIPTIONS ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1927, ZONE 6. MULTIPLY ALL DISTANCES USED IN THE ABOVE DESCRIPTIONS BY 1.00004586 TO OBTAIN GROUND LEVEL DISTANCES.

SUBJECT TO SPECIAL ASSESSMENTS IF ANY, RESTRICTIONS, RESERVATIONS, AND EASEMENTS OF RECORD.

THE ABOVE PROPERTY IS BEING SOLD AS IS AND HAS AN EXISTING ENCROACHMENT THAT IS APPROXIMATELY 2742 SQUARE FEET.

EXHIBIT "A"(cont.)

19711-01-02

TOGETHER WITH A 27 FOOT WIDE **EASEMENT** FOR INGRESS AND EGRESS UPON, OVER AND ACROSS THE FOLLOWING DESCRIBED AREA, THE CENTERLINE OF WHICH IS DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT ON THE NORTH SIDE OF PARCEL 19711-01-01, AS DESCRIBED ABOVE, WHICH BEARS NORTH 68°53'34" EAST, 16.50 FEET FROM THE NORTHWESTERLY CORNER OF PARCEL 19711-01-01, THENCE NORTH 19°17'43" WEST A DISTANCE OF 67.01 FEET TO THE SOUTHERLY RIGHT OF WAY FOR MISSION AVENUE, AND BEING THE **POINT OF TERMINUS**.

THE SIDELINES OF SAID EASEMENT SHALL BE SHORTENED OR EXTENDED TO CLOSE UPON THE SOUTHERLY RIGHT OF WAY LINE OF MISSION AVENUE.

THE INTENT OF THIS EASEMENT IS TO PROVIDE ACCESS FROM PARCEL 19711-01-01 TO MISSION AVENUE, AND NO DIRECT ACCESS TO THE FREEWAY.

THERE SHALL BE NO ABUTTER'S RIGHTS, INCLUDING ACCESS RIGHTS APPURTENANT TO THE ABOVE DESCRIBED REAL PROPERTY IN AND TO THE ADJACENT STATE HIGHWAY.

THE BEARINGS AND DISTANCES USED IN THE ABOVE DESCRIPTIONS ARE ON THE CALIFORNIA COORDINATE SYSTEM OF 1927, ZONE 6. MULTIPLY ALL DISTANCES USED IN THE ABOVE DESCRIPTIONS BY 1.00004586 TO OBTAIN GROUND LEVEL DISTANCES.

SUBJECT TO SPECIAL ASSESSMENTS IF ANY, RESTRICTIONS, RESERVATIONS, AND EASEMENTS OF RECORD.

THIS REAL PROPERTY DESCRIPTION HAS BEEN PREPARED BY ME, OR UNDER MY DIRECTION, IN CONFORMANCE WITH THE PROFESSIONAL LAND SURVEYORS ACT.

SIGNATURE _____

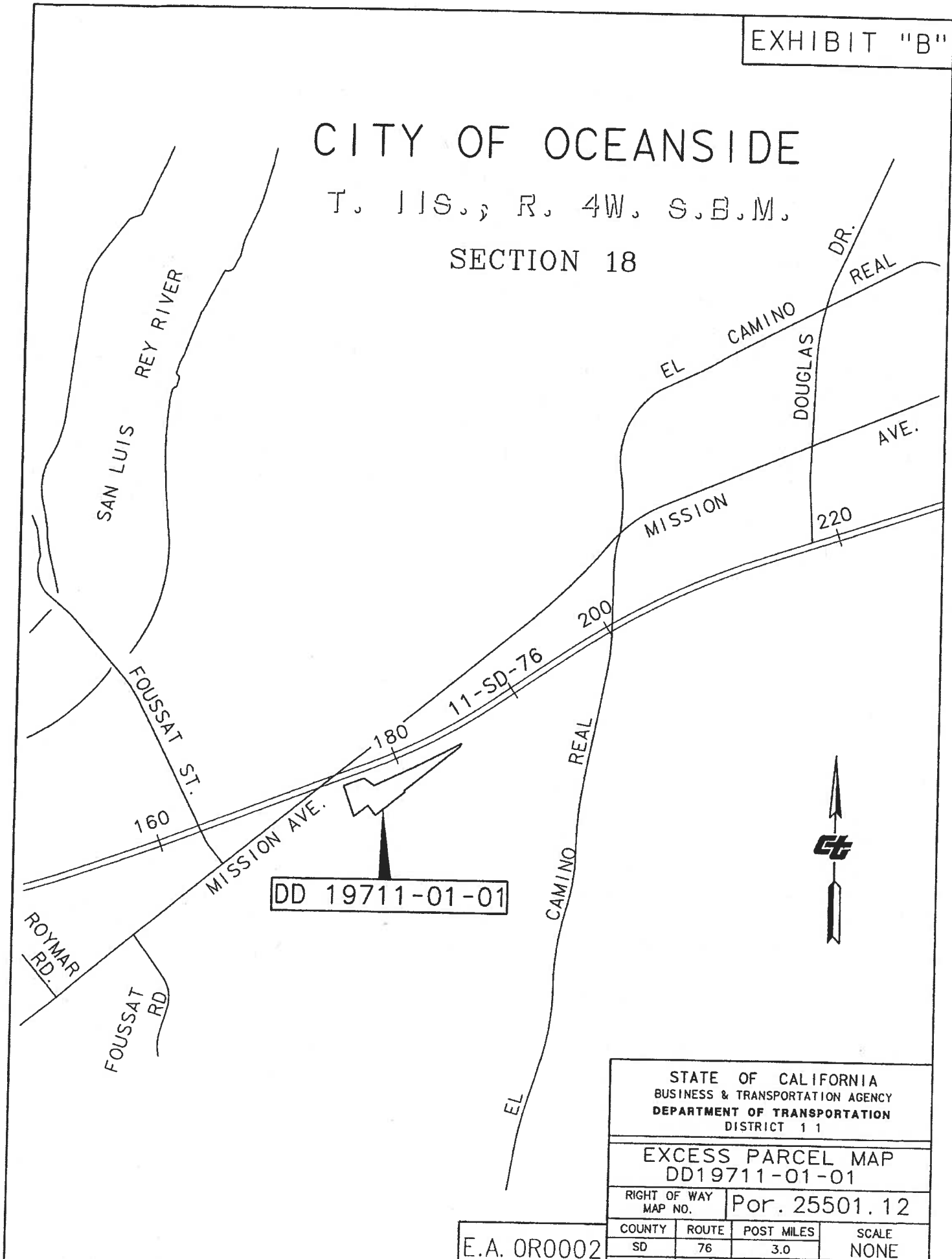
PROFESSIONAL LAND SURVEYOR

DATE 03/09/2011

AU JY
CK SD



CITY OF OCEANSIDE
 T. 11S., R. 4W. S.B.M.
 SECTION 18



DD 19711-01-01

STATE OF CALIFORNIA
 BUSINESS & TRANSPORTATION AGENCY
 DEPARTMENT OF TRANSPORTATION
 DISTRICT 1 1

EXCESS PARCEL MAP
 DD19711-01-01

RIGHT OF WAY
 MAP NO. Por. 25501.12

E.A. 0R0002

COUNTY	ROUTE	POST MILES	SCALE
SD	76	3.0	NONE

EXHIBIT "B"

CITY OF OCEANSIDE

T. 11 S. R. 4 W. S. B. & M.

SECTION 18

MISSION AVE.

11-SO-76

0.04 AC
1809 SQ. FT.
ACCESS EASEMENT TO MISSION ROAD
19711-01-02

27' WIDE
ACCESS ESMNT.

19711-01-01
136,646 SQ. FT.
3.14 AC
(EXCLUDES AREA
UNDER VIA DUCT)

DD19711-01-01

10,617 SQ. FT.
DE19711-1
TO: CITY OF
OCEANSIDE

17' WIDE
ACCESS ESMNT.

3383 SQ. FT.
DE19711-2
TO: CITY OF OCEANSIDE

12' WIDE
ACCESS ESMNT.

AREA OF ENCROACHMENT: 2742 SQ. FT.

ACCESS CONTROLLED
FREEWAY (SD-76)

DD 19711-01-01 AREA 3.14 AC
(EXCLUDES AREA UNDER VIADUCT)

LEGEND:

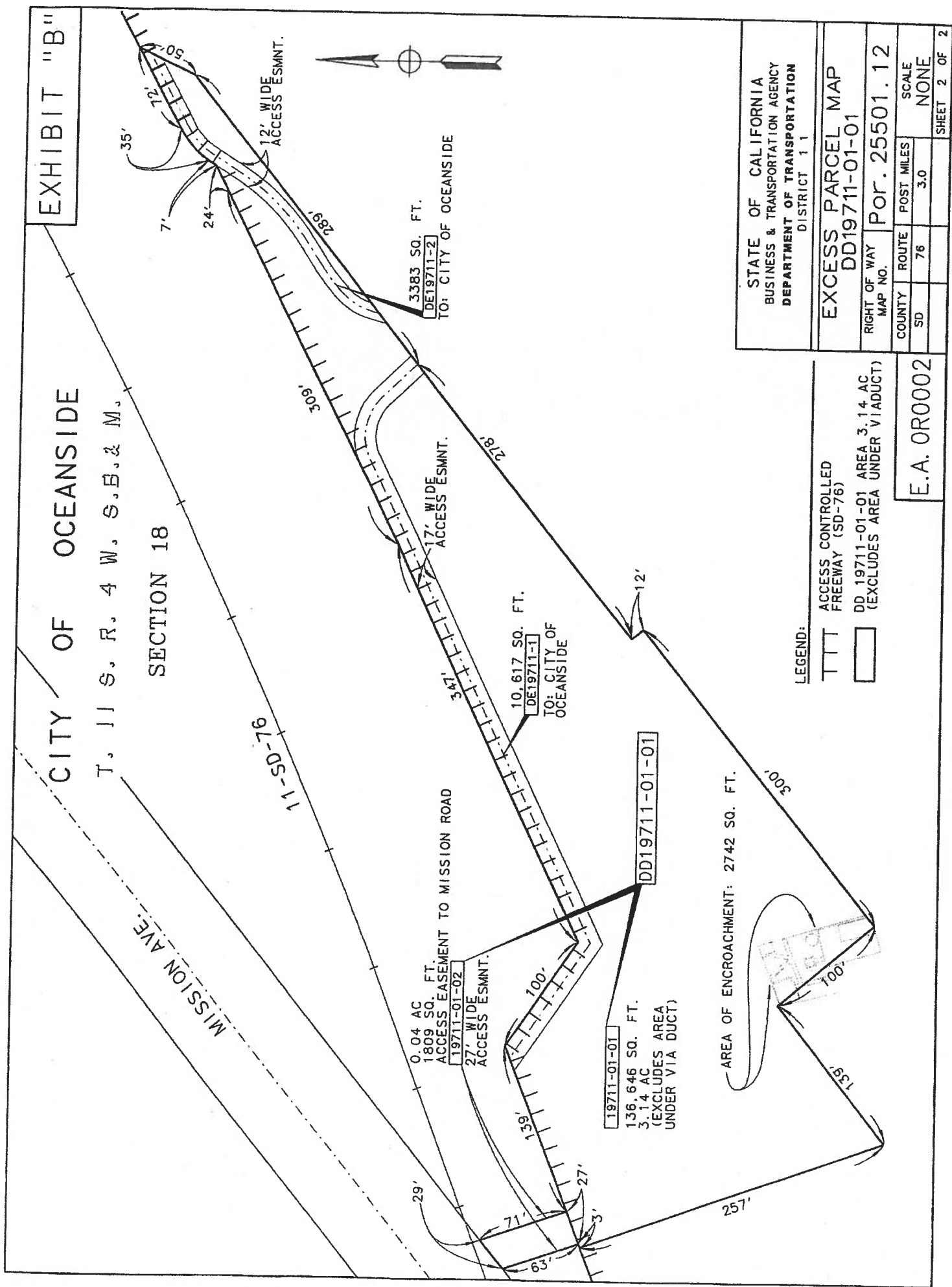
TTT ACCESS CONTROLLED
FREEWAY (SD-76)

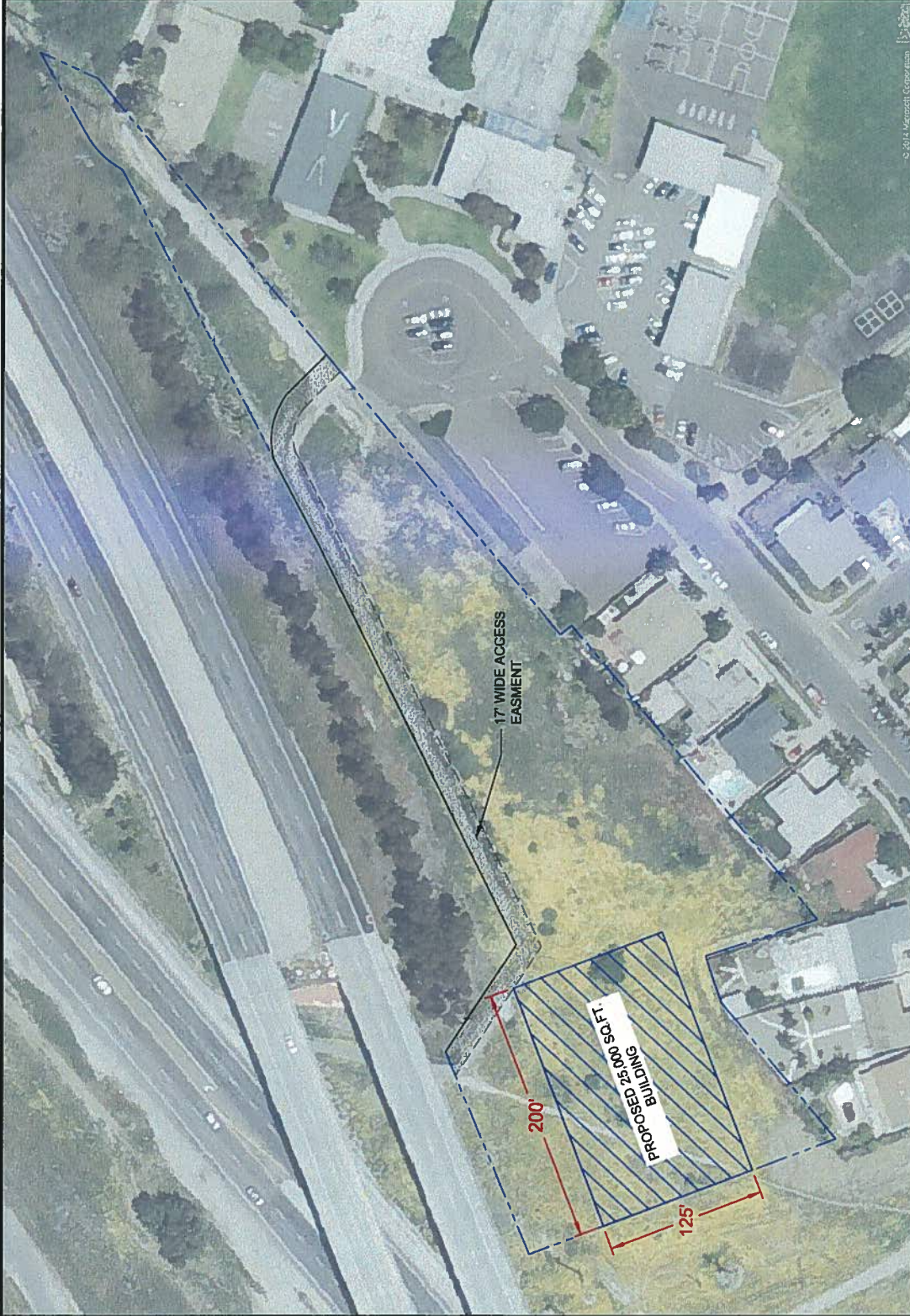
DD 19711-01-01 AREA 3.14 AC
(EXCLUDES AREA UNDER VIADUCT)



STATE OF CALIFORNIA BUSINESS & TRANSPORTATION AGENCY DEPARTMENT OF TRANSPORTATION DISTRICT 11			
EXCESS PARCEL MAP DD19711-01-01			
RIGHT OF WAY MAP NO.	DD19711-01-01		
COUNTY	ROUTE	POST MILES	SCALE
SD	76	3.0	NONE
Por. 25501.12			SHEET 2 OF 2

E.A. 0R0002





CARL KIEWITZ 4/23/2014 1:23 PM

REV.	BY	DESCRIPTION	DATE	APPROD BY

OCEANSIDE BATTERY STORAGE

CONFIDENTIAL DOCUMENTS
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604 SUTTER ST., STE 250
FOLSOM, CA 95630
Phone : 916.985.9461
Fax: 916.985.9467

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Z-GLOBAL
ENGINEERING & ENERGY SOLUTIONS

DRAWN BY: Carl Kiewitz	SHEET TITLE: SITE PLAN
CHECKED: JV	DRAWING No. S-000
DATE: 04/23/14	Scale to Confirm 11"x17" Plot
1 inch	



Residential Districts

- RE-A... Residential Estate A
- RE-B... Residential Estate B
- RS... Single Family Residential
- RM-A... Medium Density Residential A
- RM-B... Medium Density Residential B
- RM-C... Medium Density Residential C
- RH... High Density Residential
- RH-U... Urban High Density Residential
- RT... Residential Tract

Commercial Districts

- CN... Neighborhood Commercial
- CC... Community Commercial
- CG... General Commercial
- CL... Limited Commercial
- CR... Community Commercial - Recreation
- CV... Village Commercial
- CS-HO... Special Commercial Highway District
- CS-L... Limited Commercial
- CP... Professional Commercial

Mixed Use Districts

- D... Downtown Mixed Development
- PD... Planned Development

Agricultural, Open Space and Institutional Districts

- A... Agriculture
- OS... Open Space
- PS... Public Space

Overlay Zoning Districts

- HD... Village Development Overlay
- H... Historic Overlay
- SP... South Park Overlay
- NC... Neighborhood Conservation Overlay
- PBD... Planned Back Development
- IS... Inland Study Overlay
- MP... Master Plan Overlay
- N... Neighborhood Use Overlay
- EQ... Equitable Overlay
- SMH... Single House Overlay

Planned Development

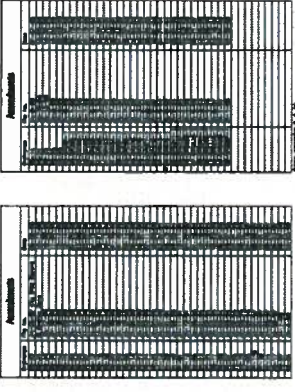
- PD-1... Rando Del Oro
- PD-2... Del Oro Hills
- PD-3... Staffing
- PD-4... Main Mesa
- PD-5... Systems Springs
- PD-6... Ivy Ranch Lot 5
- PD-7... Ivy Ranch Lot 9
- PD-8... Canyon Vista
- PD-9... Vista Del Maroon
- PD-10... Vista Del Oro
- PD-11... Ivy Ridge Subdivision
- PD-12... Ivy Ranch Lot 2
- PD-13... Main View
- PD-14... The Oaks
- PD-15... River Ranch North
- PD-16... Mesa Pacific

Specific Plan

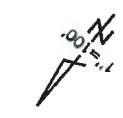
SP-1-09... B Cozzano

Master Plan

MP-1-96... Metro 110



ITEM #2

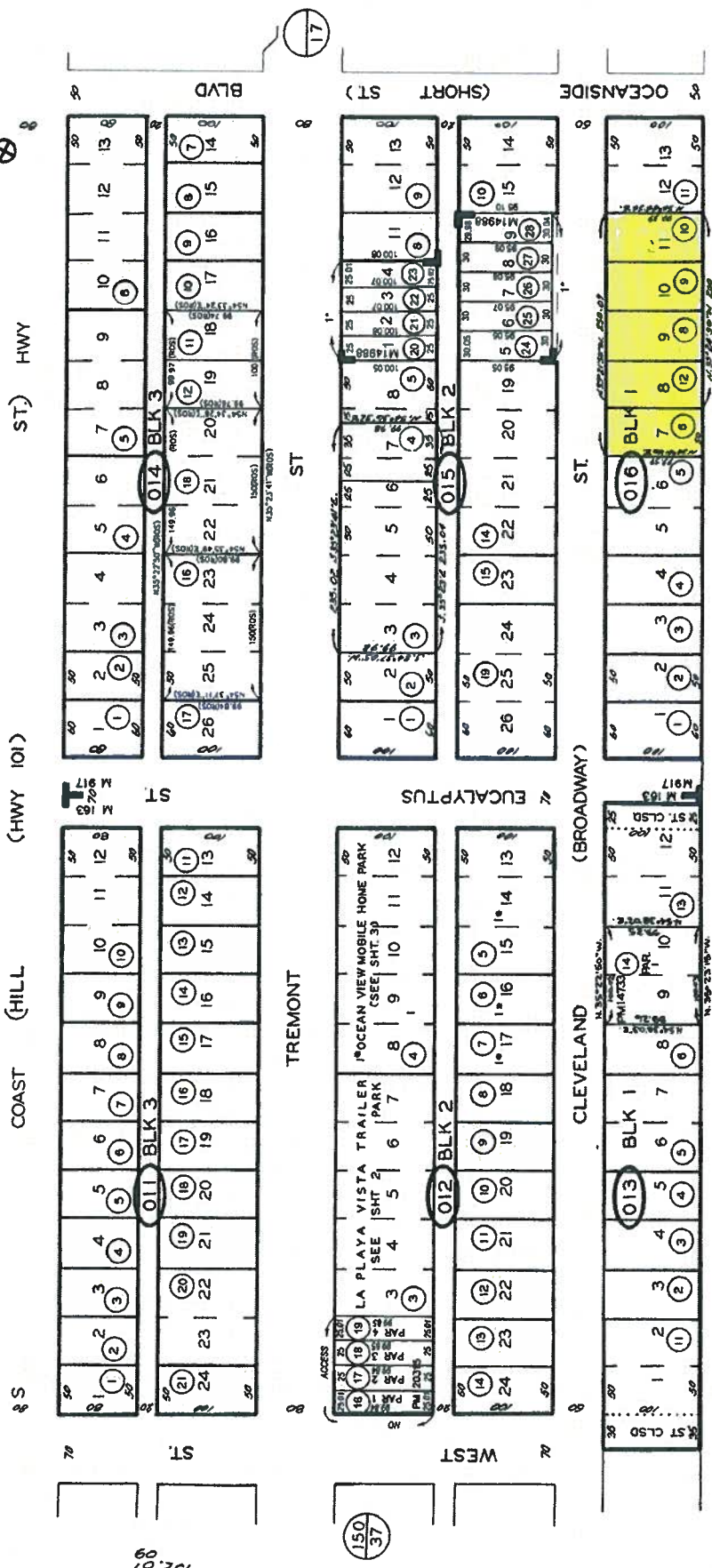


152-01
SHT 1 OF 3

STANLEY ST

THIS MAP WAS PREPARED FOR ASSESSMENT PURPOSES ONLY. NO LIABILITY IS ASSUMED FOR THE ACCURACY OF THE DATA SHOWN. ASSESSOR'S PARCELS MAY NOT COMPLY WITH LOCAL SUBDIVISION OR BUILDING ORDINANCES.

07



CHANGES

BLK OLD NEW YR CUT	013	13	09	05/01			
012	12	15	10	08/05			
016	16	18	19	01/05/17			
015	15	18	19	01/05/17			
013	13	14	08	13/11			
014	14	15	16	01/17/02			
015	15	16	17	08	45		
014	14	15	16	01	17/02		
015	15	16	17	20	20	08	45
012	12	15	16	19	08	11/46	

MAP 14988-TM T-1-03
MAP 917 - ENLOE'S ADD
MAP 163 - JARVIS ADD
ROS 14026;15021;16910

- 1* NO ACCESS
- 2* CTRL #9172

SAN DIEGO COUNTY
ASSESSOR'S MAP
BOOK 132 PAGE 01 SHT 1 OF 3

150
35

7

150
37

17

1125 S. CLEVELAND STREET

PROJECT DESCRIPTION

Applicant: Hallmark Communities Inc.

Sean Santa Cruz- Vice President of Planning and Development

740 Lomas Santa Fe Drive Suite 204

Solana Beach, CA 92075

(858) 481-3310 x122

ssantacruz@hallmarkcommunities.com

Address: 1125 S. CLEVELAND STREET OCEANSIDE, CA

APN: 152-016-06, 08, 09, 10, 12

Legal Description: LOTS 7, 8, 9, 10, 11 IN BLOCK 1 OF ENHLOE'S ADDITION TO OCEANSIDE IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 917 FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, AUGUST 24, 1904;

Total Acreage: 0.57 approx.

General Plan: High Density Residential

Zoning: R3 Medium Density Residential Zone; Coastal Zone

Land Use: Multi-Family Residential

Density: 29 du/ac to 43 du/ac (16 du to 24 du allowed)

Coast Highway Vision Plan: Arts, Technology & Environment District (Most Suited for Transit Oriented Development given proximity to Sprinter Station)

Existing Site: The existing site consists of a small single-family residence but mostly vacant land.

Adjacent Properties: North: Warehouse; South: Single Story Commercial Building; West: Railroad; East: Single-Family Row Homes & Trailer Park

Setbacks: Front (Cleveland) 15'; Sides: 5'; Rear (Railroad) 15'

Height: 35 feet

Description of Project: A Tentative Parcel Map, Development Plan, Variance, and Regular Coastal Permit to allow for the development of sixteen (16) single-family attached townhome units. The project is made

up into four-plexes, a twenty-four (24) foot wide private drive/motor court, trash enclosures, handicap parking stall, and landscape improvements. Eight (8) of the townhomes would open to and take access from Cleveland Street, and eight (8) of the townhomes would take access off either private patios in the rear yard setback (Scenario 1) or from the private drive/motor court off a dedicated four (4) foot walkway (Scenario 2). All of the units would include two (2) levels of habitable space above an enclosed two (2) car garage. Two (2) scenarios are being submitted for consideration and review by Staff.

Variance: The project requests a five (5) foot reduction in the standard fifteen (15) foot front yard setback along Cleveland Street for the following reasons:

Per Article 19 Variances Section 1900 & 1901 of the 1986 Zoning Ordinance, a practical difficulty and unnecessary hardship has resulted due to the required dedication of an additional five (5) feet of right-of-way on Cleveland Street and the resulting front yard setback imposed under the 1986 Zoning Ordinance. Under the now invalidated 1992 Zoning Ordinance for coastal properties, the required front yard setback for this property would have been ten (10) feet from the ultimate right-of-way if the property were to be developed as single family detached homes on a minimum lot of 2,500 SF, which was allowed with a Conditional Use Permit within the RH District when determined to be compatible with existing and future land uses and plans. Since the Garden Homes project (T-1-03, D-4-03, and RC-3-03) consisting of nine (9) detached single family homes is built across the street from the property, the applicant would have proposed the same development on the subject property as was approved and constructed next door, thus allowing for the same ten (10) foot front yard setback from the ultimate right-of-way on Cleveland Street.

Per Section 1701 (b) of the 1986 Ordinance, the project is required to provide a fifteen (15) foot front yard setback from the ultimate right-of-way. However, the requirement as previously indicated of an additional five (5) foot right-of-way dedication to the City of Oceanside (which is consistent with the Garden Homes Parcel Map 14988) would force the project to measure the fifteen (15) feet from the new right-of-way, and thus imposing essentially a twenty (20) foot setback from the current property line. Per Section 1703 Rear Yards, the project will require a fifteen (15) foot setback off of the rear of the property. The depths of the five (5) lots are approximately 99.39 to 99.59 feet. If the project is required to provide the aforementioned setbacks, the amount of developable area will be reduced by thirty-five (35) feet, which would place an unnecessary hardship on the project and a loss of significant amount of developable property compared to the development to the east in order to maximize density. As mentioned above, the developer of the adjacent Garden Homes property (T-1-03, D-4-03, and RC-3-03) which was permitted under the 1992 Zoning Ordinance (Section 1050 HH) was required to provide the five (5) foot right-of-way to the City of Oceanside and then was permitted under a Conditional Use Permit to have a ten (10) foot setback along Cleveland Street. The project is asking for the City to grant a variance to allow for the proposed project to match the setback approved for the adjacent nine (9) lot row home project. The Garden Homes project has been identified in the included aerial.

Additionally, the dedication is intended to provide additional right-of-way for utility burial and not for the purposes of widening the improved street dimensions. The dedicated strip of land would remain landscape area behind the sidewalk. Consequently, the proposed project would still maintain a

fifteen (15) foot setback from the improved portions of the right-of-way, all of which would be landscaped. The portion of Cleveland Street on which the project fronts is fully built out with curb, gutter, and existing paving per IP R13679 (i.e. existing paving will be replaced with required AC and AB as noted below). The desire by the City to make this section of Cleveland Street a section of road with a sixty (60) foot of right-of-way versus the more commonly found fifty-five (55) feet along Cleveland Street places an undue burden on the site and as such the project request for a variance should be considered for these aforementioned reasons.

Street Improvements: Improvement Plans (R13679)

Cleveland Street: The project proposes to follow the same improvement requirements as were implemented per IP R13679 for the Garden Homes development across the street.

The project proposes to install a new five (5) foot sidewalk made of 4" PCC over 6" Class II AB to match the existing northern and southern sidewalks. The existing 6" Type "G" Curb and Gutter is to remain in place and any necessary R&R will be performed as needed.

Water and Sewer shall be provided via the existing lines in Cleveland Street and extended to the project as needed. A new 6" PVC private sewer main shall be installed in the private drive/motor court along with new manholes and cleanouts and private 4" PVC sewer laterals to each of the sixteen (16) townhomes. The main will tie into the existing 8" Sewer Main in Cleveland Street. In regards to the water, there are two mains in Cleveland Street, a 4" AC main in front of the project, and an 8" AC main on the east side of Cleveland Street. We ask Staff to verify if we can tie into the existing 8" AC main or if the project will be required to upsize the 4" AC to an 8" AC along its frontage and tie into that main.

In regards to the AC pavement removal and replacement, per IP R13679, the Garden Homes project identified that Cleveland Street in this area is just asphalt over native with no base. They were required to replace a majority of the existing street with 3" AC over 6" AB as identified on the plan. As such, this project proposes to remove and replace the AC pavement that was NOT replaced by Garden Homes along its frontage. We ask Staff to identify any areas that maybe required to be replaced beyond the southern limits of the property line for the project.

Electrical, cable, telephone, and natural gas shall be provided from Cleveland Street, which currently exist either underground or overhead. The existing overhead lines/poles to the south and north of the property shall remain in place as they provide service to additional properties. Any secondary poles shall be removed from in front of the property. An existing street light is located on the property and an existing fire hydrant is located across the street and will be within 300' of the property.

Storm Water: The project will be classified as a Priority Development Project (Major) and the required compliance with the SUSMP and the generation of a Priority SWMP would be required for the project. All treatment would be met within the front setback along Cleveland Street (outside of the Right-of-Way). Given the quality of dirt at this location, infiltration of the storm water is anticipated and the installation of any spillover PVC pipe and D-27 area drains to discharge onto Cleveland Street can be expected.

Parking and Vehicle Circulation: The drive aisle proposed at the center of the property would provide access from Cleveland Street to a private drive/motor court which in turn would provide access to private two (2) car garages for each unit. The drive aisle and private drive/ motor court would maintain a minimum twenty-four (24) feet in width and would be surfaced with a combination of standard concrete, colored concrete, and decorative pavers. No guest parking is required under the 1986 Zoning Ordinance. However, there is on-street parking along Cleveland Street. One (1) handicap parking space is provided onsite.

Landscape: The project will provide a combination of groundcover, shrubbery, and tree species consistent with the City's Landscape Design Manual and approved street tree list within the fifteen (15) foot setback on Cleveland Street. Turf grass would be installed along the front property line as well and in the parkway if one is to be installed for the project. Ground floor AC units are proposed for the eight (8) units facing Cleveland Street, but would be screened by landscaping. The AC units for the eight (8) interior units would be placed within their private patios or rear yards depending on if Scenario 1 or 2 is considered for the proposed project. A new irrigation meter shall be installed off of Cleveland Street to provide service to the common area landscaping.

Inclusionary Housing: The project shall choose to pay the inclusionary housing in-lieu fee for all units. Additionally, the 2nd Tier Inclusionary Housing in-lieu fees shall only apply if a project exceeds base density and has more than ten (10) units proposed. Although the project proposes sixteen (16) units, it DOES NOT exceed the base density which is 16 du/ac which means it will be exempt from being required to pay the 2nd Tier Inclusionary Housing in-lieu fee.

Trash/Recycling: The project would provide two (2) common decorative trash enclosures that would provide a total of two (2) trash and two (2) recycling bins for the residents. These enclosures are placed at the rear of the property and provide access for Waste Management to roll out the bins into the private drive/ motor court on a weekly basis. Distance between the bins and the units would be less than maximum 150'. Clearances are maintained in front of the enclosures as required by the Refuse and Recycling Enclosures handout.

Walls/Fencing: The project proposes to install a six (6) to eight (8) foot tan split face masonry wall along the entire rear property line for sound attenuation purposes. The size and layout will be determined by the Acoustical Engineer for the project, however will staff support a wall higher than six (6) feet along the rear of the property? Can a two (2) foot berm with a six (6) wall on top work and still meet the requirements of Section 1721 (a) of the 1986 Zoning Code? Wood fencing would be proposed along the side yards as needed. No fencing is proposed along the Cleveland Street frontage.

Architecture: The architecture of the townhomes would be an urban beach/beach cottage design with more articulated facades and a variety of finish materials, including stucco, fiber cement siding, stone veneer, decorative rails, metal brackets, exposed rafters, projected eaves, and concrete tile roofs. Additionally it would include inset balconies, furred-out wall sections, foam window trim, and score lines. A sample of the style has been included in the package to review. NOTE: The plan submitted is just for reference and does not represent the correct width and final design of the units proposed for the

site nor does it account for the different configuration of the interior eight (8) units under both Scenario 1 and 2.

Floor Plans: The units would be as follows under the two (2) scenarios:

Scenario 1: Units would be 26' w x 23' d; First Floor: 2 car garage; entry; stairs; Second Floor: Living, Dining, Kitchen, Powder Bath; Third Floor: Beds & Baths. The 2nd and 3rd floor would have a four (4) foot cantilever into the motor court on the interior units, while the end units would be a full bridge over the private drive/motor court on the 2nd and 3rd floor. Estimated square footage of the units would be 1,200 SF to 1,600 SF. Access to eight (8) units would be from Cleveland Street and access to eight (8) units would be from a four (4) foot delineated walkway along the rear of the property and each would have private patios with 42" high fencing and gates proposed to enclose the private patios for each unit.

Scenario 2: Units would be 26' w x 21' d; First Floor: 2 car garage; entry; stairs; Second Floor: Living, Dining, Kitchen, Powder Bath; Third Floor: Beds & Baths. The 2nd and 3rd floor would have a four (4) foot cantilever on all units over the private drive/motor court. Estimated square footage of the units would be 1,100 SF to 1,300 SF. Access to eight (8) units would be from Cleveland Street and access to eight (8) units would be from the private drive/motor court via a four (4) foot delineated pathway which would be outside of the required twenty-four (24) foot private drive/motor court. As such, the eight (8) units in the rear of the property would have private rear yards abutting the railroad tracks.

QUESTIONS:

- 1) Will the City support a variance for the front yard setback for the reasons described in the Project Description?
- 2) Where does Utilities expect the placement of the individual water meters for the eight (8) units to the rear of the property? Can they be placed in the private drive/motor court and covered with steel grates or do all sixteen (16) meters need to be located in the Right-of-Way on Cleveland Street?
- 3) There appears to be two (2) water lines located in Cleveland Street. The water line closest to the project is a 4" AC line. Can the project tie into that line, or it will be required to go across Cleveland Street to tie into the existing 8" AC main on the east side of Cleveland Street? If it has to tie into the 4" AC line, will the City require an upgrade to an 8" water line along our frontage?
- 4) Will the City allow for access to front doors within the private drive/motor court so long as an additional four (4)' is provided outside of the twenty-four (24) foot drive/motor court area?
- 5) Will the layout and location of the trash enclosures and handicap spot be satisfactory for the City and Waste Management?
- 6) Will Staff support a sound attenuation wall higher than six (6) feet along the rear of the property if the Acoustical Study determines that it is mandated? What about a two (2) foot berm with a six (6) foot masonry wall on top to reach the eight (8) foot if that is what is required? Will that meet the intent of Section 1721 (a) of the 1986 Zoning Code.

Exhibits Provided:

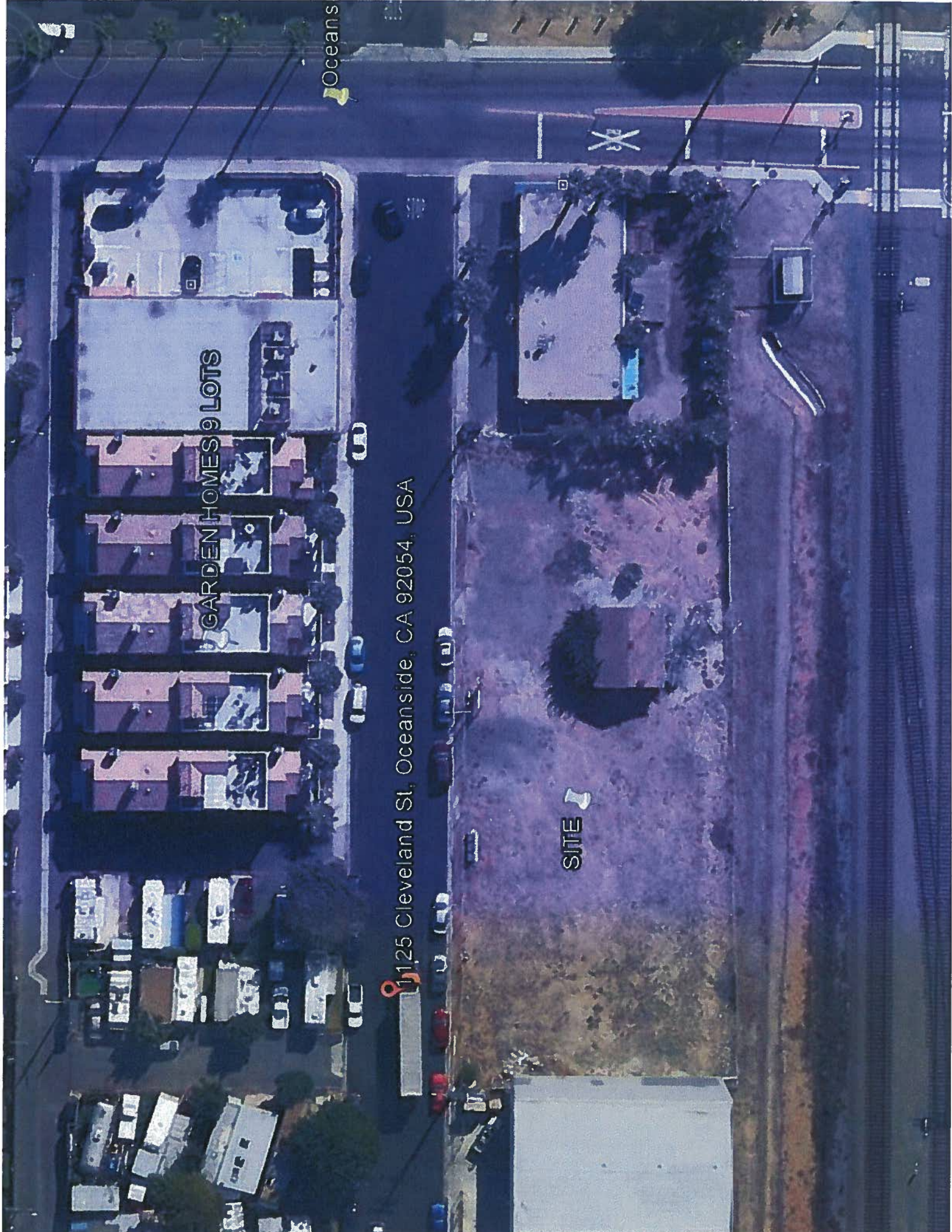
- 1) **Plat Map**
- 2) **Aerial Photo**
- 3) **Site Plan: Scenario 1**
- 4) **Site Plan: Scenario 2**
- 5) **Garden Homes Plan**

GARDEN HOMES 9 LOTS

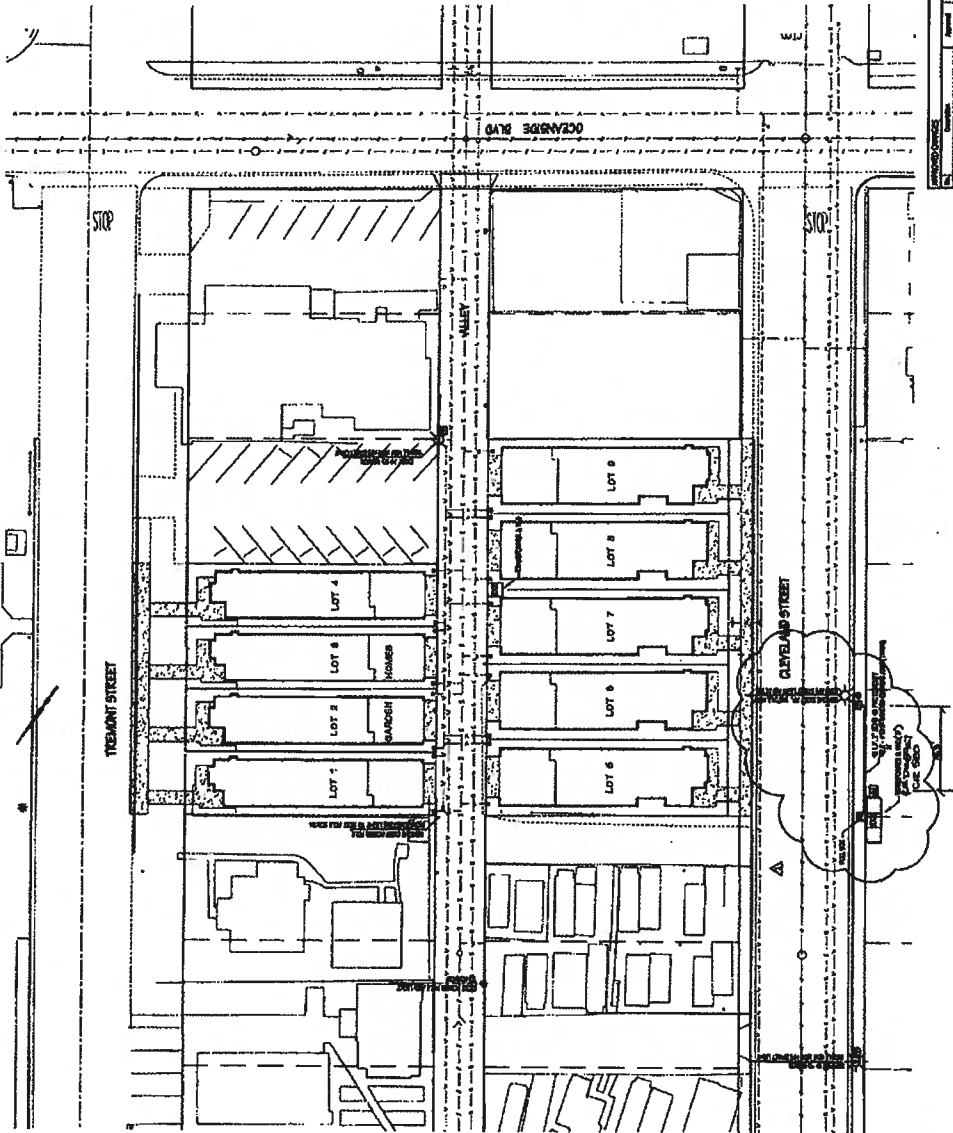
Oceanside

1125 Cleveland St, Oceanside, CA 92054, USA

SITE

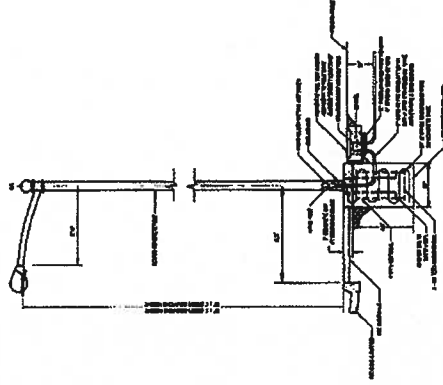


STREET LIGHTING PLAN GARDEN HOMES



LEGEND

SYMBOL	REFERENCE	STANDARD	QUANTITY
(Symbol)	CENTRAL STREET LIGHT	*	1 EA.
(Symbol)	NEW STREET LIGHT - 150 WPS	**	1 EA.
(Symbol)	NEW ROAD SIDE FURNISHED STREET LIGHT - 150 WPS	**	1 EA.



PROJECT INFORMATION

PROJECT NAME: GARDEN HOMES - CLEVELAND STREET

DATE: 11/21/14

SCALE: AS SHOWN

DESIGNER: [Signature]

CHECKED: [Signature]

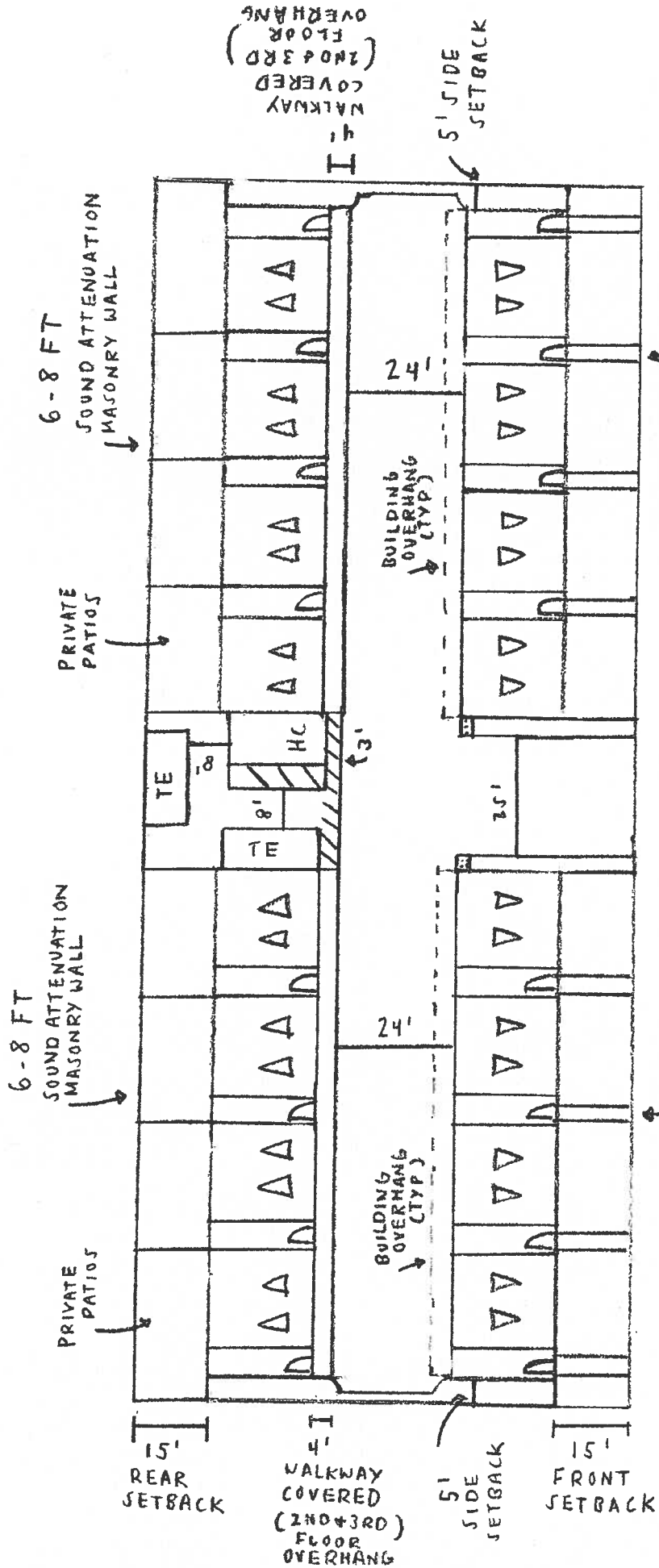
APPROVED: [Signature]

REVISIONS

NO.	DESCRIPTION	DATE
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SCENARIO 2

RAILROAD



CLEVELAND STREET

