



**CITY COUNCIL
AGENDA
MAYOR AND COUNCIL WORKSHOP**

**August 14, 2019
6:00 p.m.**

ADJOURNED MEETING

City Council Chambers
300 North Coast Highway

**CALL TO ORDER
PLEDGE OF ALLEGIANCE
ROLL CALL**

CONSENT CALENDAR ITEMS

1. City Council/Harbor/CDC/OPFA: Approval to waive reading of the text of all ordinances and resolutions considered at this meeting and provide that ordinances shall be introduced after a reading of only the title(s)

PUBLIC HEARING ITEMS

2. City Council: Take actions regarding the Coast Highway Corridor Study in order to implement Alternative 3 which proposes amendments to the Land Use and Circulation Elements of the General Plan and Zone Amendment and Local Coastal Plan Amendment that will re-designate Coast Highway from a four-lane collector to a two-lane collector from Harbor Drive to Morse Street while maintaining a four-lane collector from Morse Street to the City's southern boundary. The proposed land use changes would establish an optional zoning incentive district that would provide for additional building height and residential density in exchange for public benefits in nodal areas, standalone residential uses in avenue segments, and streamlined permit review for projects conforming to form-based development standards. The zoning incentive district would extend along Coast Highway from Seagaze Drive to Morse Street.

The actions to be taken include: 1) adoption of a resolution certifying the Final Environmental Impact Report (FEIR) and associated Mitigation, Monitoring, and Reporting Program (MMRP), Findings of Fact, and Statement of Overriding Considerations; 2) adoption of a resolution adopting General Plan Amendment (GPA16-00001) to amend the Land Use Element and the Circulation Element; 3) introduction of an ordinance adopting Zone Amendment (ZA19-00004); and 4) adoption of a resolution approving Local Coastal Plan Amendment (LCPA19-00005)

- A) Mayor opens Public Hearing
- B) Mayor requests disclosure of Councilmember and constituent contacts and correspondence
- C) City Clerk presents correspondence and/or petitions
- D) Testimony beginning with Jonathan Borrego, Development Services Director
- E) Recommendation – select Alternative 3, adopt the resolutions, and introduce the ordinance

3. Public Communication on City Council Matters (off-agenda items)

ADJOURNMENT

The next regularly scheduled meeting is at 3:30 p.m. on Wednesday, August 21, 2019

AGENDA POSTING AND MATERIALS

The agenda has been posted at least 72 hours prior to the meeting at the Civic Center Plaza, 300 North Coast Highway. The agenda may also be inspected at the City Clerk's Office at 300 North Coast Highway. Persons requiring assistance or auxiliary aids in order to participate may contact the City Clerk at 300 North Coast Highway, Oceanside, CA, telephone (760) 435-3000 at least 24 hours prior to the meeting.

STAFF REPORT



ITEM NO. 1
CITY OF OCEANSIDE

DATE: August 14, 2019
TO: Honorable Mayor and City Councilmembers
FROM: Michelle Skaggs Lawrence, City Manager
SUBJECT: WAIVER OF READING OF ORDINANCES AND RESOLUTIONS

ANALYSIS

The City Council/Harbor Board/CDC/OPFA has adopted a policy that it is sufficient to read the title of ordinances at the time of introduction and adoption, and that reading of the full text of ordinances and the full text and title of resolutions may be waived.

Approval of this consent calendar item allows the City Council/Harbor Board/CDC/OPFA to waive the reading of the full text and title of all resolutions agendized in the Consent Calendar, as well as the full text of all ordinances agendized in either the Introduction and Adoption of Ordinances or General Items sections. **This particular consent calendar item requires unanimous approval of the City Council/Harbor Board/CDC/OPFA.**

Upon approval of this item as part of the Consent Calendar, all resolutions included in the motion and second to approve the Consent Calendar shall be approved. Those resolutions removed from the Consent Calendar and considered under separate action may also be approved without the reading of the full text and title of the resolutions.

Also upon approval of this item, the City Attorney will read the titles of all ordinances included in the Introduction and Adoption of Ordinances section. After reading of the ordinance titles, the City Council/Harbor Board/CDC/OPFA may introduce and/or adopt all the ordinances in one motion and second.

RECOMMENDATION

Staff recommends that the City Council/Harbor Board/CDC/OPFA approve the waiving of reading of the text of all ordinances and the text and title of all resolutions included in this agenda. **Unanimous approval of the City Council/Harbor Board/CDC/OPFA is required.**

Respectfully Submitted,

A handwritten signature in blue ink, appearing to read "Michelle Skaggs Lawrence".

Michelle Skaggs Lawrence
City Manager



DATE: August 14, 2019
TO: Honorable Mayor and City Councilmembers
FROM: Public Works and Development Services Departments
SUBJECT: **COAST HIGHWAY CORRIDOR STUDY ACTIONS**

SYNOPSIS

Staff recommends that the City Council take the following actions regarding the Coast Highway Corridor Study in order to implement Alternative 3, which includes complete street improvements from Harbor Drive to Morse Street and a zoning incentive district from Seagaze Drive to Morse Street.

1. Adopt a resolution certifying the Final Environmental Impact Report (FEIR) and adopting the associated Mitigation, Monitoring, and Reporting Program (MMRP), Findings of Fact, and Statement of Overriding Considerations (Attachment 1).
2. Adopt a resolution adopting General Plan Amendment (GPA16-00001) to amend the Land Use Element and the Circulation Element (Attachment 2).
3. Introduce an ordinance adopting Zone Amendment (ZA19-00004)(Attachment 3).
4. Adopt a resolution approving Local Coastal Plan Amendment (LCPA 19-00005) (Attachment 4).

The recommended actions outlined above reflect the recommendation of the Planning Commission as well as prevailing public sentiment expressed at the June 10, 2019 Planning Commission public hearing on the proposed project. More specifically, the recommended actions provide for the adoption of Alternative 3 of the Final Environmental Impact Report, which recommends removing the segments south of Morse Street thus implementing complete street improvements between Harbor Drive and Morse Street and the zoning incentive district between Seagaze Drive and Morse Street.

BACKGROUND

In April 2009, the City Council adopted the Coast Highway Vision and Strategic Plan (CHVSP) (09-R0245-1). The Coast Highway Vision and Strategic Plan is based on a set of Livable Communities and Smart Growth principals intended to produce vibrant and economically successful communities. The plan provides a multi-pronged

Implementation Strategy of Planwide Initiatives, as well as specific *Action Items* for short and long-term physical enhancements (see Attachment 5, Action Items table from page 66, Coast Highway Vision and Strategic Plan).

In August 2013, the City Council approved a Professional Services Agreement (PSA) with The IBI Group (transportation) and Environmental Science Associates (ESA) (land use and environmental) to analyze the potential impacts associated with transportation and land use concepts adopted in the 2009 CHVSP.

In April of 2016, the City Council provided direction to staff to proceed with the analysis of the Preferred Project. The Preferred Project was developed as a result of community input received through four public meetings, with oversight and direction provided by the steering committee.

The Preferred Project identified in the EIR proposes to implement road diet features along the existing 3.5-mile stretch of Coast Highway between Harbor Drive and Eaton Street by reducing the travel lanes to one lane in each direction, separated by a raised median, with bicycle lanes and bicycle lane buffers, mid-block crosswalks and roundabouts. The Preferred Project also included an alternative to base zoning standards that provides for additional building height, additional residential density, and streamlined project review in exchange for public benefits along fronting properties on Coast Highway between Seagaze Drive and Morse Street.

Alternative 3 proposes road diet features along Coast Highway between Harbor Drive to Morse Street with Incentive District zoning only between Seagaze Drive and Morse Street. Other streetscape features such as mid-block crosswalks and landscaping is included south of Morse Street to south City limits.

ANALYSIS

An environmental impact report (EIR) has been prepared, and evaluated the following direct and cumulative impacts: Land Use, Geology/Soils, Aesthetics, Agricultural Resources, Hazardous Materials, Hydrology/Water Quality, Air Quality, Traffic, Greenhouse Gas, Paleontological Resources, Noise, Cultural Resources, and Biological Resources. All of the significant impacts of the Coast Highway Corridor Study are identified in the Final EIR. Under the provisions of the California Environmental Quality Act (CEQA), the City Council should adopt resolutions to certify the EIR and the Mitigation Monitoring and Report Program (MMRP), and adopt the Findings of Fact and the Statement of Overriding Considerations. A summary of findings of the FEIR is provided below.

Transportation

Alternative 3 analyzed a total of 56 intersections along Coast Highway, Harbor Drive, I-5 on-/off-ramps, Surfrider Way, Mission Avenue, Wisconsin Avenue, Oceanside Boulevard, Morse Street, Cassidy Street and Vista Way within the project study area.

Analysis of study area intersections was completed for the Alternative 3, the Preferred Project, and all other project alternatives. Of the 56 intersections, 7 intersections on Coast Highway were analyzed with roundabouts in place of existing traffic signals (12 intersections on Coast Highway were analyzed with roundabout in the Preferred Project). The City's minimum Level of Service (LOS) standard for intersections is LOS D. The Caltrans minimum LOS standard for free-way on-/off-ramps is LOS C.

The following transportation mitigation measures are recommended under Alternative 3:

- Coast Highway at Pier View Way: maintain existing traffic signal
- Seagaze Drive at Ditmar Street: install traffic signal
- Coast Highway at Wisconsin Avenue: not mitigatable
- Wisconsin Avenue at Ditmar Street (south): not mitigatable
- Oceanside Boulevard at I-5 SB On/Off-Ramp: install two left-turn lanes and a shared thru-right lane with 100 feet of storage; not fully mitigated

In addition to the Preferred Project and Alternative 3 described in the sections above, a No Project Alternative and three additional project alternative scenarios were analyzed. Evaluating these alternatives is a way to avoid or lesson the significant environmental effects resulting from the implementation of the proposed project, while attaining most of the project objectives. Below is a summary of each of the alternatives analyzed:

- **No Project Alternative:** No project would be adopted. The project area would remain as it is in existing conditions where Coast Highway would consist of four travel lanes, and the Incentive District (ID) would not be established.
- **Alternative 1:** Includes Complete Streets improvements to extend from Harbor Drive to Oceanside Boulevard. The roadway would remain four lanes between Oceanside Boulevard and Vista Way, although streetscape improvements would continue the length of the corridor. In addition, the ID would be adopted from Seagaze Drive South.
- **Alternative 2:** Includes Complete Street improvements to extend from Harbor Drive to Morse Street. The roadway would remain four lanes between Morse Street and Vista Way, although streetscape improvements would continue to occur the length of the corridor. The ID would be adopted from Seagaze Drive South.
- **Alternative 3:** Includes Complete Street improvements to extend from Harbor Drive to Morse Street. The roadway would remain four lanes between Morse Street and Vista Way. Streetscape improvements would continue to occur the length of the corridor. In addition, the ID would be adopted but its southern boundary would also terminate at Morse Street.

- **Alternative 4:** Includes Complete Streets improvements the length of the corridor (Harbor Drive to Vista Way), as is included in the Preferred Project. The ID would not be adopted for any portion.

Recommendation of the Preferred Project from Harbor Drive to Eaton Street with incentive zoning standards from Seagaze Drive to the Buena Vista Lagoon, in staff's opinion, best achieves the Vision and Strategic Plan's goal of the "revitalization and enhancement of the Coast Highway". However, on June 10, 2019, the Planning Commission recommended Alternative 3 which is considered by staff to be the best alternative to the Preferred Project. Alternative 3 responds directly to input received from South Oceanside stakeholders opposing both the road diet and incentive district.

Land Use

The proposed Coast Highway Incentive District (ID) (see Attachment 6) would implement policy direction in the Coast Highway Vision and Strategic Plan (Vision Plan) that calls for the transformation of the built environment within the Coast Highway corridor through "high quality urban and architectural design, sustainable development, synergistic land uses, and enhancement of environmental resources." The implementation strategy outlined in the Vision Plan encourages the preparation of "a set of incentives" that promote pedestrian-friendly, transit-oriented mixed-use development. In keeping with this direction, the ID establishes an optional zoning program that provides for streamlined project review and in some areas additional building height and residential density in exchange for high-quality design and public benefits that include public open space, public parking, and commercial floor area (FAR) exceeding minimum requirements.

Incorporated into the City's Zoning Ordinance as an alternative to the General Commercial (C-2), Visitor Commercial (VC), and Medium Density Residential (R-3) existing zoning districts that would remain within the Coast Highway corridor, the ID divides the corridor into distinct sub-districts, each subject to different land use and development standards. Applicants pursuing new development within the corridor would thus have the option of seeking approval under the existing zoning standards or those established by the ID.

Sub-Districts: Nodes, Avenues, and Commercial Villages

As promulgated in the Vision Plan, the ID creates three sub-districts within the Coast Highway corridor that together provide for a diversity of land uses, development patterns, and streetscapes; see map in Attachment 6. Each of these sub-districts is described below:

- **Nodes:** Areas in proximity to transit service and/or freeway and coastal access that allow for relatively intense commercial activity and residential density, with taller and more street-adjacent buildings than those found in the other two sub-districts. Development in nodal areas is eligible for additional building height and residential density in exchange for public benefits.

- **Avenues:** Segments of Coast Highway between nodal areas that feature lower-profile development with more expansive building setbacks and landscape areas. Standalone residential use is allowed in avenue segments, subject to more restrictive development standards. The building height and residential density incentives established for nodal areas are not applicable to avenue segments.
- **Commercial Villages:** Areas where low-intensity commercial use and low-profile development have created a distinct character that stakeholders generally wish to preserve. Thus, land use and development standards in the commercial villages generally align with base zoning standards. However, through conformance to form-based zoning standards outlined in the ID, applicants can pursue streamlined project review.

Incentives and Public Benefits

The ID includes a residential incentive program that allows: 1) new development in nodal areas to exceed maximum residential density standards in exchange for public benefits; and 2) standalone residential in avenue segments (where existing zoning standards allow residential use only in conjunction with mixed-use development). The incentive program seeks to address ongoing demand for new housing while ensuring that new development in the corridor results in a compatible and synergistic mix of land uses, activation of the street frontage, an attractive and inviting streetscape and support for commercial businesses.

Applicable only in nodal areas, additional density up to 63 dwelling units per acre would be allowed in exchange for the following public benefits:

- Public open space, at a minimum ratio of 200 square feet per additional dwelling unit;
- Public parking, at a minimum ratio of one parking space per additional dwelling unit;
- Increased commercial floor area, at a minimum ratio of 250 square feet per additional dwelling unit; and/or
- Payment of a public improvement fee, the amount of which would be established by the City Council.

Essentially, the above-noted public benefits provide a menu of options for achieving residential densities above the base density allowance of 43 du/acre. To accommodate additional density, additional building height up to 65 feet and six stories could be proposed through a discretionary review process (i.e., a development plan reviewed by the Planning Commission). To ensure vertical articulation and architectural interest, development over 45 feet would be limited to a 55-foot average height.

The allowance for standalone residential use in avenue segments of the corridor responds to strong demand for housing in the San Diego region and the City's need to accommodate its regional fair share of new housing growth. By incentivizing more housing within the corridor, the allowance for standalone residential would increase

support for commercial uses and bring a significant 24-hour presence to the area, which has been shown to deter uncivil and criminal activity.

With building height limits and minimum setback requirements for standalone residential being more restrictive than those for standalone commercial and mixed-use development, avenue segments are expected to exhibit a relatively spacious feel, providing a boulevard appearance with sense of openness between nodal areas.

The allowance for standalone residential in avenue segments is supported by economic analyses that indicate that the total supply of commercial land within the corridor far exceeds anticipated demand over the long term. With more than three contiguous miles of commercial zoning, private property within the ID provides for more than 1,100,000 square feet of ground-level commercial floor area (assuming an FAR of 0.25). Market studies suggest that no more than 700,000 square feet of commercial floor area will be needed to accommodate anticipated demand through 2030.

In addition to standalone residential, avenue segments would continue to allow for standalone commercial and mixed-use development through existing zoning.

Form-Based Development Standards and Streamlined Project Review

In contrast to traditional development standards, which typically only regulate the location and scale of buildings, form-based development standards are more detailed, addressing features such as horizontal and vertical articulation, architectural details, fenestration, finish materials, pedestrian access, parking configuration, and the relationship between buildings and on-site open space areas. Form-based development standards not only regulate the massing of buildings but also their visual quality and their effectiveness in accommodating intended uses and activating adjacent public spaces. Without prescribing particular architectural styles, form-based development standards ensure that building form and site design contribute to the desired look and feel of the surrounding area.

Given the level of certainty that form-based development standards provide regarding the quality and compatibility of proposed development, it is staff's position that the review process for proposals subject to such standards should be streamlined to the extent possible, while continuing to provide opportunities for public input. Accordingly, the ID establishes an administrative review process for proposals that do not exceed building height or residential density maximums. The City's administrative review process vests the City Planner with the authority to approve or deny projects, while requiring public notification when applications are submitted and when City Planner determinations are pending (per the City's Enhanced Notification Policy). Projects in nodal areas seeking additional building height and residential density would be subject to Planning Commission review and approval.

As is currently the case under base zoning standards, both City Planner and Planning Commission decisions on projects reviewed under the ID would be subject to appeal. The ID establishes grounds for appeal in Section 903(H).

Parking

Restrictive commercial parking requirements have been an impediment to infill and redevelopment within the Coast Highway corridor, particularly on smaller properties. Better suited to suburban environments with limited transit service and walkability, these costly parking standards have led to inefficient land use and poor site design. Recognizing that revitalization of the Coast Highway corridor requires new approaches to parking, the Vision Plan calls for strategies that reduce parking demand, ensure that the cost of parking is borne by those who use it, leverage technology to make parking easier to find and pay for, and invest parking revenues in those neighborhoods that supply the parking. The Vision Plan promotes a “rational” approach to parking – i.e. one that balances parking supply with demand and thereby encourages efficient use of parking resources.

As recommended by the Vision Plan, the ID establishes a minimum commercial parking requirement of one space for every 500 square feet of gross floor area. This standard is comparable to recently updated commercial parking requirements for smart growth areas in other cities in the San Diego region, including San Diego, Carlsbad, and La Mesa.

Consistent with existing district standards, the ID requires one parking space per dwelling unit in mixed-use development. For standalone residential, the ID requires one parking space for every 1,500 square feet of gross floor area.

Following adoption of the ID, staff intends to further study parking strategies outlined in a memorandum prepared by IBI. This memorandum acknowledges, refines, and builds upon the parking strategies outlined in the Vision Plan. One strategy noted in the memorandum is a parking in-lieu fee that would provide funding for City-owned and operated parking facilities within the Coast Highway corridor.

Current Building Height and Residential Density Allowances in the C-1, C-2, and V-C Zoning Districts

The vast majority of the property within the ID currently bears a zoning designation of General Commercial (C-2), with limited areas bearing the C-1 and VC designations. In place since the late 1950s, these designations allow for building height up to 45 feet/four stories and residential density up to 43 dwelling units per acre in conjunction with mixed-use development. Additional building height, with no prescribed maximum, can be approved through a conditional use permit (“High-rise structures” per Zoning Ordinance Section 1130C). Thus, while much of Coast Highway is currently characterized by relatively low-profile development and minimal residential use, existing zoning standards allow for significantly taller buildings and extensive high-density housing. More detailed analysis of the relationship between existing zoning standards and the ID is provided in the Coast Highway Incentive District Fact Sheet, appended as Attachment 6.

ENVIRONMENTAL DETERMINATION

CEQA Guidelines Section 15126.2(b) requires that the EIR describe any significant impacts that cannot be avoided, including those impacts that can be mitigated but not reduced to a less-than-significant level. A summary of the identified impacts and mitigation measures are provided in the Mitigation, Monitoring, and Report Program (MMRP) (online). Implementation of the proposed project would result in significant impacts to air quality, biological resources, cultural resources, greenhouse gas (GHG) emissions, noise and vibration, transportation and traffic. However, most of these impacts would be mitigated to below a level of significance with implementation of mitigation measures identified in the EIR.

Listed below are significant impacts that cannot be mitigated to a less-than-significant level, and therefore are considered significant unavoidable impacts:

- Contribution to an existing or projected air quality violation associated with future construction and operational activities that are related to the land uses permitted by the Incentive District and cumulative projects.
- Contribution to a cumulatively considerable net increase of a criteria pollutant for which the project region is in nonattainment associated with construction and operation of the Incentive District and cumulative projects.
- Contribution to a net increase in GHG emissions in the aggregate associated with the Incentive District and cumulative projects.
- Operational noise impacts along Wisconsin Avenue between Freeman Street and Ditmar Street associated with the Complete Streets improvements, the Incentive District, and cumulative projects.
- Temporary substantial increase in ambient noise levels associated with the Complete Streets improvements, the Incentive District, and cumulative project construction.
- Contribution to unacceptable levels of service (LOS) at the intersections of Coast Highway at Wisconsin Avenue and Vista Way at I-5 southbound ramps associated with the proposed project in the Future with Project scenario.

A Statement of Overriding Considerations has been prepared to establish that significant and unmitigable impacts associated with the project are outweighed by the project's many environmental, economic, and social benefits. These benefits are summarized in PC Resolution 2019-P23 and are included within Attachment 1 to this report.

FISCAL IMPACT

Does not apply.

INSURANCE REQUIREMENTS

Does not apply.

COMMISSION OR COMMITTEE REPORT

On June 10, 2019, the Planning Commission reviewed the proposed project and voted unanimously to recommend City Council adoption of Alternative 3 of the FEIR. The majority of public testimony at the public hearing expressed support for Alternative 3, though several speakers indicated a preference for the preferred project.

CITY ATTORNEY'S ANALYSIS

The City Council is authorized to hold a public hearing in this matter. Consideration of the matter should be based on the testimony and evidence presented at the hearing and included in the administrative record. After conducting the public hearing, the Council shall affirm, modify or deny the application. The supporting resolutions and ordinance have been approved as to form by the City Attorney.


(Continued on next page)

RECOMMENDATION


Staff recommends that the City Council take the following actions regarding the Coast Highway Corridor Study in order to implement Alternative 3, which includes complete street improvements from Harbor Drive to Morse Street and the zoning incentive district from Seagaze Drive to Morse Street:

1. Adopt a resolution certifying the Final Environmental Impact Report (FEIR) and adopting the associated Mitigation, Monitoring, and Reporting Program (MMRP), Findings of Fact, and Statement of Overriding Considerations (Attachment 1).
2. Adopt a resolution adopting General Plan Amendment (GPA16-00001) to amend the Land Use Element and the Circulation Element (Attachment 2).
3. Introduce an ordinance adopting Zone Amendment (ZA19-00004) (Attachment 3).
4. Adopt a resolution approving Local Coastal Plan Amendment (LCPA 19-00005) (Attachment 4).

PREPARED BY:



John Amberson
Transportation Planner



Russ Cunningham
Principal Planner

SUBMITTED BY:



Michelle Skaggs Lawrence
City Manager

REVIEWED BY:

Deanna Lorson, Assistant City Manager
Jonathan Borrego, Development Services Director
Kiel Koger, Public Works Director
Brian Thomas, City Engineer
David DiPierro, City Traffic Engineer
Jane McPherson, Financial Services Director



Attachments:

1. City Council Resolution (FEIR, MMRP and Findings of Fact)
2. City Council Resolution (GPA16-00001)
3. City Council Ordinance (ZA 19-00004)
4. City Council Resolution (LCPA 19-00005)
5. CHVSP Table of Action Items
6. Coast Highway Incentive District Fact Sheet
7. Planning Commission Resolutions 2019-P23 (FEIR), 2019-P21 (GPA) and 2019-P22 (ZA/LCPA)

RESOLUTION NO.

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF OCEANSIDE, CALIFORNIA CERTIFYING THE FINAL ENVIRONMENTAL IMPACT REPORT AND ADOPTING THE FINDINGS OF FACT, MITIGATION, MONITORING, AND REPORTING PROGRAM AND STATEMENT OF OVERRIDING CONSIDERATIONS FOR THE COAST HIGHWAY CORRIDOR STUDY, INCLUDING COMPLETE STREET IMPROVEMENTS AND THE COAST HIGHWAY INCENTIVE DISTRICT (GPA16-00001/ZA19-00004/LCPA19-00005)

(Applicant: City of Oceanside)

WHEREAS, on April 15, 2009, the City Council adopted the Coast Highway Vision and Strategic Plan, which provides policy direction for the revitalization of the Coast Highway corridor; and

WHEREAS, on August 21, 2013, the City Council authorized staff to initiate the Coast Highway Corridor Study, which considers roadway improvements and zoning incentives identified as action items in the Coast Highway Vision and Strategic Plan; and

WHEREAS, a Final Environmental Impact Report for the project was prepared and circulated for public and agency review, and proper notification was given, in accordance with the California Environmental Quality Act; and

WHEREAS, on April 22, 2019, the Planning Commission of the City of Oceanside, after giving the required notice, conducted a duly-advertised public hearing as prescribed by law to consider the Final Environmental Impact Report (FEIR) for the Coast Highway Corridor Study; and

WHEREAS, the Planning Commission adopted Planning Commission Resolution No. 2019-P, recommending City Council certification the FEIR, with its Mitigation, Monitoring, and Reporting Program (MMRP), Findings of Fact, and Statement of Overriding Considerations; and

1 WHEREAS, the City Council, on the 14th day of August, 2019, did conduct a
2 public hearing to consider certification of the FEIR, Findings of Fact, MMRP and
3 Statement of Overriding Considerations; and

4 WHEREAS, the MMRP, Findings of Fact, and Statement of Overriding
5 Considerations are appended to this resolution as Exhibits A, B, and C, respectively; and

6 WHEREAS, studies and investigations made by this Council and in its behalf
7 reveal the following facts:

8 For the Final Environmental Impact Report:

- 9 1. The FEIR, Findings of Fact, MMRP and Statement of Overriding Considerations
10 were completed in compliance with the provisions of the California Environmental
11 Quality Act (CEQA).
12
13 2. There are certain significant environmental effects detailed in the FEIR which have
14 been avoided or substantially lessened through measures, which are detailed in the
15 Findings of Fact and MMRP.
16
17 3. There are other significant environmental effects detailed in the FEIR which cannot
18 be feasibly mitigated to a less than significant level. For these significant and
19 unmitigable impacts, a Statement of Overriding Considerations has been prepared.
20
21 4. The FEIR, Findings of Fact, MMRP and Statement of Overriding Considerations
22 were presented to the City Council, and prior to approving the project, the City
23 Council reviewed and considered the information contained in these documents.
24 The FEIR, Findings of Fact, MMRP and Statement of Overriding Considerations
25 for the project have been determined to be accurate and adequate documents,
26 reflecting the independent judgment of the City.

27 NOW, THEREFORE, the City Council of the City of Oceanside does resolve as
28 follows:

1. The City Council does hereby certify the FEIR for the Coast Highway Corridor
Study, a copy of which is on file with the Office of the City Clerk.

1 2. Pursuant to Public Resources Code Section 21081, the City Council hereby adopts
2 the Findings of Fact.

3 3. Pursuant to Public Resources Code Section 21081.6, the City Council adopts the
4 MMRP and finds and determines that said program is designed to ensure
5 compliance with the mitigation measures throughout the implementation of the
6 project.

7 4. Pursuant to CEQA Guidelines Section 15093, the City Council concludes that, on
8 balance, the environmental, economic, and social benefits of the project outweigh
9 its significant and unmitigable impacts. The City Council hereby adopts the
10 Statement of Overriding Considerations. Benefits of the project that prevail over
11 its significant and unmitigable impacts include the following:
12

- 13 a. Accommodating future population, housing, and employment growth
14 through infill and redevelopment, thereby minimizing vehicle miles
15 traveled (VMT) and associated GHG emissions;
- 16 b. Improving public health and safety outcomes through enhanced
17 walkability;
- 18 c. Improving pedestrian and bicycle access to the coastline;
- 19 d. Encouraging energy efficiency and renewable energy in new development;
- 20 e. Enhancing visual quality through paving upgrades, landscaping, and high-
21 quality architecture and site design; and
- 22 f. Promoting economic development through synergies between commercial
23 and residential land uses.

24
25 5. Notice is HEREBY GIVEN that the time within which judicial review must be
26 sought on this decision is governed by Public Resources Code Section 21167(c).

27 //

28 //

1 PASSED and ADOPTED by the City Council of the City of Oceanside,
2 California this 14th day of August, 2019, by the following vote:

3 AYES:

4 NAYES:

5 ABSENT:

6 ABSTAIN:

7
8
9 _____
Mayor of the City of Oceanside

10
11 ATTEST:

APPROVED AS TO FORM:

12
13 _____
City Clerk

14 _____
City Attorney

EXHIBIT A

Coast Highway Corridor Final Environmental Impact Report

Mitigation, Monitoring, and Reporting Program



Mitigation Monitoring and Reporting Program for the Oceanside Coast Highway Corridor Study (SCH No. 2016051078)

Introduction

The California Public Resources Code, Section 21081.6, requires that a lead or responsible agency adopt a mitigation monitoring plan when approving or carrying out a project when an Environmental Impact Report (EIR) identifies measures to reduce potential adverse environmental impacts. As lead agency for the project, the City of Oceanside (City) is responsible for adoption and implementation of the Mitigation Monitoring and Reporting Program (MMRP).

The City has prepared an EIR in conformance with Sections 15080 through 15097 of the State Guidelines for the implementation of the California Environmental Quality Act (CEQA). The purpose of the EIR is to identify any potentially significant impacts associated with the proposed project and incorporate mitigation measures into the project as necessary to eliminate the potentially significant effects of the project or to reduce the effects to a level of insignificance.

Purpose of the MMRP

The purpose of the MMRP is to ensure that the mitigation measures required by the EIR for the Coast Highway Corridor Study Project (project) are properly implemented. The City will monitor the mitigation measures required for the project. The MMRP Checklist provides a mechanism for monitoring the mitigation measures in compliance with the EIR. General guidelines for the use and implementation of the monitoring program are described below.

Mitigation Monitoring Checklist

The Mitigation Monitoring Checklist is organized by the time of implementation and by categories of environmental impacts. For each impact area, the impacts identified in the EIR are summarized, and the required mitigation measures are listed. The following items are identified for each mitigation measure to ensure the implementation of each measure: (1) party responsible for implementation of mitigation measure; (2) party responsible for monitoring and verification of implementation of mitigation measure; and (3) monitoring schedule, or the timeframe, for implementing the mitigation measure.

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
Air Quality					
<p>MM Incentive District AIR-1a: Prior to the issuance of a grading or building permit, whichever is required to be obtained first, individual development projects proposed under the Incentive District shall comply with the following land preparation, excavation, and/or demolition mitigation measures during construction activities:</p> <ul style="list-style-type: none"> • All soil excavated or graded should be sufficiently watered to prevent excessive dust. Watering should occur with complete coverage of disturbed soil areas. Watering should be a minimum of twice daily on unpaved/untreated roads and on disturbed soil areas with active operations. • All clearing, grading, earth moving and excavation activities should cease: (a) during periods of winds greater than 20 mph (averaged over 1 hour as measured by an on-site anemometer or an off-site anemometer that is representative of the construction area), if disturbed material is easily windblown, or (b) when visible dust plumes impact public roads, occupied structures, or neighboring property. • Vehicles traveling over unpaved roadways shall be limited to 15 miles per hour or less. Signs shall be posted at construction sites identifying the maximum speed limit. • All trucks hauling dirt, sand, soil, or other loose material shall be covered or maintain at least 2 feet or freeboards, in accordance with the requirements of California Vehicle Code (CVC) Section 23114. 	Project Applicants under Incentive District and Construction Contractors	Planning Division Engineering Division Water Utilities (Environmental Services)	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<ul style="list-style-type: none"> • If more than 5,000 cubic yards of fill material will be imported or exported from the site, then all haul truck access points shall be equipped with a gravel pad, rumble pad, or similar control to reduce vehicle trackout. • Adjacent streets with visible dust, dirt, sand, or soil material accumulation shall be cleaned and the accumulated material removed using street sweepers. • Stockpiles of soil or other fine loose material shall be stabilized by watering, covered with tarp, or other appropriate method to prevent wind-blown fugitive dust. • Where acceptable to the local fire department, weed control should be accomplished by mowing instead of digging, thereby, leaving the ground undisturbed and with a mulch covering. • Locate construction staging areas away from sensitive receptor areas, such as schools, to the extent practicable. • Minimize the free drop height of excavated soil during batch-drop operations (i.e., earthwork with front-end loader or backhoe) so that the generation of dust is limited to the immediate area around the truck bed or storage pile. • Install project landscaping in appropriate areas as soon as construction in an area is complete to minimize exposed soils. 					
<p>MM Incentive District AIR-1b: Prior to the issuance of a grading or building permit, whichever is required to be obtained first, individual proposed projects shall comply with the following construction equipment mitigation measures:</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Engineering Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<ul style="list-style-type: none"> Construction equipment, on-road trucks, and emission control devices shall be properly maintained and tuned in accordance with manufacturer specifications. Construction contractors shall be required to comply with California's on-road and off-road vehicle emissions regulations, including the CARB idling restrictions and the USEPA/CARB on-road and off-road diesel vehicle emissions standards, as required by 13 CCR, Sections 2485, 2025(h), and 2449. Off-road diesel-powered construction equipment greater than 50 hp (e.g., excavators, graders, dozers, scrapers, tractors, loaders, etc.) shall be outfitted with Best Available Control Technology (BACT) devices certified by CARB such as certified Level 3 Diesel Particulate Filter or equivalent. A copy of each unit's certified BACT documentation and CARB or SCAQMD operating permit shall be provided at the time of mobilization of each applicable unit of equipment. Route construction trucks away from sensitive receptor areas. Where available, use electricity from power poles rather than temporary diesel or gasoline powered generators. 					
<p>MM Incentive District AIR-1c: Construction contractors shall ensure that interior architectural coatings have a maximum of 10 grams per liter of VOC for both residential and commercial development.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Building Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>MM Incentive District AIR-2: Prior to the issuance of a building permit, individual development projects proposed under the Incentive District regulations shall comply with the following mitigation measures:</p> <p>a. Provide direct pedestrian and bicycle access from any Incentive District residential development with a density of four or more residences per acre and in any mixed-use or commercial development to the public right-of-way. Low-, medium-, and high-density Incentive District developments shall provide curbs and sidewalks on both sides of the street all public street frontages. Curbs and sidewalks shall also be provided on both sides of all internal streets, unless an equivalent or superior pedestrian path is provided within the development.</p> <p>b. For medium to high density residential, mixed-use, or commercial developments in the Incentive District area where transit services exist but no transit stop is located within one-half mile of the development site or where transit service does not exist and the development project is within a transit district's sphere of influence, development projects shall provide plans indicating locations of bus turnouts and loading areas with shelters that are acceptable to the local transit provider.</p> <p>c. Promote the expanded use of renewable fuel and low-emission vehicles by including one or both of the following project components: provide preferential parking for ultra-low emission, zero-emission, and alternative-fuel vehicles; and/or provide electric vehicle supply equipment within the development that meets or exceeds the Tier 1 standards in the current</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Building Division	X	X	X

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>2016 Title 24 and 2016 California Green Building Standards. Nothing in this measure shall supersede an individual development project's legal responsibility to meet the applicable mandatory minimum requirements of the version of the Title 24 and California Green Building Standards in effect at the time of building permit issuance.</p> <p>d. Development projects shall be required to reduce energy consumption by designing buildings that meet or exceed the Tier 1 building energy budget standards in the current 2016 Title 24 and 2016 California Green Building Standards. Nothing in this measure shall supersede an individual development project's legal responsibility to meet the applicable mandatory minimum requirements of the version of the Title 24 and California Green Building Standards in effect at the time of building permit issuance.</p> <p>e. Development projects shall be required to reduce water consumption by installing water-efficient fixtures, appliances, toilets/urinals, and landscape irrigation systems that meet or exceed the Tier standards in the current 2016 Title 24 and 2016 California Green Building Standards. Nothing in this measure shall supersede an individual development project's legal responsibility to meet the applicable mandatory minimum requirements of the version of the Title 24 and California Green Building Standards in effect at the time of building permit issuance.</p> <p>f. Development projects shall promote transportation demand management principles such as peak hour trip reduction, staggered</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
work hours, ride sharing, telecommuting, and the use of public transportation or other measures, as appropriate.					
<p>MM Incentive District AIR-3: Prior to the issuance of a grading or building permit, whichever is required first, individual development projects proposed under the Incentive District shall comply with the following requirements:</p> <p>a. Projects locating sources of TAC emissions near sensitive receptors within the advisory guideline recommendations in the CARB Air Quality and Land Use Handbook (or future adopted subsequent document) shall conduct a health risk assessment to sufficiently demonstrate that impacts would not exceed the adopted significance thresholds inclusive of project-level design features, as appropriate and feasible. The types of projects that would be required to comply with this measure and more detail on the required features and recommendations are provided in Table 9 (CARB Recommendations on Siting and New Sensitive Land Uses).</p> <p>b. Projects requiring the use of diesel-fueled heavy-duty construction equipment that generates on-site emissions of one (1) pound per day of diesel particulate matter or more for a period of 6 months or more within 500 feet of sensitive receptors shall conduct a health risk assessment to sufficiently demonstrate that impacts would not exceed the adopted significance thresholds inclusive of project-level design features, as appropriate and feasible.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Engineering Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
Biological Resources					
<p>MM Complete Streets BIO-1: Tree removal shall take place outside of the migratory bird breeding season (February 15 through August 31). If avoidance is not feasible and tree removal is required during the avian breeding season, the following measures shall be followed:</p> <p>a. A nesting bird survey of trees planned for removal and within 300 feet of construction activities shall be conducted by a qualified avian biologist no more than 1 week prior to commencement of tree removal activities. A qualified avian biologist refers to a person with the ability to identify birds present in San Diego County to the species level by sight or sound and who is familiar with the breeding and nesting behaviors of native bird species.</p> <p>b. If active nests with eggs or chicks of bird species protected under the MBTA are detected within trees or shrubs planned for removal, the trees will remain in place until it has been determined by the avian biologist that the nest is no longer active. If active nests are detected within 300 feet of physical construction activities, an appropriate buffer shall be determined by the avian biologist and no work shall take place within the buffer until it is determined that the nest is no longer active. Additional visits after the initial survey shall be conducted as necessary to determine that nests are no longer active.</p>	<p>Public Works Department</p> <p>Engineering Division</p> <p>Water Utilities (Environmental Services)</p>	<p>Planning Division</p> <p>Engineering Division</p> <p>Water Utilities (Environmental Services)</p>	X	X	
<p>MM Complete Streets BIO-2: For physical construction activities occurring less than 300 feet from potential light-footed Ridgeway's rail habitat associated with Buena Vista Lagoon (activities south of 33.169759°, -117.357623°, including the</p>	<p>City of Oceanside, Department of Public Works</p>	<p>Planning Division</p>	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
activities planned near the Buena Vista Audubon Society building), focused protocol surveys shall be conducted by a permitted biologist. If no rails are detected, construction may commence. If rails are detected, consultation with the U.S. Fish and Wildlife Service (USFWS) would be required and may include non-disturbance areas within 300 feet of territories, implementation of noise attenuation measures, and/or daily biological monitoring and daily noise monitoring during the course of construction activities to confirm that construction activities are not adversely impacting nesting or foraging activities.					
MM Complete Streets BIO-3: This mitigation measure shall be required if removal of palm trees is proposed as part of the Complete Streets project. To avoid impacts to western yellow bats, a qualified biologist (a biologist with the ability to identify bat guano and assess habitat suitability for western yellow bats) shall inspect the base of palm skirts for guano prior to removal of skirted palm trees (i.e., palm trees with several layers of accumulated dead fronds). If bats are detected, tree removal shall avoid the yellow bat maternity season (June 1 through August 31). If tree removal cannot avoid the maternity season, bat protection protocols shall be identified and implemented by a qualified bat biologist and approved by CDFW. The protocols may require installation of bat exclusionary devices, followed by up to 4 weeks of nightly monitoring by a qualified biologist to confirm bats are being excluded without harm until it is determined bats are no longer present. The protocols may also require construction of substitute bat habitat (i.e., bat boxes, artificial tree structures) in the vicinity of bat-occupied palm	Public Works Department	Planning Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
trees, followed by monitoring by a qualified biologist to confirm bats are using the bat habitat.					
<p>MM Complete Streets BIO-4: To avoid indirect impacts to riparian habitats and sensitive natural communities adjacent to the San Luis Rey River, Loma Alta Creek, and Buena Vista Lagoon, the following measures shall be implemented:</p> <p>a. Species with a rating of moderate or high on the California Invasive Plant Council Inventory Database shall not be used for streetscaping in the Complete Streets project components.</p> <p>b. In areas with potential for erosion or construction-generated runoff, sedimentation, or dust from construction activities to impact adjacent Habitat Group A through E communities, best management practices (BMPs), such as silt fencing and/or straw wattles, shall be installed on the downslope portion of grading or disturbance areas during project construction activities. This measure applies to Complete Streets improvements south of Eaton Street and adjacent to Loma Alta Creek.</p>	<p>Public Works Department</p> <p>Engineering Division</p> <p>Water Utilities (Environmental Services)</p>	<p>Planning Division</p> <p>Engineering Division</p> <p>Water Utilities (Environmental Services)</p>	X	X	
<p>MM Incentive District BIO-1: If tree removal is required for a project proposed under the Incentive District, tree removal and construction activities shall take place outside of the migratory bird breeding season (February 15 through August 31). If avoidance is not feasible and tree removal is required during the avian breeding season, the following measures shall be followed:</p> <p>a. A nesting bird survey of trees planned for removal and within 300 feet of construction activities shall be conducted by a qualified avian biologist no more than 1 week prior to commencement of tree removal activities. A qualified avian biologist refers to a person with the</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>ability to identify birds present in San Diego County to the species level by sight or sound and who is familiar with the breeding and nesting behaviors of native bird species.</p> <p>b. If active nests with eggs or chicks of bird species protected under the MBTA are detected within trees or shrubs planned for removal, the trees will remain in place until it has been determined by the avian biologist that the nest is no longer active. If active nests are detected within 300 feet of physical construction activities, an appropriate buffer shall be determined by the avian biologist and no work shall take place within the buffer until it is determined that the nest is no longer active. Additional visits after the initial survey shall be conducted as necessary to determine that nests are no longer active.</p>					
<p>MM Incentive District BIO-2: For development activities occurring less than 300 feet from potential light-footed Ridgeway's rail habitat associated with Buena Vista Lagoon (development southwest of the intersection of Eaton Street and South Coast Highway), focused protocol surveys shall be conducted by a permitted biologist. If no rails are detected, construction may commence. If rails are detected, consultation with the USFWS would be required and may include non-disturbance areas within 300 feet of territories, implementation of noise attenuation measures, and/or daily biological monitoring and daily noise monitoring during the course of construction activities to confirm that construction activities are not adversely impacting nesting or foraging activities.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>MM Incentive District BIO-3: This mitigation measure shall be required if removal of palm trees (which may contain western yellow bats) is proposed as part of a project proposed under the Incentive District. To avoid impacts to western yellow bats, a qualified biologist (a biologist with the ability to identify bat guano and assess habitat suitability for western yellow bats.) shall inspect the base of palm skirts for guano prior to removal of skirted palm trees (i.e., palm trees with several layers of accumulated dead fronds). If bats are detected, tree removal shall avoid the yellow bat maternity season (June 1 through August 31). If tree removal cannot avoid the maternity season, project-specific bat mitigation protocols shall be identified and implemented by a qualified bat biologist and approved by CDFW. The protocols may require installation of bat exclusionary devices, followed by up to 4 weeks of nightly monitoring by a qualified biologist to confirm bats are being excluded without harm until it is determined bats are no longer present. The protocols may also require construction of substitute bat habitat (i.e., bat boxes, artificial tree structures) in the vicinity of bat-occupied palm trees, followed by monitoring by a qualified biologist to confirm bats are using the bat habitat.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division	X	X	
<p>MM Incentive District BIO-4: To avoid impacts to narrow endemic rare plants, including Nutall's lotus, Coulter's saltbush, smooth tarplant, Orcutt's pincushion, Blochman's dudleya, cliff spurge, San Diego barrel cactus, decumbent goldenbush, sea dahlia, and spreading navarretia that may occur within the Incentive District, a qualified rare plant biologist shall conduct a preconstruction rare plant survey in areas with potential habitat for rare plants.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
including in areas that are considered disturbed. Qualified rare plant biologist refers to a person with knowledge of these species (appropriate plant survey windows and species identification). The qualified rare plant biologist shall work with the City to identify project-specific measures that are consistent with the specifications of the Multiple Habitat Conservation Program and these measures shall be implemented prior to and concurrent with project construction, as applicable. Measures may include salvage of rare plants prior to construction, transfer of salvaged plants within similar habitat in non-impacted areas, followed up with monitoring by a qualified biologist to confirm at least 80% survival of salvaged plants.					
<p>MM Incentive District BIO-5: To avoid indirect and direct impacts to riparian habitats and sensitive natural communities near the San Luis Rey River, Loma Alta Creek, and Buena Vista Lagoon, the following measures shall be implemented:</p> <p>a. For non-developed areas southwest of the intersection of Eaton Street and South Coast Highway, immediately north of Loma Alta Creek and along the railroad tracks, the following measures shall be implemented to protect sensitive riparian or upland vegetation communities.</p> <p>i. A site-specific assessment of biological resources by qualified biologist shall be conducted to confirm the absence or presence of sensitive biological resources prior to the City's approval of project plans. The qualified biologist shall determine the site-specific habitat type.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>ii. If the vegetation communities outlined in Table 3.3-1 would not be directly impacted by the proposed development project, no further assessment would be required.</p> <p>iii. If there is potential for riparian, wetland, and/or sensitive upland communities to be impacted, these impacts would be required to be compensated according to vegetation community type at the ratios provided in Table 3.3-1 which supports the Multiple Habitat Conservation Program policy for no net loss of wetland/riparian vegetation and incorporates the mitigation ratios implemented in the City Subarea Plan. For impacts to these riparian and upland areas, a restoration/revegetation plan shall be prepared by a qualified restoration ecologist (experienced with riparian and upland restoration/revegetation planning) in coordination with the City and implemented by an experienced restoration contractor, with oversight by the City.</p> <p>b. The City shall prohibit the use of species with a rating of moderate or high on the California Invasive Plant Council Inventory Database in landscape plans used for development southwest of the corner of Eaton Street and South Coast Highway that is adjacent to undeveloped habitat.</p> <p>c. In areas where there is potential for erosion or construction-generated runoff, sedimentation, or dust from construction activities to impact adjacent Habitat Group A through E communities, best management practices (BMPs), such as silt fencing and/or straw wattles, shall be installed on the downslope portion of grading or disturbance areas during project construction activities. This measure applies to development southwest of</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
intersection of Eaton Street and South Coast Highway and adjacent to Loma Alta Creek.					
<p>MM Incentive District BIO-6: Individual development projects implemented under the Incentive District that would impact the areas southwest of the intersection of Eaton Street and South Coast Highway or adjacent to or within Loma Alta Creek may include jurisdictional wetlands or waters and shall be subject to a site-specific assessment of biological resources prior to the City's approval of project plans. If it is determined through the site-specific assessment that excavation, fill, or other modification of wetlands and waters under the jurisdiction of the United States Army Corps of Engineers (USACE), Regional Water Quality Control Board, and California Department of Fish and Wildlife would occur as a result of the project, the project proponent shall be required to conduct a formal jurisdictional delineation in accordance with the U.S. Army Corps of Engineers Wetland Delineation Manual (Environmental Laboratory 1987), and Regional Supplement to the Corps of Engineers Wetlands Delineation Manual: Arid West Region (Version 2.0) (USACE 2008). Permits from the respective regulatory agencies shall also be required, and will likely require mitigation resulting in no net loss of jurisdictional wetlands and waters. It is intended that implementation of the mitigation required through the project permits be consistent and meet the Multiple Habitat Conservation Program goal of no net loss of jurisdictional wetlands and waters.</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division: Applicable Wildlife Agencies	X		

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
Cultural Resources					
MM Complete Streets CR-1: Prior to the issuance of a grading permit, the City of Oceanside shall enter into a pre-excavation agreement with a representative of the San Luis Rey Band of Mission Indians, otherwise known as a Tribal Cultural Resources Treatment and Tribal Monitoring Agreement. A copy of the agreement shall be included in the grading plan submittals for the grading permit. The purpose of this agreement shall be to formalize protocols and procedures between the Applicant/Owner and the San Luis Rey Band for the protection and treatment of, including but not limited to, Native American human remains, funerary objects, cultural and religious landscapes, ceremonial items, traditional gathering areas and cultural items, located and/or discovered through a monitoring program in conjunction with the construction of the Complete Streets improvements, including additional archaeological surveys and/or studies, excavations, geotechnical investigations, grading, and all other ground-disturbing activities, such as the installation and/or removal of infrastructure and existing foundations, that may impact the native soils subsurface to the existing road bed.	Engineering Division	Planning Division Engineering Division	X		
MM Complete Streets CR-2: Prior to the issuance of a grading permit, the grading contractor shall provide a written and signed letter to the City Planner stating that a qualified archaeologist and Luiseño Native American Monitor have been retained at the grading contractor's expense to implement the monitoring program, as described in the pre-excavation agreement.	Engineering Division City's Construction Contractor	Planning Division Engineering Division	X		

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
MM Complete Streets CR-3: Prior to the release of the grading bond, a monitoring report and/or evaluation report, if appropriate, which describes the results, analysis and conclusions of the archaeological monitoring program (e.g., data recovery plan) shall be submitted by the qualified archaeologist, along with the Luiseño Native American monitor's notes and comments, to the City Planner for approval.	Engineering Division City-retained qualified archaeologist and Luiseño Native American monitor	Planning Division Engineering Division	X		
MM Complete Streets CR-4: The qualified archaeologist shall maintain ongoing collaborative consultation with the Luiseño Native American monitor during all ground-disturbing activities that may impact subsurface native soils. The requirement for the monitoring program shall be noted on all applicable construction documents, including demolition plans, grading plans, etc. The grading contractor shall notify the City Planner of the start and end of all ground-disturbing activities.	Engineering Division City-retained qualified archaeologist and Luiseño Native American monitor in cooperation with Contractor	Planning Division Engineering Division		X	
MM Complete Streets CR-5: The qualified archaeologist and Luiseño Native American Monitor shall attend all applicable pre-construction meetings with the general contractor and/or associated subcontractors to present the archaeological monitoring program. The qualified archaeologist and Luiseño Native American Monitor shall be present on-site during any ground-disturbing activities that may impact subsurface native soils.	Engineering Division City-retained qualified archaeologist and Luiseño Native American monitor	Planning Division Engineering Division	X	X	
MM Complete Streets CR-6: The qualified archaeologist or the Luiseño Native American monitor may halt ground-disturbing activities if unknown archaeological artifact deposits or cultural features are discovered. Ground-disturbing activities shall be directed away from these	Engineering Division City-retained qualified archaeologist and Luiseño Native American monitor	Planning Division Engineering Division		X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>deposits to allow a determination of potential importance. Isolates and clearly non-significant deposits will be minimally documented in the field, and before grading proceeds these items shall be given to the San Luis Rey Band so that they may be repatriated at the site on a later date. If a determination is made that the unearthed artifact deposits or cultural features are considered potentially significant, the San Luis Rey Band shall be notified and consulted with in regards to the respectful and dignified treatment of those resources.</p> <p>The avoidance and protection of the significant cultural resource and/or unique archaeological resource is the preferable mitigation. If, however, a data recovery plan is authorized by the City as the Lead Agency under CEQA, the San Luis Rey Band shall be notified and consulted regarding the drafting and finalization of any such recovery plan. For significant artifact deposits or cultural features that are part of a data recovery plan, an adequate artifact sample to address research avenues previously identified for sites in the project area will be collected using professional archaeological collection methods. If the qualified archaeologist collects such resources, the Luiseño Native American monitor must be present during any testing or cataloging of those resources. Moreover, if the qualified archaeologist does not collect the cultural resources that are unearthed during the ground-disturbing activities, the Luiseño Native American monitor, may at their discretion, collect said resources and provide them to the San Luis Rey Band for respectful and dignified treatment in accordance with the Tribe's cultural and spiritual traditions.</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>MM Complete Streets CR-7: Any and all uncovered tribal cultural resources of Native American importance shall be returned to the San Luis Rey Band of Mission Indians, and/or the Most Likely Descendant, if applicable, and not be curated.</p>	Public Works Department	The San Luis Rey Band of Mission Indians		X	X
<p>MM Complete Streets CR-8: As specified by California Health and Safety Code Section 7050.5, if human remains are found in the project area during construction or during archaeological work, the person responsible for the excavation, or his or her authorized representative, shall immediately notify the San Diego County Coroner's office by telephone. No further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains shall occur until the Coroner has made the necessary findings as to origin and disposition pursuant to PRC 5097.98. If such a discovery occurs, a temporary construction exclusion zone shall be established, surrounding the area of the discovery so that the area would be protected, and consultation and treatment could occur as prescribed by law. By law, the Coroner will determine within two working days of being notified if the remains are subject to his or her authority. If the Coroner recognizes the remains to be Native American, he or she shall contact the NAHC within 24 hours. The NAHC will make a determination as to the Most Likely Descendant. If Native American remains are discovered, the remains shall be kept in situ, or in a secure location in close proximity to where they were found, and the analysis of the remains shall only occur on-site in the presence of a Luiseño Native American monitor.</p>	<p>Engineering Division</p> <p>City-retained qualified archaeologist and Luiseño Native American monitor</p>	<p>Planning Division</p> <p>Engineering Division</p>		X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>MM Complete Streets CR-9: The qualified archeologist, or an archeologist working under the direction of the qualified archeologist, and the Luiseño Native American monitor shall conduct pre-construction cultural resources sensitivity training to inform construction personnel of the types of cultural resources that may be encountered, and of the proper procedures to be enacted in the event of an inadvertent discovery of archaeological resources or human remains. The applicant shall ensure that construction personnel are made available for and attend the training and shall retain documentation demonstrating attendance.</p>	<p>Engineering Division City-retained qualified archeologist and Luiseño Native American monitor</p>	<p>Planning Division Engineering Division</p>	X		
<p>MM Incentive District CR-1: Individual development projects implemented under the Incentive District shall be subject to a Phase I cultural resources inventory (cultural resources inventory) prior to the City's approval of project plans. This requirement shall be implemented for all projects for which the Incentive District is employed (Administrative Approval, Development Plan Review, and Conditional Use Permit processing requirements as specified in Section 3901 of the Coast Highway Incentive District). The cultural resources inventory would consist of: a cultural resources records search to be conducted at the South Coastal Information Center; scoping with the California Native American Heritage Commission (NAHC); a pedestrian archaeological survey if visible ground surface is present; and recordation of all identified archaeological resources on California Department of Parks and Recreation 523 forms. The cultural resources inventory shall be carried out by a qualified archeologist, defined as an archeologist meeting</p>	<p>Project Applicants under Incentive District and retained archeologists and Native American monitors</p>	<p>Planning Division</p>	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>the Secretary of the Interior's Standards for professional archaeology, and shall be conducted in consultation with the appropriate Native American groups as identified through outreach to the NAHC and through consultation.</p> <p>If potentially significant cultural resources are encountered during the survey, and if the project has the potential to impact those resources, the City shall require that the resources be evaluated for their eligibility for listing in the CRHR and for significance as unique archaeological resource. Recommendations shall be made for the treatment of unique archaeological resources or resources found eligible for the CRHR should the development project have the potential to adversely impact the resources. These studies shall be conducted in consultation with the City and the appropriate Native American groups as identified through consultation. Project redesign and preservation in place shall be the preferred means of mitigation to avoid impacts to significant cultural resources, including prehistoric and historic archaeological sites, locations of importance to Native Americans, human remains, historical buildings, structures and landscapes. Methods of avoidance may include, but shall not be limited to, project re-design or identification of protection measures such as capping or fencing. If it is demonstrated that resources cannot be avoided, the qualified archaeologist shall develop additional treatment measures, which may include data recovery or other appropriate measures, in consultation with the City and appropriate Native American groups as identified through consultation.</p> <p>In addition, the project proponent shall retain archaeological monitors and Native American</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>monitors during ground-disturbing activities that have the potential to impact significant cultural resources as determined by a qualified archaeologist in consultation with the City and the appropriate Native American groups.</p> <p>During project-level construction, should prehistoric or historic subsurface cultural resources be discovered, all activity in the vicinity of the find shall stop and a qualified archaeologist shall be contacted to assess the significance of the find. If any find is determined to be significant, meaning it qualifies as a unique archaeological resource or is determined eligible for the CRHR, the archaeologist shall determine, in consultation with the City and the appropriate Native American groups, suitable avoidance measures, data recovery measures, or other appropriate mitigation, such as capping.</p> <p>All significant cultural materials recovered, either prior to or during construction, shall be, as necessary and at the discretion of the consulting archaeologist and in consultation with the appropriate Native American groups, subject to scientific analysis, professional museum curation, and documentation according to current professional standards. If materials need to be recovered, protocols for proper removal and treatment shall be implemented. The specific protocols for proper removal shall be detailed in a monitoring or data recovery plan prior to recovery of the materials.</p>					
<p>MM Incentive District CR-2: Project-level development on individual properties containing structures at least 50 years old shall be subject to a historic built environment survey which will include an evaluating of the potential historic significance of the structures, prior to the City's approval of</p>	<p>Project Applicants under Incentive District and retained qualified historian or architectural historian.</p>	<p>Planning Division</p>	<p>X</p>		

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>project plans. This requirement shall be implemented for all projects on properties for which the Incentive District is employed which contain existing structures (Administrative Approval, Development Plan Review, and Conditional Use Permit processing requirements as specified in Section 3901 of the Coast Highway Incentive District). The survey shall be carried out by a qualified historian or architectural historian meeting the Secretary of the Interior's Standards for Architectural History. If potentially significant historic resources are encountered during the survey, demolition or substantial alteration of such resources identified shall be avoided, as specified by the qualified historian or architectural historian.</p>					
<p>MM Incentive District CR-3: For project-level development in the Incentive District involving ground disturbance, a qualified paleontologist shall be retained to determine the necessity of conducting a study of the project area(s) based on the potential sensitivity of the project for paleontological resources, and the potential for the project to impact paleontologically sensitive geological deposits. If deemed necessary, the paleontologist shall conduct a paleontological resources inventory designed to identify potentially significant resources. The paleontological resources inventory would consist of a paleontological resources records search to be conducted at the SDNHM; a field survey, if deemed appropriate by the paleontologist; and recordation of all identified paleontological resources. The paleontologist shall provide recommendations regarding additional work for the project. Impacts to significant paleontological resources, if identified, shall be avoided.</p>	<p>Project Applicants under Incentive District and retained qualified paleontologist</p>	<p>Planning Division</p>	<p>X</p>	<p>X</p>	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>In addition, the project proponent shall retain paleontological monitors during construction for ground-disturbing activities that have the potential to impact significant paleontological resources as determined by a qualified paleontologist.</p> <p>In the event that paleontological resources are discovered, the project proponent will notify a qualified paleontologist. The paleontologist will document the discovery as needed, evaluate the potential resource, and assess the significance of the find under the criteria set forth in CEQA Guidelines Section 15064.5. If fossil or fossil bearing deposits are discovered during construction, excavations within 50 feet of the find will be temporarily halted or diverted until the discovery is examined by a qualified paleontologist, in accordance with Society of Vertebrate Paleontology standards. The paleontologist will notify the appropriate agencies to determine procedures that would be followed before construction is allowed to resume at the location of the find. If avoidance is determined to be infeasible, the qualified paleontologist shall implement a paleontological mitigation program. At each fossil locality, field data forms shall be used to record pertinent geologic data, stratigraphic sections shall be measured, appropriate sediment samples shall be collected and submitted for analysis, and any other activities necessary for the timely and professional documentation and removal of fossils. Any fossils encountered and recovered shall be prepared to the point of identification, catalogued, and donated to a public, non-profit institution with a research interest in the materials. Accompanying notes, maps, and photographs shall also be filed at the repository.</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
Hazards and Hazardous Materials					
<p>MM Incentive District HAZ-1: To assess the status of the remediation of the contaminated sites listed above, as well as checking for any newly contaminated sites, individual project proponents for each proposed project within the Incentive District area (the applicant or its contractor) shall conduct a Phase I Environmental Site Assessment in general accordance with ASTM Standard 1527-13, Standard Practice for Environmental Site Assessments: Phase I Environmental Site Assessment Process, or later versions if any. The ASTM standard requires checking for active contaminated sites within a specified radius that have the potential to affect a given project. In the event that the extent of contamination from a site extends to a proposed project site, the applicant or its contractor for each proposed project would implement MM Incentive District HAZ-2.</p>	Project Applicants under Incentive District	Planning Division Engineering Division Fire Department Water Utilities (Environmental Services)	X		
<p>MM Incentive District HAZ-2: If the Phase I Environmental Site Assessment prepared in accordance with MM Incentive District HAZ-1 determines that contamination of a project site proposed for development is present, the following additional measures shall be required:</p> <p>a. The applicant's construction contractor(s) shall prepare and implement a site-specific Health and Safety Plan in accordance with 29 CFR 1910.120 to protect construction workers and the public during all excavation and grading activities. This plan shall be submitted to the City for review prior to commencement of construction. Note that the project applicant or its contractor would also be required to implement MM Incentive District HAZ-2b, Soil</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Engineering Division Fire Department Water Utilities (Environmental Services)	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>and Groundwater Management Plan, described further below. The Health and Safety Plan shall include, but is not limited to, the following elements:</p> <ul style="list-style-type: none"> • Designation of a trained, experienced site safety and health supervisor who has the responsibility and authority to develop and implement the site health and safety plan; • A summary of all potential risks to construction workers and maximum exposure limits for all known and reasonably foreseeable site chemicals; • Specified personal protective equipment and decontamination procedures, if needed; • Emergency procedures, including route to the nearest hospital; and • Procedures to be followed in the event that evidence of potential soil contamination (such as soil staining, noxious odors, debris or buried storage containers) is encountered. These procedures shall be in accordance with hazardous waste operations regulations and specifically include, but are not limited to, the following: immediately stopping work in the vicinity of the unknown hazardous materials release, notifying the County of San Diego Department of Environmental Health, and retaining a qualified environmental firm to perform sampling and remediation. <p>b. In support of the Health and Safety Plan described above, the applicant or its contractor shall develop and implement a Soil and Groundwater Management Plan that includes a</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
materials disposal plan specifying how the construction contractor will remove, handle, transport, and dispose of all excavated material and groundwater from dewatering activities in a safe, appropriate, and lawful manner. The plan must identify protocols for soil and groundwater testing and disposal, identify the approved disposal site, and include written documentation that the disposal site will accept the waste. Contract specifications shall mandate full compliance with all applicable local, state, and federal regulations related to the identification, transportation, and disposal of hazardous materials, including those encountered in excavated soil or groundwater.					
Noise					
<p>MM Complete Streets NOI-1: The following field techniques shall be implemented by the City's construction contractor to reduce construction-related noise at nearby noise-sensitive receptors (residential uses):</p> <p>a. Unless safety provisions require otherwise, the Complete Streets construction contractor shall adjust all audible back-up alarms to the lowest volume appropriate for safety purposes (i.e., still maintaining adequate signal-to-noise ratio for alarm effectiveness). The contractor shall consider signal persons, strobe lights, or alternative safety equipment and/or processes as allowed, for reducing reliance on high-amplitude sonic alarms.</p> <p>b. The construction contractor shall place stationary noise sources at the construction site, such as generators and air compressors, away from affected noise-sensitive receivers (residential and school uses). Non-noise-</p>	<p>Engineering Division</p> <p>City-retained Construction Contractors</p>	<p>Planning Division</p> <p>Engineering Division</p> <p>Building Division</p>	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>producing mobile equipment, such as trailers, shall be located in the direct sound pathways between suspected major noise-producing sources and sensitive receptors.</p> <p>c. Noise producing equipment (e.g., jackhammers and pavement breakers) shall use noise-attenuating shields, shrouds, or portable barriers or enclosures, to reduce operating noise.</p> <p>d. Line or cover hoppers, storage bins, and chutes shall include sound-deadening material (e.g., apply wood or rubber liners to metal bin impact surfaces).</p> <p>e. To the extent practicable and available, the construction contractor shall use construction equipment manufactured or modified to reduce noise and vibration emissions, such as: electric instead of diesel-powered equipment, hydraulic tools instead of pneumatic tools, and electric saws instead of air- or gasoline-driven saws.</p>					
<p>MM Complete Streets NOI-2: Where feasible, the City's contractor shall install temporary, field-erected noise barriers to block the line-of-site between construction equipment and sensitive receptors prior to construction (in the Complete Streets project area these are limited to residential uses). Noise barriers could include sound blankets hanging on existing fences, or the use of freestanding portable sound walls. Noise barriers should be a minimum of 8-feet in height and continuous between the source of noise and adjacent or nearby noise-sensitive receptors. Noise barriers are most effective when placed directly adjacent to either the noise source or receptor. Barrier construction may include, but not necessarily limited to, using appropriately thick</p>	<p>Engineering Division</p> <p>City-retained Construction Contractors</p>	<p>Planning Division</p> <p>Engineering Division</p> <p>Building Division</p>	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
wooden panel walls (at least one-half inch thick), which are tall enough to block the line-of-sight between the dominant construction noise source(s) and the noise-sensitive receptor. Such barriers can reduce construction noise by 5 to 15 dBA at nearby noise-sensitive receptor locations, depending on barrier height and length, and the distance between the barrier and the noise-producing equipment or activity. Alternatively, field-erected noise curtain assemblies could be installed around specific equipment sites or zones of anticipated mobile or stationary activity. These techniques are most effective and practical when the construction activity noise source is stationary (e.g., auger or drill operation) and the specific source locations of noise emissions are near the ground and can be placed as close to the equipment/activity-facing side of the noise barrier as possible. Barrier layout and other implementation details would vary by construction site.					
<p>MM Incentive District NOI-1: For development projects considered under the Incentive District provisions, a project-level vibration analysis would be required if the construction plans for the project would include the use of any of the following:</p> <ol style="list-style-type: none"> 1. Typical heavy construction equipment within 25 feet of existing inhabited structures. Typical heavy equipment is defined as equipment with an engine size of 600 horsepower or greater and includes: large dozers, large excavators, and large loaders. 2. Vibratory compaction rollers for use within 80 feet of inhabited structures. 3. Pile drivers are proposed for use within 150 feet of inhabited structures. 	Project Applicants under Incentive District and retained noise and vibration specialists	Planning Division Building Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>If none of the construction methods mentioned in the enumerated list above are proposed within the described boundaries, no further analysis would be required since the distances to sensitive receptors would create enough of a buffer to ensure impacts are less than significant.</p> <p>The purpose of each project-level vibration analysis would be to determine if the specific project-level construction would generate vibration levels exceeding the human perception threshold of 0.1 in/sec PPV at the receptor. Project specific details that would be required in each analysis would include, but not be limited to, actual construction equipment type, sizes, and horsepower to be used, specific locations of each activity, and actual distances from the activity to inhabited buildings. Vibration levels of actual equipment to be used shall be estimated from Federal Transit Administration (FTA) vibration guidance documents (FTA 2006), attenuated with distance to the inhabited structures, and compared to the Caltrans vibration threshold for human perception. If applicable, the intervening ground between equipment and structures would be considered for its soil properties for additional vibration attenuation.</p> <p>If the project-specific analysis determines that a project-specific significant impact could occur mitigation shall be required to reduce the impact to less than significant. Alternative construction methods and equipment that generate lower vibration levels shall be considered. Estimated construction vibration levels would be required to not to exceed the vibration threshold of human perception at inhabited buildings (0.1 in/sec PPV at the receptor). Field vibration measurement surveys</p>					

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>of actual construction vibration would be considered, as determined to be required by the vibration specialist, as part of construction vibration compliance with the threshold.</p> <p>This requirement shall be implemented for all projects under the Incentive District (Administrative Approval, Development Plan Review, and Conditional Use Permit processing requirements as specified in Section 3901 of the Coast Highway Incentive District).</p>					
<p>MM Incentive District NOI-2: For individual development projects proposed under the Incentive District, the following field techniques shall be implemented by the project construction contractor to reduce construction-related noise at noise-sensitive receptors within 100 feet of construction activity:</p> <p>a. Unless safety provisions require otherwise, the Incentive District construction contractor shall adjust all audible back-up alarms to the lowest volume appropriate for safety purposes (i.e., still maintaining adequate signal-to-noise ratio for alarm effectiveness). The contractor shall consider signal persons, strobe lights, or alternative safety equipment and/or processes as allowed, for reducing reliance on high-amplitude sonic alarms.</p> <p>b. The construction contractor shall place stationary noise sources at the construction site, such as generators and air compressors, as far away as possible from affected noise-sensitive receivers (residential and school uses). Non-noise-producing equipment, such as trailers, may be located as a sound barrier</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Building Division		X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>between suspected major noise-producing sources and sensitive receptors.</p> <p>c. Noise producing equipment (e.g., jackhammers and pavement breakers) shall use noise attenuating shields, shrouds, or portable barriers or enclosures, to reduce operating noise.</p> <p>d. Line or cover hoppers, storage bins, and chutes shall include sound-deadening material (e.g., apply wood or rubber liners to metal bin impact surfaces).</p> <p>e. To the extent practicable and available, the construction contractor shall use construction equipment manufactured or modified to reduce noise and vibration emissions, such as: electric instead of diesel-powered equipment, hydraulic tools instead of pneumatic tools, and electric saws instead of air- or gasoline-driven saws.</p>					
<p>MM Incentive District NOI-3: Where feasible, temporary, field-erected noise barriers to block the line-of-site between construction equipment and sensitive receptors shall be installed prior to construction of the individual development projects under the Incentive District. Noise barriers could include sound blankets hanging on existing fences, or freestanding portable sound walls. Noise barriers should be a minimum of 8-feet in height and continuous between the source of noise and adjacent or nearby noise-sensitive receptors. Noise barriers are most effective when placed directly adjacent to either the noise source or receptor. Barrier construction may include, but not necessarily limited to, using appropriately thick wooden panel walls (at least one-half inch thick).</p>	Project Applicants under Incentive District and Construction Contractors	Planning Division Engineering Division Building Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>as shown in Figure 3.10-2, which are tall enough to block the line-of-sight between the dominant construction noise source(s) and the noise-sensitive receptor. Such barriers can reduce construction noise by 5 to 15 dBA at nearby noise-sensitive receptor locations, depending on barrier height and length, and the distance between the barrier and the noise-producing equipment or activity. Alternatively, field-erected noise curtain assemblies could be installed around specific equipment sites or zones of anticipated mobile or stationary activity. These techniques are most effective and practical when the construction activity noise source is stationary (e.g., auger or drill operation) and the specific source locations of noise emissions are near the ground and can be placed as close to the equipment/activity-facing side of the noise barrier as possible. Barrier layout and other implementation details would vary by construction site.</p>					
Transportation and Traffic					
<p>MM Complete Streets TR-1: In order to mitigate the deficient LOS at the two study area intersections under the Existing + Project scenario, the City shall implement the following measures to improve intersection operations to an acceptable LOS. The City shall include the project modifications in the Complete Streets construction plans or completed prior to the finalization of the construction plans. The improvements shall be completed either prior to or concurrent with the Complete Streets improvements.</p>	Public Works Department (Traffic)	Planning Division Traffic Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
The specific measures for the two degraded study intersections in the Existing + Project scenario are in the table that follows.					

MM Complete Streets TR-1: Specific Measures and Mitigated Conditions

#	Intersection	Measure	Comment	Delay (sec/ vehicle)	LOS
27	Coast Highway & Oceanside Boulevard	Maintain Existing Traffic Signal	Merging of two lanes into one lane would occur north of Wisconsin Avenue	41.2	D
35	Coast Highway & Cassidy Street	Maintain Existing Traffic Signal	No other adjustments required	19.2	B

MM Complete Streets TR-2: In order to mitigate the deficient LOS at the eight degraded study area intersections predicted under the Future + Project scenario, the City shall implement the following measures to improve intersection operations to an acceptable LOS. The City shall include the project modifications in the Complete Streets construction plans prior to the finalization of the construction plans. The improvements shall be completed either prior to or concurrent with the Complete Streets improvements.	Public Works Department (Traffic)	Planning Division Traffic Division	X	X	
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MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
The nine mitigation measures for the eight degraded study intersections in the Future Conditions + Project scenario are in the summary table that follows. Note that the Oceanside Boulevard and I-5 SB On-/Off-Ramps intersection has two specific measures to address both the AM and PM peak hours.					

MM Complete Streets TR-2: Specific Measures and Mitigated Conditions

#	Intersection	Measure	Comment	Delay (sec/ vehicle)	LOS
4	Coast Hwy & Surfider Way	Maintain Existing Traffic Signal	None	19.6	B
6	Coast Hwy & Pier View Way	Maintain Existing Traffic Signal	None	8.7	A
15	Seagaze St & Ditmar St	Convert AWSC to Traffic Signal	None	13.2	B
27	Coast Hwy & Oceanside Blvd	Maintain Signal	None	47.4	D
29	Coast Hwy & Morse St	Maintain existing Traffic Signal	None	25.9	C
35	Coast Hwy & Cassidy Street	Maintain existing Traffic Signal	Implementation of this mitigation measure will not fully mitigate the project's impacts to this intersection	66.4	E
42	Vista Way & Ditmar St	Convert SSSC to Traffic Signal	None	11.5	B
52 (AM Peak Hour)	Oceanside Blvd & I-5 SB On-/Off-Ramps	Southbound configuration will include two left turn lanes and a shared thru-right lane with a storage length of 100 feet	None	33.9	C
52 (PM Peak Hour)	Oceanside Blvd & I-5 SB On-/Off-Ramps	Southbound configuration will include two left turn lanes and a shared thru-right lane with a storage length of 100 feet	Implementation of this mitigation measure will not fully mitigate the project's impacts to this intersection	44.2	D

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Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
<p>MM Complete Streets TR-3: Prior to the start of construction of the Complete Streets improvements, the City shall require the construction contractor to prepare a Traffic Control Plan. The Traffic Control Plan will show all signage, striping, delineated detours, flagging operations and any other devices that will be used during construction to guide motorists safely through the construction area and allow for adequate access and circulation to the satisfaction of the City. The Traffic Control Plan will be prepared in accordance with the City's traffic control guidelines and will be prepared to ensure that access will be maintained to individual properties, and that emergency access will not be restricted. The Traffic Control Plan will ensure that congestion and traffic delay are not substantially increased as a result of the construction activities. In addition, the City shall provide written notice at least 2 weeks prior to the start of construction to owners/occupants along streets to be affected during construction.</p> <p>During construction, the City will maintain continuous vehicular and pedestrian access to residential driveways from the public street to the private property line, except where necessary construction precludes such continuous access for reasonable periods of time. Access will be reestablished at the end of the workday. If a driveway needs to be closed or interfered with as described above, the City shall notify the owner or occupant of the closure of the driveway at least five working days prior to the closure. The Traffic Control Plan shall include provisions to ensure that the construction of the Complete Streets improvements does not interfere unnecessarily with the work of other agencies such as emergency</p>	Public Works Department (Traffic)	Planning Division Traffic Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
service providers, mail delivery, school buses, and municipal waste services.					
<p>MM Incentive District TR-1: Prior to submittal of grading plans for development and redevelopment projects under the Incentive District that would result in temporary interferences along roadways within the project area, project applicants and/or private developers shall prepare a Traffic Control Plan for approval by the City Transportation Division. The Traffic Control Plan will show all signage, striping, delineated detours, flagging operations and any other devices that will be used during construction to guide motorists safely through the construction area and allow for adequate access and circulation to the satisfaction of the City. The Traffic Control Plan will be prepared in accordance with the City's traffic control guidelines and will be prepared to ensure that access will be maintained to individual properties, and that emergency access will not be restricted. The Traffic Control Plan will ensure that congestion and traffic delay are not substantially increased as a result of the construction activities. In addition, the project applicants and/or private developers shall provide written notice at least 2 weeks prior to the start of construction to owners/occupants along streets to be affected during construction.</p> <p>During construction, continuous vehicular and pedestrian access to residential driveways from the public street to the private property line will be maintained, except where necessary construction precludes such continuous access for reasonable periods of time. Access will be reestablished at the end of the workday. If a driveway needs to be closed or interfered with as described above, the</p>	Public Works Department (Traffic)	Planning Division	X	X	

MITIGATION MONITORING AND REPORTING PROGRAM, OCEANSIDE COAST HIGHWAY, COMPLETE STREETS AND INCENTIVE DISTRICT

Mitigation Measures	Responsible Party	Verification Responsibility	Monitoring Schedule		
			Before Construction	During Construction	After Construction
project applicants and/or private developers shall notify the owner or occupant of the closure of the driveway at least 5 working days prior to the closure. The Traffic Control Plan shall include provisions to ensure that the construction does not interfere unnecessarily with the work of other agencies such as emergency service providers, mail delivery, school buses, and municipal waste services					

EXHIBIT B

Coast Highway Corridor Final Environmental Impact Report

Findings of Fact

Findings of Fact for the Oceanside Coast Highway Corridor Study (SCH No. 2016051078)

I. Introduction

The Findings of Fact (Findings) presented herein address the environmental effects associated with the Coast Highway Corridor Study Project (hereinafter referred to as the project) that are described and analyzed within the Final Environmental Impact Report (EIR) (the Comprehensive EIR contained in Volume 3 of the Final EIR is collectively comprised of the Draft EIR and the Partially Recirculated Draft EIR). These Findings have been made pursuant to Public Resources Code Sections 21081 and 21081.6, as well as the California Environmental Quality Act (CEQA) Guidelines (Cal. Code Regs., tit. 14, § 15000 et seq.) Sections 15090, 15091, 15092, and 15093. Public Resources Code Section 21081 and CEQA Guidelines Section 15091 require that the City of Oceanside, as the Lead Agency for this project, prepare written findings for any identified significant environmental effects along with a brief explanation of the rationale for each finding. The possible specific findings under CEQA Guidelines Section 15091(a) are:

- (1) Changes or alterations have been required in, or incorporated into, the project which avoid or substantially lessen the significant environmental effects as identified in the Final EIR.
- (2) Such changes or alterations are within the responsibility and jurisdiction of another public agency and not the agency making the finding. Such changes have been adopted by such other agency or can and should be adopted by such other agency.
- (3) Specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the Final EIR.

In accordance with Public Resources Code Section 21081 and CEQA Guidelines Sections 15092 and 15093, whenever significant effects cannot be mitigated to below a level of significance, the City of Oceanside as the decision-making Lead Agency is required to balance, as applicable, the economic, legal, social, technological, or other benefits of the project against its significant unavoidable environmental impacts when determining whether to approve the project. If the benefits of a project outweigh the significant unavoidable adverse environmental effects, the adverse effects may be considered “acceptable,” in which case the Lead Agency must adopt a formal statement of overriding considerations. (The City of Oceanside prepared a Statement of Overriding Considerations for the project separate from these Findings of Fact, which is included as part of the project submittal package presented to the City Council for approval.) In addition, when making the findings, the Lead Agency shall also adopt a program for reporting on or monitoring the changes, which it has either required in the project or made a condition of approval to avoid or substantially lessen significant environmental effects. These measures

must be fully enforceable through permit conditions, agreements, or other measures. (CEQA Guidelines, §§ 15091(d), 15097.)

Pursuant to the requirements of CEQA, the City Council hereby makes the following environmental findings in connection with the proposed Coast Highway Corridor Study Project (SCH No. 2016051078). These findings are based upon the evidence presented in the record of these proceedings, both written and oral, the Draft EIR, the Partially Recirculated Draft EIR, and all of their content, the Comments and Responses to Comments on the EIR (collectively the Draft EIR and the Partially Recirculated Draft EIR), and staff and consultants' reports presented through the hearing process, which comprise the Final EIR.

These Findings have been organized as follows:

Section I, Introduction, provides an introduction to these Findings.

Section II, Project Objectives, provides the list of goals and objectives established for the project by the City of Oceanside.

Section III, Project Overview, provides a summary of the components proposed under the Coast Highway Corridor Study project.

Section IV, Public Participation and Record of Proceedings, provides a summary of public participation in the environmental review and an overview of the administrative record that has been developed for the project.

Section V, Mitigation Monitoring and Reporting Program, provides the findings regarding the Mitigation Monitoring and Reporting Program as required by CEQA Section CEQA Guidelines, §§ 15091(d), 15097.

Section VI, Environmental Issues Determined Not to be Affected by the Project, sets forth findings for regarding the environmental impacts which were determined during the scoping period to clearly not manifest at levels that were deemed to be significant for consideration in the EIR.

Section VII, Environmental Issues Determined to Be Less than Significant, sets forth findings regarding the environmental issues areas which were determined to result in less than significant impacts without the need for the incorporation of mitigation.

Section VIII, Findings Regarding Potentially Significant Environmental Effects that can be Reduced to Less Than Significant Through Mitigation Measures, sets forth findings regarding significant or potentially significant environmental impacts identified in the Final EIR which the City of Oceanside has determined can be lessened or reduced to a less than significant level through the imposition of mitigation measures included in the MMRP for the project.

Section IX, Findings Regarding Potentially Significant Environmental Effects that Remain Significant and Unavoidable After Mitigation, sets forth findings regarding significant environmental impacts identified in the Final EIR, which the City of Oceanside has adopted mitigation measures that will substantially lessen or reduce significant impacts, but the City of Oceanside has determined will remain significant and unavoidable after mitigation.

Section X, Findings Regarding Alternatives, sets forth findings regarding alternatives to the project, which were determined not to be implemented by the City of Oceanside.

II. Project Objectives

As set forth in the EIR, the City of Oceanside, as lead agency, has defined the following goals and objectives of the proposed project, as follows:

Goal 1: Transform Coast Highway into a “Complete Streets” that accommodates all roadway users (pedestrians, bicyclists, and automobiles).

Objectives:

- 1.1 Improve the pedestrian environment
- 1.2 Provide a continuous striped bicycle lane
- 1.3 Improve traffic flow and implement traffic calming measures to reduce traffic intrusion to adjacent neighborhoods

Goal 2: Improve safety for all roadway users.

Objectives:

- 2.1 Slow traffic speeds and improve traffic flow
- 2.2 Implement roundabouts in place of traffic signals where feasible to reduce auto and pedestrian conflicts at intersections
- 2.3 Add new, mid-block pedestrian crossing opportunities between major intersections to facilitate pedestrian crossing of the roadway

Goal 3: Facilitate implementation of the Coast Highway Vision and Strategic Plan.

Objectives:

- 3.1 Encourage redevelopment and continued investment within the Incentive District by providing development incentives in exchange for community benefits to enhance and revitalize the project area
- 3.2 Increase on-street parking supply corridor-wide to support new land uses
- 3.3 Foster a built environment along Coast Highway that includes:
 - Streets and spaces that are pedestrian-scale and pleasurable to walk within
 - Architecture that announces gateways, key intersections, and public spaces
 - A consistent street frontage throughout the nodes
 - Building architecture that is high quality and provides variation and diversity

III. Project Overview

The Coast Highway Corridor Study Project would modify an approximate 3.5-mile-long segment of the Coast Highway corridor that runs through the city of Oceanside as well as create new development guidelines and regulations to encourage redevelopment and revitalization of the area. The proposed project consists of two components, the Complete Streets improvements and the Coast Highway Incentive

District (herein referred to as the Incentive District). When referring specifically to each project component, the terms “Complete Streets improvements” and “Incentive District” are used to describe the individual component. When referring to the area which encompasses both the Complete Streets improvements and the Incentive District, the term “project area” is used.

The Complete Streets improvements include proposed modifications to the Coast Highway corridor and roadway, including lane conversions, street improvements, intersection roundabouts, and increased parking and bicycle facilities. The 3.5-mile stretch of Coast Highway currently operates with four travel lanes, two northbound and two southbound, with limited on-street parking and no designated bicycle facilities. Implementation of the proposed project would improve infrastructure for all modes of transportation, including bicycle, pedestrian, and transit services, while also accommodating forecast future traffic volumes within the corridor. Specifically, the Complete Streets improvements would convert Coast Highway from four lanes to two lanes (one travel lane in each direction) for the length of the corridor, with segments of two southbound travel lanes between State Route (SR) 76 and Surfrider Way, and south of Kelly Street to Eaton Street.

Key elements of the Complete Streets improvements include a continuous Class II striped bicycle lane from Harbor Drive to the southern city limit, 10 mid-block crosswalks to facilitate safe and convenient pedestrian crossings of the corridor, 12 roundabouts in place of traffic signals where physically feasible and where the intersection traffic volumes support implementation, traffic-calming measures, and streetscape enhancements, such as removing dead trees and replanting trees. The 12 roundabouts would include dedicated, setback pedestrian crosswalks along all roadways leading into the roundabout. In combination with the 10 mid-block crosswalks, the proposed project would result in 22 new pedestrian crosswalks along Coast Highway, which would increase pedestrian safety and allow for greater access to the coastal area. These enhancements to the landscaping and roadway would help implement the vision of the corridor established within the Coast Highway Vision and Strategic Plan (Vision Plan).

The Incentive District is an amendment to the Zoning Ordinance. If adopted, the Incentive District would be an optional zoning program that individual developers could choose to apply for new development or redevelopment within the Incentive District boundary in lieu of the existing zoning. However, if a developer or property owner does not choose to adhere to the Incentive District, then future development may still occur solely consistent with the existing zoning. The Incentive District would facilitate implementation of the Vision Plan by encouraging redevelopment and revitalization of the Coast Highway corridor.

The primary purpose of the Incentive District is to encourage redevelopment and revitalization of the Coast Highway corridor through land use regulations, design and development criteria, and development incentives that will encourage sustainable, high-quality development. The Incentive District incents development and redevelopment by offering a streamlined development review process, expanding the land uses permitted by right, reforming parking standards, and allowing increased height of buildings in certain planning areas, with discretionary approval. In addition, the Incentive District includes a Residential Density Incentive Program that allows for increased residential density for nodal development in exchange for public benefits. These benefits include providing one or more of the following: additional open space, public parking, additional commercial floor area, and payment to a Public Improvement Fee. Furthermore, the Incentive District would provide form-based design and development standards to

achieve the pedestrian-scale and architectural variation of buildings advocated in the Vision Plan. Implementation of the Incentive District would require amendments to the City of Oceanside's General Plan, Local Coastal Program (LCP), and Zoning Ordinance.

IV. Public Participation and Record of Proceedings

Public Input

The City of Oceanside has provided opportunities for public review and comment, including but not limited to the public forums set forth below:

- Draft EIR Notice of Preparation Public Comment Period: June 1, 2016 – July 1, 2016
- Notice of Preparation Scoping Meeting: June 23, 2016
- Draft EIR Public Review Period: July 13, 2017 – August 28, 2017
- Partially Recirculated Draft EIR Public Review Period: November 14, 2018 – January 14, 2019

Record of Proceedings

For purposes of CEQA compliance and these Findings and Statement of Overriding Considerations, the Record of Proceedings for the project consists of the following documents and other evidence at a minimum:

- The Notice of Preparation and all other public notices issued by the City of Oceanside in conjunction with the project;
- The Draft EIR;
- The Partially Recirculated Draft EIR;
- The Final EIR;
- All written comments and verbal public testimony presented during the public comment period on the Draft EIR and the Partially Recirculated Draft EIR or during a noticed public hearing for the project at which such testimony was taken;
- The Mitigation Monitoring and Reporting Program;
- All findings, ordinances, and resolutions recommended by the Planning Commission and adopted by the City Council in connection with the project and all documents incorporated by reference therein;
- All final reports, studies, memoranda, maps, staff reports, or other planning documents relating to the project prepared by the City of Oceanside, consultants to the City of Oceanside, or responsible or trustee agencies with respect to the City of Oceanside's compliance with the requirements of CEQA and with respect to the City of Oceanside's actions on the project;
- All documents submitted to the City of Oceanside by other public agencies or members of the public in connection with the project, through the close of the public hearing;
- Any transcript or minutes of the proceedings at which the decision-making body of the City of Oceanside heard testimony on, or considered any environmental document on the project, and any transcript or minutes of proceedings before any advisory body to the City of Oceanside that

were presented to the decision-making body prior to action on the environmental document or on the project.

- Any documentary or other evidence submitted to the City of Oceanside at such information sessions, public meetings, and public hearings;
- Matters of common knowledge to the City of Oceanside, including, but not limited to federal, state, and local laws and regulations;
- The City of Oceanside's General Plan/Local Coastal Program and Municipal Code;
- Any documents expressly cited in these findings in addition to those cited above, including the Coast Highway Vision and Strategic Plan; and,
- Any other materials required to be in the record of proceedings by Section 21167.6(e) of CEQA.

The custodian of the documents comprising the record of proceedings is the City Clerk, whose office is located at 300 North Coast Highway, Oceanside, CA 92054. Copies of all of these documents, which constitute the record of proceedings upon which the City of Oceanside's decision is based, are and at all relevant times have been available upon request at the offices of the City of Oceanside.

V. Mitigation Monitoring and Reporting Program

CEQA requires the lead agency approving a project to adopt a Mitigation Monitoring and Reporting Program for the changes to the project that it had adopted or made a condition of project approval in order to ensure compliance with project implementation. (CEQA Guidelines, §§ 15091(d), 15097.) A Mitigation Monitoring and Reporting Program has been prepared for the project and has been adopted concurrently with these Findings. (See Pub. Res. Code §21081.6 (a) (1).) The Mitigation Monitoring and Reporting Program designates responsibility and anticipated timing for the implementation of mitigation measures that are to be implemented for the project. The City of Oceanside will track compliance with project mitigation measures. The City of Oceanside has prepared the Mitigation Monitoring and Reporting Program for the project separate from these Findings of Fact, which is included as part of the project submittal package presented to the City Council for approval. The City of Oceanside finds that the Mitigation Monitoring and Reporting Program meets the requirements of Public Resources Code Section 21081.6 by providing for the implementation and monitoring of Project conditions intended to mitigate potential environmental effects of the Project.

VI. Environmental Issues Determined Not to be Affected by the Project

Based on the public's responses to the project's Notice of Preparation, the environmental issue areas of agricultural resources and mineral resources were determined by the City of Oceanside to be inapplicable to the project due to the nature of the project and the absence of any potential impact related to agricultural or mineral resources. No substantial evidence has been presented to or identified by the City of Oceanside which would modify or otherwise alter the City of Oceanside's no impact determination for the agricultural resources and mineral resources environmental issue areas.

VII. Environmental Issues Determined to Be Less than Significant

Based on the City of Oceanside's review of the Comprehensive EIR contained in Volume 3 of the Final EIR, the City of Oceanside finds that the project would result in less than significant impacts for the following thresholds of significance¹ without the need for mitigation:

A. Aesthetics

1. The project will not have a substantial adverse effect on a scenic vista.
2. The project will not substantially damage scenic resources, including but not limited to trees, rock outcroppings, and historic buildings within a state scenic highway.
3. The project will not substantially degrade the existing visual character or quality of the site and its surroundings.
4. The project will not create a new source of substantial light or glare that would adversely affect day or nighttime views in the area.
5. The project will not substantially contribute to a cumulatively considerable impact related to aesthetics.

B. Air Quality

1. The project will not conflict with or obstruct implementation of the applicable air quality plan.
5. The project will not create objectionable odors affecting a substantial number of people.

C. Biological Resources

6. The project will not conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance.
7. The project will not substantially contribute to a cumulatively considerable impact related to biological resources.

D. Cultural Resources

6. The project will not substantially contribute to a cumulatively considerable impact related to biological resources.

E. Geology, Soils, and Seismicity

1. The project will not expose people or structures to potential substantial adverse effects, including risk of loss, injury, or death involving:

¹ Numbering of the thresholds of significance correspond to the numbering used in the Comprehensive EIR contained in Volume 3 of the Final EIR.

- i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault.
 - ii. Strong seismic ground-shaking.
 - iii. Seismic-related ground failure, including liquefaction.
 - iv. Landslides.
2. The project will not result in substantial soil erosion or the loss of topsoil.
3. The project will not be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in onsite or offsite landslides, lateral spreading, subsidence, liquefaction, or collapse.
4. The project will not be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994)², creating substantial risks to life or property.
5. The project will not have soils incapable of adequately supporting septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater
6. The project will not substantially contribute to a cumulatively considerable impact related to geology, soils, and seismicity.

F. Greenhouse Gas Emissions

2. The project will not conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gas.

G. Hazards and Hazardous Materials

1. The project will not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials.
2. The project will not create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.
3. The project will not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within 0.25 mile of an existing or proposed school.
5. Because the project is not located within an area covered by an airport land use plan or, where such a plan has not been adopted, or within 2 miles of a public airport or public use airport, the project will not result in a safety hazard for people residing or working in the project area.
6. Because the project is not located within the vicinity of a private airstrip, the project will not result in a safety hazard for people residing or working in the project area.

² The California Building Code, based on the International Building Code and the now defunct Uniform Building Code, no longer includes a Table 18-1-B. Instead, Section 1803.5.3 of the California Building Code describes the criteria for analyzing expansive soils.

7. The project will not expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands.
8. The project will not substantially contribute to a cumulatively considerable impact related to hazards and hazardous materials.

H. Hydrology and Water Quality

1. The project will not violate water quality standards or waste discharge requirements.
2. The project will not substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been approved).
3. The project will not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion, siltation on- or offsite.
4. The project will not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site.
5. The project will not create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff.
6. The project will not otherwise substantially degrade water quality.
7. The project will not place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map.
8. The project will not place structures within a 100-year flood hazard area which would impede or redirect flood flows.
9. The project will not expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam.
10. The project will not result in inundation by seiche, tsunami or mudflow.
11. The project will not substantially contribute to a cumulatively considerable impact related to hydrology and water quality.

I. Land Use and Planning

1. The project will not physically divide an established community.
2. The project will not conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan,

specific plan, local coastal program or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect.

3. The project will not substantially contribute to a cumulatively considerable impact related to land use and planning.

J. Noise and Vibration

1. The project will not exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies.
5. Because the project is not located within an airport land use plan area or, where such a plan has not been adopted, within two miles of a public airport or public use airport, the project will not expose people residing or working in the area to excessive noise levels.
6. Because the project is not located within the vicinity of a private airstrip, the project will not expose people residing or working in the project area to excessive noise levels.

K. Population and Housing

1. The project will not induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure) beyond the growth characterized by the project description and addressed in the technical analyses of this EIR.
2. The project will not displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere.
3. The project will not displace substantial numbers of people, necessitating the construction of replacement housing elsewhere.
4. The project will not substantially contribute to a cumulatively considerable impact related to population and housing.

L. Public Services

1. The proposed project will not result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities or the need for new or physically altered government facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for the following public services:
 - i. Fire and Police Protection
 - ii. Schools
 - iii. Libraries
2. The project will not substantially contribute to a cumulatively considerable impact related to public services.

M. Recreation and Parks

1. The project will not increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial deterioration of the facility would occur or be accelerated.
2. The project will not include recreational facilities or require the construction or expansion of recreational facilities in order to maintain performance objectives, which might have an adverse physical impact on the environment.
3. The project will not substantially contribute to a cumulatively considerable impact related to recreation and parks.

N. Transportation and Traffic

2. The project will not result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks.
3. The project will not substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).
5. The project will not conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities.

O. Utilities and Service Systems

1. The project will not exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board.
2. The project will not require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.
3. The project will not require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.
4. The project will have sufficient water supplies available to serve the project from existing entitlements and resources, and will not need new or expanded entitlements.
5. The project will result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the projects projected demand in addition to the provider's existing commitments.
6. The project will be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs.
7. The project will comply with federal, state, and local statutes and regulations related to solid waste.
8. The project will not substantially contribute to a cumulatively considerable impact related to utilities and service systems.

VIII. Findings Regarding Potentially Significant Environmental Effects that can be Reduced to Less Than Significant through Mitigation Measures

The City of Oceanside, having reviewed and considered the information contained in the Final EIR, finds pursuant to Public Resources Code §21081(a)(1) and CEQA Guidelines Section 15091(a)(1) that changes or alterations have been required in, or incorporated into, the proposed project which will mitigate, avoid, or substantially lessen to below a level of significance the following potential significant environmental effects as identified in the Comprehensive EIR contained in Volume 3 of the Final EIR.

Air Quality

Air Quality Threshold of Significance 4: Expose Sensitive Receptors to Substantial Pollutant Concentrations

Finding: Construction-related activities occurring under the Incentive District could result in the emission of toxic air contaminants (TACs) above significance thresholds, which could result in potentially significant impacts to nearby sensitive receptors. In addition, impacts related to operational TAC emissions could be considered potentially significant when considering the various development projects that could be constructed under the Incentive District.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Incentive District AIR-3: Prior to the issuance of a grading or building permit, whichever is required first, individual development projects proposed under the Incentive District shall comply with the following requirements:

- a. Projects locating sources of TAC emissions near sensitive receptors within the advisory guideline recommendations in the *CARB Air Quality and Land Use Handbook* (or future adopted subsequent document) shall conduct a health risk assessment to sufficiently demonstrate that impacts would not exceed the adopted significance thresholds inclusive of project-level design features, as appropriate and feasible. The types of projects that would be required to comply with this measure and more detail on the required features and recommendations are provided in Table 9 (CARB Recommendations on Siting and New Sensitive Land Uses).
- b. Projects requiring the use of diesel-fueled heavy-duty construction equipment that generates on-site emissions of 1 pound or more per day of diesel particulate matter for a period of 6 months or more within 500 feet of sensitive receptors shall conduct a health risk assessment to sufficiently demonstrate that impacts would not exceed the adopted significance thresholds inclusive of project-level design features, as appropriate and feasible.

Facts in Support of Findings: The primary TACs that could be emitted during construction would be diesel PM from construction equipment exhaust. Diesel PM is emitted by heavy equipment operations during grading, excavation, and transportation activities.

The construction period for the potential development and redevelopment of an individual project as result of adoption of the Incentive District would be much less than the 30-year period used for risk determination for residential exposures. Because off-road heavy-duty diesel equipment would be used only for short time periods of generally 1 to 2 years for typical development projects, project-level construction during future development projects would typically not expose sensitive receptors to substantial emissions of TACs that exceed the established significance thresholds. However, given the potential amount of development associated with implementation of the Incentive District it is reasonable to assume that on a programmatic level some large-scale construction activities that generate TAC emissions exceeding the established significance thresholds could occur near sensitive receptors, thereby potentially resulting in significant impacts.

In addition, potential development and redevelopment under the Incentive District would generally result in an increase in density in the project corridor, and it is possible that sensitive uses could be located near sources of TAC emissions within the distances specified in the CARB advisory recommendations.

Implementation of MM Incentive District AIR-3, which requires compliance with requirements established by CARB and incorporation of project-design features to reduce impacts of diesel-fueled heavy-duty construction equipment, will reduce potentially significant impacts related to operational TAC emissions associated with the construction and operation of development and redevelopment projects under the Incentive District to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.2, Air Quality, the City of Oceanside finds that implementation of MM Incentive District AIR-3 will reduce the significant project impact associated with TACs to a less than significant level.

Biological Resources

Biological Resources Threshold of Significance 1: Substantial Adverse Effects to Candidate, Sensitive, or Special-Status Species

Findings:

Complete Streets Improvements

Construction of the Complete Streets improvements has the potential to significantly impact migratory birds (including raptors) and several special-status wildlife species due to construction-related noise as well as Western yellow bats if removal of skirted palm trees is required for roundabout installation.

Incentive District

Construction and operation of development and redevelopment projects under the Incentive District have the potential to significantly impact migratory birds (including raptors) and several special-status wildlife species due to construction-related noise, pocketed free-tailed bats and western yellow bats if removal of skirted palm trees is required, and special-status plant species, especially along the rail line, north of Loma Alta Creek, and south of Vista Way.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Complete Streets BIO-1: Tree removal shall take place outside of the migratory bird breeding season (February 15 through August 31). If avoidance is not feasible and tree removal is required during the avian breeding season, the following measures shall be followed:

- a. A nesting bird survey of trees planned for removal and within 300 feet of construction activities shall be conducted by a qualified avian biologist no more than 1 week prior to commencement of tree removal activities. A qualified avian biologist refers to a person with the ability to identify birds present in San Diego County to the species level by sight or sound and who is familiar with the breeding and nesting behaviors of native bird species.
- b. If active nests with eggs or chicks of bird species protected under the MBTA are detected within trees or shrubs planned for removal, the trees will remain in place until it has been determined by the avian biologist that the nest is no longer active. If active nests are detected within 300 feet of physical construction activities, an appropriate buffer shall be determined by the avian biologist and no work shall take place within the buffer until it is determined that the nest is no longer active. Additional visits after the initial survey shall be conducted as necessary to determine that nests are no longer active.

MM Complete Streets BIO-2: For physical construction activities occurring less than 300 feet from potential light-footed Ridgeway's rail habitat associated with Buena Vista Lagoon (activities south of 33.169759°, -117.357623°, including the activities planned near the Buena Vista Audubon Society building), focused protocol surveys shall be conducted by a permitted biologist. If no rails are detected, construction may commence. If rails are detected, consultation with the USFWS would be required and may include non-disturbance areas within 300 feet of territories, implementation of noise attenuation measures, and/or daily biological monitoring and daily noise monitoring during the course of construction activities to confirm that construction activities are not adversely impacting nesting or foraging activities.

MM Complete Streets BIO-3: This mitigation measure shall be required if removal of palm trees is proposed as part of the Complete Streets project. To avoid impacts to western yellow bats, a qualified biologist (a biologist with the ability to identify bat guano and assess habitat suitability for western yellow bats) shall inspect the base of palm skirts for guano prior to removal of skirted palm trees (i.e., palm trees with several layers of accumulated dead fronds). If bats are detected, tree removal shall avoid the yellow bat maternity season (June 1 through August 31). If tree removal cannot avoid the maternity season, bat protection protocols shall be identified and implemented by a qualified bat biologist and approved by CDFW. The protocols may require installation of bat exclusionary devices, followed by up to 4 weeks of nightly monitoring by a qualified biologist to confirm bats are being excluded without harm until it is determined bats are no longer present. The protocols may also require construction of substitute bat habitat (i.e., bat boxes, artificial tree structures) in the vicinity of bat-occupied palm trees, followed by monitoring by a qualified biologist to confirm bats are using the bat habitat.

MM Incentive District BIO-1: If tree removal is required for a project proposed under the Incentive District, tree removal and construction activities shall take place outside of the migratory bird breeding season (February 15 through August 31). If avoidance is not feasible and tree removal is required during the avian breeding season, the following measures shall be followed:

- a. A nesting bird survey of trees planned for removal and within 300 feet of construction activities shall be conducted by a qualified avian biologist no more than 1 week prior to commencement of tree removal activities. A qualified avian biologist refers to a person with the ability to identify birds present in San Diego County to the species level by sight or sound and who is familiar with the breeding and nesting behaviors of native bird species.
- b. If active nests with eggs or chicks of bird species protected under the MBTA are detected within trees or shrubs planned for removal, the trees will remain in place until it has been determined by the avian biologist that the nest is no longer active. If active nests are detected within 300 feet of physical construction activities, an appropriate buffer shall be determined by the avian biologist and no work shall take place within the buffer until it is determined that the nest is no longer active. Additional visits after the initial survey shall be conducted as necessary to determine that nests are no longer active.

MM Incentive District BIO-2: For development activities occurring less than 300 feet from potential light-footed Ridgeway's rail habitat associated with Buena Vista Lagoon (development southwest of the intersection of Eaton Street and South Coast Highway), focused protocol surveys shall be conducted by a permitted biologist. If no rails are detected, construction may commence. If rails are detected, consultation with the USFWS would be required and may include non-disturbance areas within 300 feet of territories, implementation of noise attenuation measures, and/or daily biological monitoring and daily noise monitoring during the course of construction activities to confirm that construction activities are not adversely impacting nesting or foraging activities.

MM Incentive District BIO-3: This mitigation measure shall be required if removal of palm trees (which may contain western yellow bats) is proposed as part of a project proposed under the Incentive District. To avoid impacts to western yellow bats, a qualified biologist (a biologist with the ability to identify bat guano and assess habitat suitability for western yellow bats.) shall inspect the base of palm skirts for guano prior to removal of skirted palm trees (i.e., palm trees with several layers of accumulated dead fronds). If bats are detected, tree removal shall avoid the yellow bat maternity season (June 1 through August 31). If tree removal cannot avoid the maternity season, project-specific bat mitigation protocols shall be identified and implemented by a qualified bat biologist and approved by CDFW. The protocols may require installation of bat exclusionary devices, followed by up to 4 weeks of nightly monitoring by a qualified biologist to confirm bats are being excluded without harm until it is determined bats are no longer present. The protocols may also require construction of substitute bat habitat (i.e., bat boxes, artificial tree structures) in the vicinity of bat-occupied palm trees, followed by monitoring by a qualified biologist to confirm bats are using the bat habitat.

MM Incentive District BIO-4: To avoid impacts to narrow endemic rare plants, including Nutall's lotus, Coulter's saltbush, smooth tarplant, Orcutt's pincushion, Blochman's dudleya, cliff spurge, San Diego barrel cactus, decumbent goldenbush, sea dahlia, and spreading navarretia that may occur within the Incentive District, a qualified rare plant biologist shall conduct a preconstruction rare plant survey in areas with potential habitat for rare plants, including in areas that are considered disturbed. Qualified rare plant biologist refers to a person with knowledge of these species (appropriate plant survey windows and species identification). The qualified rare plant biologist shall work with the City of Oceanside to identify project-specific measures that are consistent with the specifications of the Multiple Habitat Conservation Program, and these measures shall be implemented prior to and concurrent with project construction, as applicable. Measures may include salvage of rare plants prior to construction, transfer of salvaged plants to similar habitat in non-impacted areas, followed up with monitoring by a qualified biologist to confirm at least 80 percent survival of salvaged plants.

Facts in Support of Findings:

Complete Streets Improvements

Migratory birds (including raptors) and several special-status wildlife species have the potential to occur within the Complete Streets improvements area and/or buffer, and could be impacted by the project as a result of tree removal and/or construction noise during the breeding season. Migratory birds may nest in trees located along the area planned for the Complete Streets improvements. If trees with nesting birds were to be removed, direct mortality to individuals or eggs could occur, which would be considered a significant impact. Implementation of MM Complete Streets BIO-1 and BIO-2 will require additional biological surveys and measures during construction to ensure impacts to migratory birds and several special-status wildlife species will be reduced to a less than significant level.

Western yellow bats may occur within skirted palm trees within the Complete Streets improvements area. Removal of skirted palm trees, if required for roundabout installation, may result in direct western yellow bat mortality or disturbance of maternity roosts, and would be considered a significant impact. Incorporation of MM Complete Streets BIO-3 will require additional biological surveys and protocols to ensure that impacts to western yellow bats will be reduced to a less than significant level.

Incentive District

Migratory birds (including raptors) have the potential to occur within the Incentive District area and could be impacted by future development during the breeding season. Removal of trees with nesting birds could result in direct mortality to individuals or eggs, which would be considered a significant impact. Construction noise could also result in a significant impact to breeding activities. Indirect impacts to light-footed Ridgway's rail related to noise during construction activities would occur within 300 feet or less of potential habitats for these species located at Buena Vista Lagoon. Noise above baseline levels during the breeding season at a distance of less than 300 feet would be considered a potentially significant impact to this special-status species. Implementation of MM Incentive District BIO-1 and BIO-2 will require additional biological surveys and measures during construction and/or operation to ensure impacts to migratory birds and special-status species will be reduced to a less than significant level.

Western yellow bats also have the potential to have maternity roosts within palm trees within the Incentive District and could be directly impacted by palm tree removal. Implementation of MM Incentive District BIO-3 will require additional biological surveys and protocols in the event that palm tree removal is required to ensure impacts to bat species will be reduced to a less than significant level.

Future projects implemented under the Incentive District have the potential to directly impact special-status plants where potential habitat for these species occurs within the Incentive District within the disturbed areas along the rail line, north of Loma Alta Creek, and south of Vista Way. Indirect impacts could also result from activities adjacent to habitat due to the introduction or spread of invasive species that compete with special-status plants or the generation of construction-related runoff, sedimentation, or dust that could degrade potential habitat. Implementation of MM Incentive District BIO-4 will require qualified rare plant surveys and require compliance with the specifications of the Multiple Habitat Conservation Program to ensure impacts to special-status and rare plant species are reduced to less than significant.

For the reasons discussed above and in the Final EIR in Section 3.3, Biological Resources, the City of Oceanside finds that implementation of MM Complete Streets BIO-1 through BIO-3 and MM Incentive District BIO-1 through BIO-4 will reduce the significant project impacts associated with adverse effects to candidate, sensitive, or special-status species to a less than significant level.

Biological Resources Threshold of Significance 2: Riparian Habitat and Sensitive Natural Communities

Findings:

Complete Streets

Physical construction activities that could indirectly impact riparian habitats and sensitive natural communities at Loma Alta Creek and Buena Vista Marsh include mid-block crosswalks proposed across Coast Highway adjacent to the Loma Alta Creek footpath (south of the existing Loma Alta Creek bridge) and near the Buena Vista Audubon Society driveway south of Eaton Street near Buena Vista Lagoon.

Incentive District

Construction and operational activities associated with the future development and redevelopment projects which could occur under the Incentive District could result in potentially significant impacts to riparian and sensitive natural habitats.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Complete Streets BIO-4: To avoid indirect impacts to riparian habitats and sensitive natural communities adjacent to the San Luis Rey River, Loma Alta Creek, and Buena Vista Lagoon, the following measures shall be implemented:

- a. Species with a rating of moderate or high on the California Invasive Plant Council Inventory Database shall not be used for streetscaping in the Complete Streets project components.
- b. In areas with potential for erosion or construction-generated runoff, sedimentation, or dust from construction activities to impact adjacent Habitat Group A through E communities, best management practices (BMPs), such as silt fencing and/or straw wattles, shall be installed on the downslope portion of grading or disturbance areas during project construction activities. This measure applies to Complete Streets improvements south of Eaton Street and adjacent to Loma Alta Creek.

MM Incentive District BIO-5: To avoid indirect and direct impacts to riparian habitats and sensitive natural communities near the San Luis Rey River, Loma Alta Creek, and Buena Vista Lagoon, the following measures shall be implemented:

- a. For non-developed areas southwest of the intersection of Eaton Street and South Coast Highway, immediately north of Loma Alta Creek and along the railroad tracks, the following measures shall be implemented to protect sensitive riparian or upland vegetation communities.
 - i. A site-specific assessment of biological resources by qualified biologist shall be conducted to confirm the absence or presence of sensitive biological resources prior to the City of

Oceanside’s approval of project plans. The qualified biologist shall determine the site-specific habitat type.

- ii. If the vegetation communities outlined in Table 3.3-1 would not be directly impacted by the proposed development project, no further assessment would be required.
 - iii. If there is potential for riparian, wetland, and/or sensitive upland communities to be impacted, these impacts would be required to be compensated according to vegetation community type at the ratios provided in Table 3.3-1 which supports the Multiple Habitat Conservation Program policy for no net loss of wetland/riparian vegetation and incorporates the mitigation ratios implemented in the City Subarea Plan. For impacts to these riparian and upland areas, a restoration/revegetation plan shall be prepared by a qualified restoration ecologist (experienced with riparian and upland restoration/revegetation planning) in coordination with the City of Oceanside and implemented by an experienced restoration contractor, with oversight by the City of Oceanside.
- b. The City of Oceanside shall prohibit the use of species with a rating of moderate or high on the California Invasive Plant Council Inventory Database in landscape plans used for development southwest of the corner of Eaton Street and South Coast Highway that is adjacent to undeveloped habitat.
- c. In areas where there is potential for erosion or construction-generated runoff, sedimentation, or dust from construction activities to impact adjacent Habitat Group A through E communities, best management practices (BMPs), such as silt fencing and/or straw wattles, shall be installed on the downslope portion of grading or disturbance areas during project construction activities. This measure applies to development southwest of intersection of Eaton Street and South Coast Highway and adjacent to Loma Alta Creek.

**TABLE 3.3-1
MITIGATION RATIOS FOR IMPACTS TO VEGETATION COMMUNITIES**

Vegetation Community/Land Cover Type	MHCP Habitat Group ¹	Location of Impact within Coastal Zone, Pre-approved Mitigation Area ² , or FPA	Location of Impact Outside of FPA
Riparian and Wetlands¹			
Disturbed Wetland (11200)	A	1:1 to 2:1	1:1 to 2:1
Emergent Wetland (52440)	A	4:1	4:1
Coastal Brackish Marsh (52200)	A	4:1	4:1
Coastal and Valley Freshwater Marsh (52410)	A	4:1	4:1
Brackish water Estuary (64133)	A	4:1	4:1
Non-Vegetated Floodplain or Channel (64200)	A	1:1 to 2:1	1:1 to 2:1
Non-Native Riparian (65000)	A	3:1	3:1
Uplands			
Diegan Coastal Sage Scrub (32500)	C	3:1	3:1
Flat-topped Buckwheat (32800)	D	1:1	0.5:1

¹ The wetlands mitigation ratios should provide a standard for each habitat type but may be adjusted depending on the functions and values of both the impacted wetlands as well as the wetlands mitigation proposed by the project. The City of Oceanside may also consider the types of wetland habitat being impacted and utilized for mitigation in establishing whether these standards have been met. All impacts to riparian/wetland habitats and mitigation for such impacts must be reviewed and approved by Federal and State agencies with jurisdiction over these vegetation communities.

² Pre-approved mitigation areas are depicted on Figure 3.3-1

SOURCE: SANDAG 2003; City of Oceanside 2010.

Facts in Support of Findings:

Complete Streets Improvements

Work adjacent to the San Luis Rey River, Loma Alta Creek, and Buena Vista Lagoon could result in indirect impacts to riparian habitats and sensitive natural communities by contributing to the spread of invasive species or generation of construction-related runoff, sedimentation, or dust. However, work adjacent to the San Luis Rey River will be limited to road restriping and will not require asphalt grinding or other activities that will result in creation of debris, sedimentation, or run-off. Therefore, no indirect impacts will occur to riparian habitat and other sensitive natural communities near the San Luis Rey River. Physical construction activities that could indirectly impact riparian habitats and sensitive natural communities at Loma Alta Creek and Buena Vista Marsh include mid-block crosswalks proposed across Coast Highway adjacent to the Loma Alta Creek footpath (south of the existing Loma Alta Creek bridge) and near the Buena Vista Audubon Society driveway south of Eaton Street near Buena Vista Lagoon. Incorporation of MM Complete Streets BIO-4 will require additional biological measures to be implemented for areas near riparian habitats in order to ensure impacts will be reduced to a less than significant level.

Incentive District

Future development and redevelopment which could occur under the Incentive District could result in direct impacts to riparian habitat and other sensitive natural communities through habitat removal or alteration, specifically within non-developed areas southwest of the intersection of Eaton Street and South Coast Highway, immediately north of Loma Alta Creek and along the railroad tracks. In addition, potential indirect effects, such as spread of invasive species or generation of construction-related runoff, sedimentation, or dust, may occur to adjacent vegetation communities associated with Loma Alta Creek and Buena Vista Lagoon. Incorporation of MM Incentive District BIO-5, which requires implementation of various additional biological measures specific to different biological areas, will ensure that potentially significant impacts to riparian habitat and other sensitive natural communities will be reduced to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.3, Biological Resources, the City of Oceanside finds that implementation of MM Complete Streets BIO-4 and MM Incentive District BIO-5 will reduce the significant project impacts associated with adverse effects to riparian habitat and sensitive natural communities to a less than significant level.

Biological Resources Threshold of Significance 3: Federally Protected Wetlands

Findings:

Complete Streets Improvements

No federal or state wetlands or other waters occur within the Complete Streets improvements area; therefore, no direct impacts to jurisdictional wetlands or waters will occur. Physical construction activities that could indirectly impact federal or state wetlands or other waters include mid-block crosswalks proposed across Coast Highway adjacent to the Loma Alta Creek footpath (south of the existing Loma Alta Creek bridge) and near the Buena Vista Audubon Society driveway south of Eaton Street near Buena Vista Lagoon.

Incentive District

While no significant impacts are anticipated to currently known wetland resources, the presence and distribution of wetland resources can change over time and a formal wetland delineation was not conducted throughout the entire Incentive District area. For this reason, significant impacts to jurisdictional wetlands and water could occur as a result of future development within the Incentive District area.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Complete Streets BIO-4, which is presented above under Threshold of Significance 2, shall be implemented to address impacts to federal or state wetlands or other waters for the Complete Streets project component.

MM Incentive District BIO-6: Individual development projects implemented under the Incentive District that would impact the areas southwest of the intersection of Eaton Street and South Coast Highway or adjacent to or within Loma Alta Creek may include jurisdictional wetlands or waters and shall be subject to a site-specific assessment of biological resources prior to the City of Oceanside's approval of project plans. If it is determined through the site-specific assessment that excavation, fill, or other modification of wetlands and waters under the jurisdiction of the United States Army Corps of Engineers (USACE), Regional Water Quality Control Board, and California Department of Fish and Wildlife would occur as a result of the project, the project proponent shall be required to conduct a formal jurisdictional delineation in accordance with the U.S. Army Corps of Engineers Wetland Delineation Manual (Environmental Laboratory 1987), and Regional Supplement to the Corps of Engineers Wetlands Delineation Manual: Arid West Region (Version 2.0) (USACE 2008). Permits from the respective regulatory agencies shall also be required, and will likely require mitigation resulting in no net loss of jurisdictional wetlands and waters. It is intended that implementation of the mitigation required through the project permits be consistent and meet the Multiple Habitat Conservation Program goal of no net loss of jurisdictional wetlands and waters.

Facts in Support of Findings:

Complete Streets Improvements

To account for indirect impacts to jurisdictional wetlands or waters associated with the construction of the Complete Streets improvements, MM Complete Streets BIO-4, which requires specific biological

measures for areas around jurisdictional wetlands or waters, will be implemented to ensure impacts will be reduced to less than significant.

Incentive District

While no significant impacts are anticipated to occur to currently known wetland resources with implementation of the Incentive District, MM Incentive District BIO-6 will ensure all potentially significant impacts to federally protected wetlands will be reduced to a less than significant level. MM Incentive District BIO-6 requires a site-specific assessment of biological resources prior to the City of Oceanside's approval of project plans as well as subsequent actions to be implemented depending on the results of the survey.

For the reasons discussed above and in the Final EIR in Section 3.3, Biological Resources, the City of Oceanside finds that implementation of MM Complete Streets BIO-4 and MM Incentive District BIO-6 will reduce the significant project impacts associated with adverse effects to federally protected wetlands to a less than significant level.

Biological Resources Threshold of Significance 4: Interfere with Migratory Wildlife Corridors or Native Wildlife Nursery Sites

Findings: Future development that may occur under the Incentive District will be prioritized within urban/developed areas which have limited potential to support wildlife movement or habitat linkages, but may occur within undeveloped habitat that function as habitat linkages. These types of impacts are similar to the direct impacts discussed for sensitive vegetation communities such as habitat removal or alteration, such as invasive species, construction-related runoff, sedimentation, and dust, or indirect impacts, such as construction and/or operational noise.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Incentive District BIO-5, which is presented above under Biological Resources Threshold of Significance 2, shall be implemented.

Facts in Support of Findings: Development or redevelopment that could occur under the Incentive District could result in direct impacts previously discussed for sensitive vegetation communities, such as habitat removal or alteration, and indirect impacts such as invasive species, construction-related runoff, sedimentation, and dust, or indirect impacts, such as construction and/or operational noise. With implementation of MM Incentive District BIO-5, potentially significant impacts to migratory wildlife corridors or native wildlife nursery sites will be reduced to a less than significant level by requiring specific biological measures per each biological habitat affected by future development.

For the reasons discussed above and in the Final EIR in Section 3.3, Biological Resources, the City of Oceanside finds that implementation of MM Incentive District BIO-5 will reduce the significant project impacts associated with interference with migratory wildlife corridors or native wildlife nursery sites to a less than significant level.

Biological Resources Threshold of Significance 6: Conflict with Applicable Habitat Conservation Plans

Findings: The entire Incentive District is within the Multiple Habitat Conservation Program (MHCP) established for portions of Northwestern San Diego County. The Incentive District is outside of but adjacent to the hardline areas within Buena Vista Lagoon. The remainder of the Incentive District, including Loma Alta Creek, is outside of the Focused Planning Areas. The developed area within the Incentive District is not considered a conserved vegetation community under the MHCP. In undeveloped areas southwest of the intersection of Eaton Street and South Coast Highway Incentive District projects could affect MHCP Habitat Group A communities, including sensitive riparian and upland vegetation communities. These potential effects will be limited to the non-developed areas southwest of the intersections of Easton Street and Coast Highway and along the railroad tracks.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Incentive District BIO-5, which is presented above under Biological Resources Threshold of Significance 2, shall be implemented to address consistency of the projects developed under the Incentive District with the MHCP.

Facts in Support of Findings: Future development under the Incentive District could result in conflicts with the MHCP, especially in the non-developed areas southwest of the intersections of Easton Street and Coast Highway and along the railroad tracks. Incorporation of MM Incentive District BIO-5 requires specific biological measures for each biological habitat affected by future development, which will reduce potentially significant impacts related to conflicts with the MHCP to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.3, Biological Resources, the City of Oceanside finds that implementation of MM Incentive District BIO-5 will reduce the significant project impacts associated with conflicts with applicable habitat conservation plans, including the MHCP, to a less than significant level.

Cultural Resources

Cultural Resources Threshold of Significance 1: Adverse Change to Significance of a Historical Resources

Finding:***Complete Streets Improvements***

Although the Complete Streets improvements area is largely developed, it is possible that subsurface prehistoric and historic-period archaeological resources have been paved over and are obscured. The Complete Streets improvements area should be considered sensitive for the presence of archaeological resources and it should be noted that the project has the potential to significantly impact undocumented subsurface archaeological deposits that may qualify as historical resources pursuant to CEQA.

Incentive District

The project area is considered sensitive for the presence of archaeological resources and future projects within the Incentive District area may significantly impact previously undocumented subsurface archaeological resources that may qualify as historical pursuant to CEQA. Furthermore, the Incentive District area contains resources that may qualify as historical resources. As such, future projects within the Incentive District area have the potential to significantly impact historical resources.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Complete Streets CR-1: Prior to the issuance of a grading permit, the City of Oceanside shall enter into a pre-excavation agreement with a representative of the San Luis Rey Band of Mission Indians, otherwise known as a Tribal Cultural Resources Treatment and Tribal Monitoring Agreement. A copy of the agreement shall be included in the grading plan submittals for the grading permit. The purpose of this agreement shall be to formalize protocols and procedures between the applicant/owner and the San Luis Rey Band for the protection and treatment of, including but not limited to, Native American human remains, funerary objects, cultural and religious landscapes, ceremonial items, traditional gathering areas, and cultural items located and/or discovered through a monitoring program in conjunction with the construction of the Complete Streets improvements, including additional archaeological surveys and/or studies, excavations, geotechnical investigations, grading, and all other ground-disturbing activities, such as the installation and/or removal of infrastructure and existing foundations, that may impact the native soils subsurface to the existing road bed.

MM Complete Streets CR-2: Prior to the issuance of a grading permit, the grading contractor shall provide a written and signed letter to the City Planner stating that a qualified archaeologist and Luiseño Native American Monitor have been retained at the grading contractor's expense to implement the monitoring program, as described in the pre-excavation agreement.

MM Complete Streets CR-3: Prior to the release of the grading bond, a monitoring report and/or evaluation report, if appropriate, which describes the results, analyses, and conclusions of the archaeological monitoring program (e.g., data recovery plan) shall be submitted by the qualified archaeologist, along with the Luiseño Native American monitor's notes and comments, to the City Planner for approval.

MM Complete Streets CR-4: The qualified archaeologist shall maintain ongoing collaborative consultation with the Luiseño Native American monitor during all ground-disturbing activities that may impact subsurface native soils. The requirement for the monitoring program shall be noted on all applicable construction documents, including demolition plans, grading plans, etc. The grading contractor shall notify the City Planner of the start and end of all ground-disturbing activities.

MM Complete Streets CR-5: The qualified archaeologist and Luiseño Native American monitor shall attend all applicable pre-construction meetings with the general contractor and/or associated subcontractors to present the archaeological monitoring program. The qualified archaeologist and Luiseño Native American monitor shall be present onsite during any ground-disturbing activities that may impact subsurface native soils.

MM Complete Streets CR-6: The qualified archaeologist or the Luiseño Native American monitor may halt ground-disturbing activities if unknown archaeological artifact deposits or cultural features are discovered. Ground-disturbing activities shall be directed away from these deposits to allow a determination of potential importance. Isolates and clearly non-significant deposits will be minimally documented in the field, and, before grading proceeds, these items shall be given to the San Luis Rey Band so that they may be repatriated at the site on a later date. If a determination is made that the unearthed artifact deposits or cultural features are considered potentially significant, the San Luis Rey Band shall be notified and consulted with in regard to the respectful and dignified treatment of those resources.

The avoidance and protection of the significant cultural resource and/or unique archaeological resource is the preferable mitigation. If, however, a data recovery plan is authorized by the City as the lead agency under CEQA, the San Luis Rey Band shall be notified and consulted regarding the drafting and finalization of any such recovery plan. For significant artifact deposits or cultural features that are part of a data recovery plan, an adequate artifact sample to address research avenues previously identified for sites in the project area will be collected using professional archaeological collection methods. If the qualified archaeologist collects such resources, the Luiseño Native American monitor must be present during any testing or cataloging of those resources. Moreover, if the qualified archaeologist does not collect the cultural resources that are unearthed during the ground-disturbing activities, the Luiseño Native American monitor may, at their discretion, collect said resources and provide them to the San Luis Rey Band for respectful and dignified treatment in accordance with the Tribe's cultural and spiritual traditions.

MM Complete Streets CR-7: Any and all uncovered tribal cultural resources of Native American importance shall be returned to the San Luis Rey Band of Mission Indians, and/or the Most Likely Descendant, if applicable, and not be curated.

MM Complete Streets CR-8: As specified by California Health and Safety Code Section 7050.5, if human remains are found in the project area during construction or during archaeological work, the person responsible for the excavation, or his or her authorized representative, shall immediately notify the San Diego County Coroner's office by telephone. No further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains shall occur until the Coroner has made the necessary findings as to origin and disposition pursuant to PRC 5097.98. If such a discovery occurs, a temporary construction exclusion zone shall be established, surrounding the area of the discovery so that the area would be protected, and consultation and treatment could occur as prescribed by law. By law, the Coroner will determine within 2 working days of being notified if the remains are subject to his or her authority. If the Coroner recognizes the remains to be Native American, he or she shall contact the Native American Heritage Commission (NAHC) within 24 hours. The NAHC will make a determination as to the Most Likely Descendant. If Native American remains are discovered, the remains shall be kept in situ, or in a secure location in close proximity to where they were found, and the analysis of the remains shall occur only on-site in the presence of a Luiseño Native American monitor.

MM Complete Streets CR-9: The qualified archeologist, or an archaeologist working under the direction of the qualified archaeologist, and the Luiseño Native American monitor shall conduct pre-construction cultural resources sensitivity training to inform construction personnel of the types of cultural resources that may be encountered, and of the proper procedures to be enacted in the event of an inadvertent discovery of archaeological resources or human remains. The applicant/owner shall ensure that construction personnel are made available for and attend the training and shall retain documentation demonstrating attendance.

MM Incentive District CR-1: Individual development projects implemented under the Incentive District shall be subject to a Phase I cultural resources inventory (cultural resources inventory) prior to the City of Oceanside's approval of project plans. This requirement shall be implemented for all projects for which the Incentive District is employed (Administrative Approval, Development Plan Review, and Conditional Use Permit processing requirements as specified in Section 1203 of the Coast Highway Incentive District). The cultural resources inventory would consist of: a cultural resources records search to be conducted at the South Coastal Information Center; scoping with the California Native American Heritage Commission (NAHC); a pedestrian archaeological survey if visible ground surface is present; and recordation of all identified archaeological resources on California Department of Parks and Recreation 523 forms. The cultural resources inventory shall be carried out by a qualified archaeologist, defined as an archaeologist meeting the Secretary of the Interior's Standards for professional archaeology, and shall be conducted in consultation with the appropriate Native American groups as identified through outreach to the NAHC and through consultation.

If potentially significant cultural resources are encountered during the survey, and if the project has the potential to impact those resources, the City of Oceanside shall require that the resources be evaluated for their eligibility for listing in the California Register of Historical Resources (CRHR) and for significance as unique archaeological resource. Recommendations shall be made for the treatment of unique archaeological resources or resources found eligible for the CRHR should the development project have the potential to adversely impact the resources. These studies shall be conducted in consultation with the City of Oceanside and the appropriate Native American groups as identified through consultation. project redesign and preservation in place shall be the preferred means of mitigation to avoid impacts to significant cultural resources, including prehistoric and historic archaeological sites, locations of importance to Native Americans, human remains, historical buildings, structures, and landscapes. Methods of avoidance may include, but shall not be limited to, project redesign or identification of protection measures such as capping or fencing. If it is demonstrated that resources cannot be avoided, the qualified archaeologist shall develop additional treatment measures, which may include data recovery or other appropriate measures, in consultation with the City of Oceanside and appropriate Native American groups as identified through consultation.

In addition, the project proponent shall retain archaeological monitors and Native American monitors during ground-disturbing activities that have the potential to impact significant cultural resources as determined by a qualified archaeologist in consultation with the City of Oceanside and the appropriate Native American groups.

During project-level construction, should prehistoric or historic subsurface cultural resources be discovered, all activity in the vicinity of the find shall stop and a qualified archaeologist shall be contacted to assess the significance of the find. If any find is determined to be significant, meaning it qualifies as a unique archaeological resource or is determined eligible for the CRHR, the archaeologist shall determine, in consultation with the City of Oceanside and the appropriate Native American groups, suitable avoidance measures, data recovery measures, or other appropriate mitigation, such as capping.

All significant cultural materials recovered, either prior to or during construction, shall be, as necessary and at the discretion of the consulting archaeologist and in consultation with the appropriate Native American groups, subject to scientific analysis, professional museum curation, and documentation according to current professional standards. If materials need to be recovered, protocols for proper removal and treatment shall be implemented. The specific protocols for proper removal shall be detailed in a monitoring or data recovery plan prior to recovery of the materials.

MM Incentive District CR-2: Project-level development on individual properties containing structures at least 50 years old shall be subject to a historic built environment survey, which will include an evaluating of the potential historic significance of the structures, prior to the City of Oceanside's approval of project plans. This requirement shall be implemented for all projects on properties for which the Incentive District is employed and that contain existing structures (Administrative Approval, Development Plan Review, and Conditional Use Permit processing requirements as specified in Section 1203 of the Coast Highway Incentive District). The survey shall be carried out by a qualified historian or architectural historian meeting the Secretary of the Interior's Standards for Architectural History. If potentially significant historic resources are encountered during the survey, demolition or substantial alteration of such resources identified shall be avoided, as specified by the qualified historian or architectural historian.

Facts in Support of Findings: Construction of both the Complete Streets improvements and the Incentive District have the potential to impact subsurface prehistoric and historic-period archaeological resources which could have been paved over and are obscured. Furthermore, the Incentive District area contains resources that may qualify as historical resources. With implementation of MM Complete Streets CR-1 through CR-9, MM Incentive District CR-1 and CR-2, potentially significant impacts related to historical resources will be reduced to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.4, Cultural Resources, the City of Oceanside finds that implementation of MM Complete Streets CR-1 through CR-9, MM Incentive District CR-1 and CR-2 will reduce the significant project impacts associated with causing adverse changes to the significance of historical resources to a less than significant level.

Cultural Resources Threshold of Significance 2: Adverse Change to Significance of an Archaeological Resources

Finding:

Complete Streets Improvements

Although the Complete Streets improvements area is largely developed, it is possible that subsurface prehistoric and historic-period archaeological resources have been paved over and are obscured. Therefore, the Complete Streets improvements have the potential to significantly impact undocumented subsurface archaeological deposits that may qualify as unique archaeological resources.

Incentive District

The Incentive District contains known archaeological resources, and is considered sensitive for the presence of archaeological resource that could qualify as unique archaeological resources under CEQA Guidelines Section 15064.5. As such, future projects within the Incentive District could significantly impact previously undocumented subsurface archaeological resources that may qualify as unique archaeological resources.

Specifically, the project will be conditioned to incorporate MM Complete Streets CR-1 through CR-9 and MM Incentive District CR-1 and CR-2 (provided above under Threshold of Significance 1), which will reduce project-related significant impacts to previously unidentified archaeological resources within the Complete Streets improvements and Incentive District project areas.

Facts in Support of Findings:

Complete Streets Improvements

Although the Complete Streets improvements area is largely developed, the research conducted for the EIR, including a records search, indicates that previously recorded archaeological resources have been documented within and in the vicinity of the Complete Streets improvements project area and that areas within the vicinity of the project are sensitive for the presence of prehistoric archeological resources. Thus, the Complete Streets improvements have the potential to significantly impact undocumented subsurface archaeological deposits that may qualify as unique archaeological resources. Implementation of MM Complete Streets CR-1 through CR-9 will reduce significant impacts related to archaeological resources to a less than significant level.

Incentive District

The Incentive District contains three known archaeological resources, and is considered sensitive for the presence of archaeological resource that could qualify as unique archaeological resources under CEQA Guidelines Section 15064.5. As such, future projects within the Incentive District could significantly impact previously undocumented subsurface archaeological resources that may qualify as unique archaeological resources. With implementation of MM Incentive District CR-1 and CR-2, will reduce significant impacts related to archaeological resources to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.4, Cultural Resources, the City of Oceanside finds that implementation of MM Complete Streets CR-1 through CR-9, MM Incentive District CR-1 and CR-2 will reduce the significant project impacts associated with causing adverse changes to the significance of archaeological resources to a less than significant level.

Cultural Resources Threshold of Significance 3: Directly or Indirectly Destroy a Unique Paleontological Resources

Finding: Given that the Incentive District is underlain by paleontologically sensitive formations and that the depths of ground disturbance associated with future projects in the Incentive District are unknown, there exists the possibility that unique paleontological resources or unique geologic features may be impacted by future projects proposed under the Incentive District.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Incentive District CR-3: For project-level development in the Incentive District involving ground disturbance, a qualified paleontologist shall be retained to determine the necessity of conducting a study of the project area(s) based on the potential sensitivity of the project for paleontological resources, and the potential for the project to impact paleontologically sensitive geological deposits. If deemed necessary, the paleontologist shall conduct a paleontological resources inventory designed to identify potentially significant resources. The paleontological resources inventory would consist of a paleontological resources records search to be conducted at the SDNHM; a field survey, if deemed appropriate by the paleontologist; and recordation of all identified

paleontological resources. The paleontologist shall provide recommendations regarding additional work for the project. Impacts to significant paleontological resources, if identified, shall be avoided.

In addition, the project proponent shall retain paleontological monitors during construction for ground-disturbing activities that have the potential to impact significant paleontological resources as determined by a qualified paleontologist.

In the event that paleontological resources are discovered, the project proponent will notify a qualified paleontologist. The paleontologist will document the discovery as needed, evaluate the potential resource, and assess the significance of the find under the criteria set forth in CEQA Guidelines Section 15064.5. If fossil or fossil-bearing deposits are discovered during construction, excavations within 50 feet of the find will be temporarily halted or diverted until the discovery is examined by a qualified paleontologist, in accordance with Society of Vertebrate Paleontology standards. The paleontologist will notify the appropriate agencies to determine procedures that would be followed before construction is allowed to resume at the location of the find. If avoidance is determined to be infeasible, the qualified paleontologist shall implement a paleontological mitigation program. At each fossil locality, field data forms shall be used to record pertinent geologic data, stratigraphic sections shall be measured, appropriate sediment samples shall be collected and submitted for analysis, and any other activities necessary for the timely and professional documentation and removal of fossils shall be conducted. Any fossils encountered and recovered shall be prepared to the point of identification, catalogued, and donated to a public, nonprofit institution with a research interest in the materials. Accompanying notes, maps, and photographs shall also be filed at the repository.

Facts in Support of Findings: Future development under the Incentive District could reach depths that are in native soils and have the potential to significantly impact unknown buried paleontological resources. Incorporation of MM Incentive District CR-3 requires a site-specific paleontological study for each future project within the Incentive District that has ground disturbance to assess and specifically mitigate any impacts to paleontological resources on a site-by-site basis.

For the reasons discussed above and in the Final EIR in Section 3.4, Cultural Resources, the City of Oceanside finds that implementation of MM Incentive District CR-3 will reduce the significant project impacts associated with damaging or destroying unique paleontological resources to a less than significant level.

Cultural Resources Threshold of Significance 4: Disturb Human Remains

Finding: No known human remains exist within the project area. However, since the nature of the proposed project will involve ground-disturbing activities, it is possible that such actions could unearth, expose, or disturb previously unknown human remains.

Specifically, the project will be conditioned to incorporate MM Complete Streets CR-8 and MM Incentive District CR-1 (provided above under Threshold of Significance 1), which will reduce project-related significant impacts to disturbing unknown buried human remains within the Complete Streets improvements and Incentive District project areas.

Facts in Support of Findings: While there are no known cemeteries or burial sites located within the project area, ground-disturbing activities associated with construction of the Complete Streets improvements and future projects proposed under the Incentive District could potentially disturb

unknown buried human remains. Implementation of MM Complete Streets CR-8 and MM Incentive District CR-1, which require implementation of the protocols for the discovery of human remains established by California Health and Safety Code Section 7050.5 and Public Resource Code 5097.98, will reduce significant impacts related to the disturbance of human remains to a less than significant level.

For the reason discussed above and in the Final EIR in Section 3.4, Cultural Resources, the City of Oceanside finds that implementation of MM Complete Streets CR-8 and MM Incentive District CR-1 will reduce the significant project impacts associated with disturbing unknown human remains to a less than significant level.

Cultural Resources Threshold of Significance 5: Tribal Cultural Resources

Finding: The Sacred Lands File search conducted by the Native American Heritage Commission and Assembly Bill 52/Senate Bill 18 consultation conducted by the City of Oceanside have not identified any tribal cultural resources within the project area. However, this does not preclude the possibility that tribal cultural resources may be encountered as a result of further consultation or during ground disturbance during project implementation. As such, it is possible that project implementation may impact tribal cultural resources.

Specifically, the project will be conditioned to incorporate MM Complete Streets CR-1 through CR-9 and MM Incentive District CR-1 and CR-2 (provided above under Threshold of Significance 1), which will reduce project-related significant impacts to tribal cultural resources within project area.

Facts in Support of Findings: While there are no known tribal cultural resources within the project area, ground-disturbing activities associated with construction of the Complete Streets improvements and future projects proposed under the Incentive District have the potential to significantly impact unknown buried tribal cultural resources. Implementation of MM Complete Streets CR-1 through CR-9 and MM Incentive District CR-1 and CR-2 will required to ensure that the appropriate measures as agreed upon with the San Luis Rey Band of Mission Indians are in place during construction of the Complete Streets improvements and future projects proposed under the Incentive District. These measures include, but are not limited to, requirement of a Tribal Cultural Resources Treatment and Tribal Monitoring Agreement by the City of Oceanside and the San Luis Rey Band of Mission Indians, the presence of a Luiseño Native American monitor in addition to a qualified archaeologist during all pre-construction meetings and ground-disturbing construction activities, and agreement that any and all uncovered tribal cultural resources of Native American importance shall be returned to the San Luis Rey Band of Mission Indians. Therefore, incorporation of MM Complete Streets CR-1 through CR-9 and MM Incentive District CR-1 and CR-2 will reduce significant impacts related to tribal cultural resources d to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.4, Cultural Resources, the City of Oceanside finds that implementation of MM Complete Streets CR-1 through CR-9 and MM Incentive District CR-1 and CR-2 will reduce the significant project impacts related to tribal cultural resources to a less than significant level.

Hazards and Hazardous Materials

Hazards and Hazardous Materials Threshold of Significance 4: Located on a site that is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5

Finding: Future soil excavation activities within the Incentive District could encounter contaminated soil, soil vapor, and/or groundwater contamination.

The City of Oceanside finds that changes or alterations have been required in or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Incentive District HAZ-1: To assess the status of the remediation of the contaminated sites listed above, as well as checking for any newly contaminated sites, individual project proponents for each proposed project within the Incentive District area (the applicant or its contractor) shall conduct a Phase I Environmental Site Assessment in general accordance with ASTM Standard 1527-13, *Standard Practice for Environmental Site Assessments: Phase I Environmental Site Assessment Process*, or later versions if any. The ASTM standard requires checking for active contaminated sites within a specified radius that have the potential to affect a given project. In the event that the extent of contamination from a site extends to a proposed project site, the applicant or its contractor for each proposed project would implement MM Incentive District HAZ-2.

MM Incentive District HAZ-2: If the Phase I Environmental Site Assessment prepared in accordance with MM Incentive District HAZ-1 determines that contamination is present on a project site proposed for development, the following additional measures shall be required:

- a. The applicant's construction contractor(s) shall prepare and implement a site-specific Health and Safety Plan in accordance with 29 CFR 1910.120 to protect construction workers and the public during all excavation and grading activities. This plan shall be submitted to the City of Oceanside for review prior to commencement of construction. Note that the project applicant or its contractor would also be required to implement MM Incentive District HAZ-2b, Soil and Groundwater Management Plan, described further below. The Health and Safety Plan shall include, but is not limited to, the following elements:
 - Designation of a trained, experienced site safety and health supervisor who has the responsibility and authority to develop and implement the site health and safety plan.
 - A summary of all potential risks to construction workers and maximum exposure limits for all known and reasonably foreseeable on-site chemicals.
 - Specified personal protective equipment and decontamination procedures, if needed.
 - Emergency procedures, including route to the nearest hospital.
 - Procedures to be followed in the event that evidence of potential soil contamination (such as soil staining, noxious odors, debris, or buried storage containers) is encountered. These procedures shall be in accordance with hazardous waste operations regulations and specifically include, but not be limited to, the following: immediately stopping work in the vicinity of the unknown hazardous materials release, notifying the County of San Diego Department of Environmental Health, and retaining a qualified environmental firm to perform sampling and remediation.

- b. In support of the Health and Safety Plan described above, the applicant or its contractor shall develop and implement a Soil and Groundwater Management Plan that includes a materials disposal plan specifying how the construction contractor will remove, handle, transport, and dispose of all excavated material and groundwater from dewatering activities in a safe, appropriate, and lawful manner. The plan must identify protocols for soil and groundwater testing and disposal, identify the approved disposal site, and include written documentation that the disposal site will accept the waste. Contract specifications shall mandate full compliance with all applicable local, state, and federal regulations related to the identification, transportation, and disposal of hazardous materials, including those encountered in excavated soil or groundwater.

Facts in Support of Findings: Future soil excavation activities within the Incentive District could encounter contaminated soil, soil vapor, and/or groundwater contamination at or associated with Buck's Texaco (628 South Coast Highway), Pop's Hot Rod Garage (305 Wisconsin Avenue), Rashid South Hill Shell (1202 South Coast Highway), H.G. Fenton (1517 South Coast Highway), Mobil 18 GCL (1742 South Coast Highway), Econo Lube'N Tube (1942 South Coast Highway), and Golden State Gas Inc. (1943 South Coast Highway). While regulatory compliance would require best management practices during construction, the potential for contaminated soil and soil vapor to be encountered and released into the environment during project construction is considered a significant impact. Implementation of MM Incentive District HAZ-1 and HAZ-2 will require preparation of a site-specific Phase I Environmental Site Assessment to determine the appropriate hazardous materials and/or remediation activities required on a project-by-project basis, which will reduce impacts to less than significant.

For the reasons discussed above and in the Final EIR in Section 3.7, Hazards and Hazardous Materials, the City of Oceanside finds that implementation of MM Incentive District HAZ-1 and HAZ-2 will reduce the significant project impacts associated with the potential for contaminated soil and soil vapor to be encountered and released into the environment to a less than significant level.

Noise and Vibration

Noise and Vibration Threshold of Significance 2: Expose Persons to Excess Ground-borne Vibration or Noise Levels

Finding: Due to the densely developed area within the Incentive District boundaries and the inability to know the exact nature of future proposed projects under the Incentive District, development within the Incentive District zone could be adjacent to other properties with existing structures (e.g., residences, commercial businesses). Therefore, construction activities that use typical heavy construction equipment associated with future development under the Incentive District could result in temporary significant ground-borne vibration impacts that will exceed the threshold of human perception to sensitive receptors.

The City of Oceanside finds that changes or alterations have been required in, or incorporated into the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Incentive District NOI-1: For development projects considered under the Incentive District provisions, a project-level vibration analysis would be required if the construction plans for the project would include the use of any of the following:

- a) Typical heavy construction equipment within 25 feet of existing inhabited structures. Typical heavy equipment is defined as equipment with an engine size of 600 horsepower or greater and includes: large dozers, large excavators, and large loaders.
- b) Vibratory compaction rollers for use within 80 feet of inhabited structures.
- c) Pile drivers for use within 150 feet of inhabited structures.

If none of the construction methods mentioned in the list enumerated above are proposed within the described boundaries, no further analysis would be required, since the distances to sensitive receptors would create enough of a buffer to ensure impacts are less than significant.

The purpose of each project-level vibration analysis would be to determine if the specific project-level construction would generate vibration levels exceeding the human perception threshold of 0.1 in/sec PPV at the receptor. project-specific details that would be required in each analysis would include, but not be limited to, type, size, and horsepower of the actual construction equipment to be used; specific locations of each activity; and actual distances from the activity to inhabited buildings. Vibration levels of actual equipment to be used shall be estimated from FTA vibration guidance documents (FTA 2006), attenuated with distance to the inhabited structures, and compared to the Caltrans vibration threshold for human perception. If applicable, the intervening ground between equipment and structures would be considered for its soil properties for additional vibration attenuation.

If the project-specific analysis determines that a project-specific significant impact could occur, mitigation shall be required to reduce the impact to less than significant. Alternative construction methods and equipment that generate lower vibration levels shall be considered. Estimated construction vibration levels would be required to not to exceed the vibration threshold of human perception at inhabited buildings (0.1 in/sec PPV at the receptor). Field vibration measurement surveys of actual construction vibration would be considered, as determined to be required by the vibration specialist, as part of construction vibration compliance with the threshold.

This requirement shall be implemented for all projects under the Incentive District (Administrative Development Plan Review, Development Plan Review, and Conditional Use Permit processing requirements as specified in Section 1203 of the Coast Highway Incentive District).

Facts in Support of Findings: With regard to construction activities of the potential development under the Incentive District, ground-borne vibration would be generated by the operation of heavy equipment primarily during site clearing and grading activities and to a lesser degree by off-site haul trucks traveling on surface streets. Construction activities that use typical heavy construction equipment associated with future development under the Incentive District could result in temporary significant ground-borne vibration impacts that would exceed the threshold of human perception to sensitive receptors. The type of structures and construction methods and equipment of each of the potential developments of the Incentive District is unknown at this time. Depending on the location of future development projects occurring under the provisions of the Incentive District, sensitive receptors may be located near the development that will be potentially be affected by construction vibration. Implementation of MM Incentive District NOI-1 will require a site-specific vibration analysis on a project-by-project basis, which will reduce vibration impacts to a less than significant level.

For the reasons discussed above and in the Final EIR in Section 3.10, Noise and Vibration, the City of Oceanside finds that implementation of MM Incentive District NOI-1 will reduce the significant project impacts associated vibration effects related to development proposed under the Incentive District to a less than significant level.

Transportation and Traffic

Transportation and Traffic Threshold of Significance 1: Conflict with an Applicable Plan, Ordinance, or Policy Establishing Measures of Effectiveness for the Performance of the Circulation System

Finding: The Existing Conditions + Project scenario was modeled with implementation of the Complete Streets improvements and with a land use condition representative of existing land uses within the city in 2013. As detailed in the EIR, implementation of the Complete Streets improvements would result in an unacceptable LOS (LOS E or LOS F) at two study intersections, both of which are locations where roundabouts would be installed:

- 27. Coast Highway & Oceanside Boulevard – LOS F during PM peak-hour
- 35. Coast Highway & Cassidy Street – LOS F during PM peak-hour

Thus, a potentially significant impact would occur at these two study intersections under the Existing Conditions + Project scenario.

The City of Oceanside finds that changes or alterations have been required in, or incorporated into, the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Complete Streets TR-1: In order to mitigate the deficient LOS at the two study area intersections under the Existing Conditions + Project scenario, the City of Oceanside shall implement the following measures to improve intersection operations to an acceptable LOS. The City of Oceanside shall include the project modifications in the Complete Streets construction plans or completed prior to the finalization of the construction plans. The improvements shall be completed either prior to or concurrent with the Complete Streets improvements.

The specific measures for the two degraded study intersections in the Existing Conditions + Project scenario are as follows:

	Location	Mitigation Measure	Additional Comments	Mitigated Conditions		Reduced to Less than Significant
				Delay (sec)	LOS	
27	Coast Hwy & Oceanside Blvd	Maintain Existing Traffic Signal	Merging of two lanes into one lane would occur north of intersection before Wisconsin Avenue	41.2	D	Yes ¹
35	Coast Hwy & Cassidy St	Maintain Existing Signal	No other adjustments required	19.2	B	Yes

NOTE: ¹ Because Intersection 27 is in the City of Oceanside's jurisdiction, LOS D is considered an acceptable LOS.

SOURCE: IBI 2018

Facts in Support of Findings: Implementation of MM Complete Streets TR-1 will improve operations at the two degraded intersections under the City of Oceanside's jurisdiction to an acceptable LOS in the Existing Conditions + Project Scenario by requiring that the City of Oceanside maintain the existing traffic signal instead of installing a roundabout. Therefore, with maintenance of the existing traffic signals at these two intersections as required by MM Complete Streets TR-1, potentially significant traffic impacts under the Existing Conditions + Project scenario will be reduced to a less than significant level.

For the reason discussed above and in the Final EIR in Section 3.14, Transportation and Traffic, the City of Oceanside finds that implementation of MM Incentive District Complete Streets TR-1 will reduce the significant traffic impacts under the Existing + Project scenario to a less than significant level.

Transportation and Traffic Threshold of Significance 4: Interfere with Emergency Response Plan or Evacuation Plan

Finding: Construction of the Complete Streets improvements and future development and redevelopment under the Incentive District could potentially result in temporary roadway interferences and/or closures, which would result in potentially significant impacts to emergency access, including during an evacuation.

The City of Oceanside finds that changes or alterations have been required in, or incorporated into, the project, which avoid or substantially lessen these significant environmental effects. Specifically, the project will be conditioned as follows:

MM Complete Streets TR-3: Prior to the start of construction of the Complete Streets improvements, the City of Oceanside shall require the construction contractor to prepare a Traffic Control Plan. The Traffic Control Plan will show all signage, striping, delineated detours, flagging operations, and any other devices that will be used during construction to guide motorists safely through the construction area and allow for adequate access and circulation to the satisfaction of the City of Oceanside. The Traffic Control Plan will be prepared in accordance with the City of Oceanside's traffic control guidelines and will be prepared to ensure that access will be maintained to individual properties, and that emergency access will not be restricted. The Traffic Control Plan will ensure that congestion and traffic delay are not substantially increased as a result of the construction activities. In addition, the City of Oceanside shall provide written notice at least 2 weeks prior to the start of construction to owners/occupants along streets to be affected during construction.

During construction, the City of Oceanside will maintain continuous vehicular and pedestrian access to residential driveways from the public street to the private property line, except where necessary construction precludes such continuous access for reasonable periods of time. Access will be reestablished at the end of the workday. If a driveway needs to be closed or interfered with as described above, the City of Oceanside shall notify the owner or occupant of the closure of the driveway at least 5 working days prior to the closure. The Traffic Control Plan shall include provisions to ensure that the construction of the Complete Streets improvements does not interfere unnecessarily with the work of other agencies such as emergency service providers, mail delivery, school buses, and municipal waste services.

MM Incentive District TR-1: Prior to submittal of grading plans for development and redevelopment projects under the Incentive District that would result in temporary interferences along roadways within the project area, project applicants and/or private developers shall prepare a Traffic

Control Plan for approval by the City of Oceanside Transportation Division. The Traffic Control Plan will show all signage, striping, delineated detours, flagging operations, and any other devices that will be used during construction to guide motorists safely through the construction area and allow for adequate access and circulation to the satisfaction of the City of Oceanside. The Traffic Control Plan will be prepared in accordance with the City of Oceanside's **traffic control guidelines** and to ensure that access will be maintained to individual properties, and that emergency access will not be restricted. The Traffic Control Plan will ensure that congestion and traffic delay are not substantially increased as a result of the construction activities. In addition, the project applicants and/or private developers shall provide written notice at least 2 weeks prior to the start of construction to owners/occupants along streets to be affected during construction.

During construction, continuous vehicular and pedestrian access to residential driveways from the public street to the private property line will be maintained, except where necessary construction precludes such continuous access for reasonable periods of time. Access will be reestablished at the end of the workday. If a driveway needs to be closed or interfered with as described above, the project applicants and/or private developers shall notify the owner or occupant of the closure of the driveway at least 5 working days prior to the closure. The Traffic Control Plan shall include provisions to ensure that the construction does not interfere unnecessarily with the work of other agencies such as emergency service providers, mail delivery, school buses, and municipal waste services.

Facts in Support of Findings: Construction of the project would involve temporary lane closures which will include partial lane closures, construction vehicles and equipment entering and exiting the project area, and pedestrian and/or bicycle lane closures. Construction of the Complete Streets improvements will require temporary interference along Coast Highway. While all construction activities for the Complete Streets improvements will be phased to not occur simultaneously throughout the corridor, the partial lane and intersection closures along Coast Highway and proposed intersections could potentially result in temporary impacts to emergency access.

Future development and redevelopment projects which may occur under the Incentive District could also include construction activities that could result in temporary interferences along the Coast Highway corridor or surrounding roadways. Temporary interferences could include, but are not limited to, temporary lane closures during periods of loading and/or unloading of trucks, construction vehicles and equipment entering and exiting the project sites, and other construction activities, such as trenching for utility connections, near roadways within the project area. Therefore, there is a potential that temporary emergency access impacts could occur. Implementation of MM Complete Streets TR-3 and MM Incentive District TR-1 will require preparation and implementation of Traffic Control Plan during temporary lane closures, which will reduce impacts to less than significant.

For the reasons discussed above and in the Final EIR in Section 3.14, Transportation and Traffic, the City of Oceanside finds that implementation of MM Complete Streets TR-3 and MM Incentive District TR-1 will reduce significant project impacts related to interference with emergency response plan or evacuation plan to a less than significant level.

IX. Findings Regarding Significant Environmental Effects that Remain Significant and Unavoidable After Mitigation

The Final EIR identifies four environmental issue areas in which the project will result in a significant impact on the environment even after the application of all feasible mitigation measures identified in the Final EIR: Air Quality, Greenhouse Gas Emissions, Noise and Vibration, and Transportation and Traffic. The City of Oceanside finds that the following environmental impacts of the project are significant and unavoidable and cannot be reduced below significance through feasible mitigation measures.

Air Quality

Air Quality Threshold of Significance 2: Air Quality Violation

Finding: Future project-specific construction activities that would occur as a result of the Incentive District would cause temporary, short-term emissions of nonattainment air pollutants in the San Diego Air Basin — specifically, O₃ precursors (i.e., VOCs and NO_x), and PM₁₀ and PM_{2.5}—as a result of construction activities.

In addition, future development that could occur as a result of the Incentive District could result in an increase in the total amount of VMT since the Incentive District would allow for an increase in overall intensity of development within the Incentive District. Such an increase may result in an increase in the total amount of vehicle miles travelled (VMT) due to increased overall density, which may result in an overall increase in mobile source emissions despite the improved transportation efficiency and per-capita emissions reductions expected from increasingly stringent vehicle emissions standards. Therefore, the operation of projects developed under the Incentive District would result in a potentially significant impact.

The City of Oceanside finds that even with the feasible changes or alterations that have been required in, or incorporated into the project, which lessen these significant environmental effects, impacts will remain significant. Specifically, the project will be conditioned as follows:

Incentive District Air-1a: Prior to the issuance of a grading or building permit, whichever is required to be obtained first, individual development projects proposed under the Incentive District shall comply with the following land preparation, excavation, and/or demolition mitigation measures during construction activities:

- All soil excavated or graded should be sufficiently watered to prevent excessive dust. Watering should occur with complete coverage of disturbed soil areas. Watering should be a minimum of twice daily on unpaved/untreated roads and on disturbed soil areas with active operations.
- All clearing, grading, earth moving and excavation activities should cease: (a) during periods of winds greater than 20 mph (averaged over 1 hour as measured by an on-site anemometer or an off-site anemometer that is representative of the construction area), if disturbed material is easily windblown, or (b) when visible dust plumes impact public roads, occupied structures, or neighboring property.

- Vehicles traveling over unpaved roadways shall be limited to 15 miles per hour or less. Signs shall be posted at construction sites identifying the maximum speed limit.
- All trucks hauling dirt, sand, soil, or other loose material shall be covered or maintain at least 2 feet of freeboard, in accordance with the requirements of California Vehicle Code (CVC) Section 23114.
- If more than 5,000 cubic yards of fill material will be imported or exported from the site, all haul truck access points shall be equipped with a gravel pad, rumble pad, or similar control to reduce vehicle trackout.
- Adjacent streets with visible dust, dirt, sand, or soil material accumulation shall be cleaned and the accumulated material removed using street sweepers.
- Stockpiles of soil or other fine loose material shall be stabilized by watering, covered with tarp, or other appropriate method to prevent wind-blown fugitive dust.
- Where acceptable to the local fire department, weed control should be accomplished by mowing instead of digging, thereby, leaving the ground undisturbed and with a mulch covering.
- Locate construction staging areas away from sensitive receptor areas, such as schools, to the extent practicable.
- Minimize the free drop height of excavated soil during batch-drop operations (i.e., earthwork with front-end loader or backhoe) so that the generation of dust is limited to the immediate area around the truck bed or storage pile.
- Install project landscaping in appropriate areas as soon as construction in an area is complete to minimize exposed soils.

MM Incentive District AIR-1b: Prior to the issuance of a grading or building permit, whichever is required to be obtained first, individual proposed projects shall comply with the following construction equipment mitigation measures:

- Construction equipment, on-road trucks, and emission control devices shall be properly maintained and tuned in accordance with manufacturer specifications.
- Construction contractors shall be required to comply with California's on-road and off-road vehicle emissions regulations, including the CARB idling restrictions and the USEPA/CARB on-road and off-road diesel vehicle emissions standards, as required by 13 CCR, Sections 2485, 2025(h), and 2449.
- Off-road diesel-powered construction equipment greater than 50 hp (e.g., excavators, graders, dozers, scrapers, tractors, loaders, etc.) shall be outfitted with Best Available Control Technology (BACT) devices certified by CARB such as certified Level 3 Diesel Particulate Filter or equivalent. A copy of each unit's certified BACT documentation and CARB or SCAQMD operating permit shall be provided at the time of mobilization of each applicable unit of equipment.
- Route construction trucks away from sensitive receptor areas.
- Where available, use electricity from power poles rather than temporary diesel or gasoline powered generators.

MM Incentive District AIR-1c: Construction contractors shall ensure that interior architectural coatings have a maximum of 10 grams per liter of VOC for both residential and commercial development.

MM Incentive District AIR-2: Prior to the issuance of a building permit, individual development projects proposed under the Incentive District regulations shall comply with the following mitigation measures:

- a. Provide direct pedestrian and bicycle access from any Incentive District residential development with a density of four or more residences per acre and in any mixed-use or commercial development to the public right-of-way. Low-, medium-, and high-density Incentive District developments shall provide curbs and sidewalks all public street frontages. Curbs and sidewalks shall also be provided on both sides of all internal streets, unless an equivalent or superior pedestrian path is provided within the development.
- b. For medium- to high-density residential, mixed-use, or commercial developments in the Incentive District area where transit services exist but no transit stop is located within 0.5 mile of the development site, or where transit service does not exist and the development project is within a transit district's sphere of influence, development projects shall provide plans indicating locations of bus turnouts and loading areas with shelters that are acceptable to the local transit provider.
- c. Promote the expanded use of renewable fuel and low-emission vehicles by including one or both of the following project components: preferential parking for ultra-low emission, zero-emission, and alternative-fuel vehicles; and/or electric vehicle supply equipment within the development that meets or exceeds the Tier 1 standards in the current 2016 Title 24 and 2016 California Green Building Standards. Nothing in this measure shall supersede an individual development project's legal responsibility to meet the applicable mandatory minimum requirements of the version of the Title 24 and California Green Building Standards in effect at the time of building permit issuance.
- d. Development projects shall be required to reduce energy consumption by designing buildings that meet or exceed the Tier 1 building energy budget standards in the current 2016 Title 24 and 2016 California Green Building Standards. Nothing in this measure shall supersede an individual development project's legal responsibility to meet the applicable mandatory minimum requirements of the version of the Title 24 and California Green Building Standards in effect at the time of building permit issuance.
- e. Development projects shall be required to reduce water consumption by installing water-efficient fixtures, appliances, toilets/urinals, and landscape irrigation systems that meet or exceed the Tier 1 standards in the current 2016 Title 24 and 2016 California Green Building Standards. Nothing in this measure shall supersede an individual development project's legal responsibility to meet the applicable mandatory minimum requirements of the version of the Title 24 and California Green Building Standards in effect at the time of building permit issuance.
- f. Development projects shall promote transportation demand management principles such as peak hour trip reduction, staggered work hours, ride sharing, telecommuting, and the use of public transportation or other measures, as appropriate.

While MM Incentive District AIR-1a through AIR-1c represent feasible measures to reduce potential impacts associated with construction, impacts would not be reduced to a less than significant level. Additional feasible mitigation measures cannot be developed without knowing the exact timing or location of the construction projects. Because there is no way to accurately predict the intensity of construction of development projects under the Incentive District or their implementation timing, impacts related to resulting in an air quality violation is considered significant and unavoidable even with implementation of MM Incentive District AIR-1a through AIR-1c.

Facts in Support of Findings: Information regarding the size, duration, and construction requirements of specific development projects would be required in order to quantify impacts associated with the construction activities of these individual projects. However, what is known at this time is that the

construction of potential future projects under the Incentive District would be required to comply with applicable State and SDAPCD air quality regulations, including CARB's on-road and off-road vehicle rules on idling limits; NO_x, PM₁₀, and PM_{2.5} exhaust standards; and SDAPCD Rules 55 and 51 (Fugitive Dust and Nuisance) that limit fugitive dust emissions. Additionally, the maximum residential density in the Incentive District would allow for 63 dwelling units per acre. Retail and commercial uses would also be allowed within the Incentive District.

Construction of individual development projects could exceed the SDAPCD thresholds specified for daily emissions of criteria air pollutants. Thus, even with compliance with the applicable rules and regulations, future construction activities associated with the land uses permitted by the Incentive District would have the potential to contribute substantially to an existing or projected air quality violation. Therefore, this impact would be potentially significant.

The Traffic Impact Analysis, which is provided in Appendix G of the EIR, evaluates daily per capita vehicles miles traveled for 2008 base-year conditions and for 2035 both with and without project implementation. Per-capita emissions of mobile source exhaust pollutants (from vehicles), in particular VOC, NO_x, and CO, are expected to decline in future years relative to existing conditions due to improved vehicle emission standards and fuel economy standards that have been adopted by the USEPA and State of California (i.e., emissions standards through vehicle model year 2025). Under current USEPA standards, by vehicle model year 2025, passenger cars and light-duty trucks are required to achieve 54.5 miles per gallon (if emissions reductions are achieved exclusively through fuel economy improvements) and 163 grams of CO₂ emissions per mile. According to the USEPA, a model-year 2025 vehicle would emit approximately one-half of the greenhouse gas emissions from a model-year 2010 vehicle (USEPA 2012). Nonetheless, future development that could occur as a result of adoption of the Incentive District could result in an increase in the total amount of VMT due to increased overall density, which may result in an overall increase in mobile source emissions despite the improved transportation efficiency and per-capita emissions reductions expected from increasingly stringent vehicle emissions standards. For these reasons, the operation of projects developed under the Incentive District would result in a potentially significant air quality impacts.

With implementation of MM Incentive District AIR-1a through AIR-1c, a maximum of 63 dwelling units per acre, and up to 30,000 square feet of retail development per acre could occur simultaneously and result in less-than-significant impacts (i.e., emissions below the daily emissions thresholds). Construction of this level of development could occur in the following ways:³

- Up to six 1-acre lots
- Up to three 2-acre lots
- Up to one 5-acre and four 1-acre lots
- Up to one 5-acre lot and two 2-acre lots
- Up to two 5-acre lots

³ Since the average lot size is between 1 and 2 acres, the analysis focused on 1 and 2 acres with a maximum of 5 acres as an outside development size.

Construction activities exceeding these development levels would likely result in emissions above the daily thresholds resulting in short-term emissions of nonattainment air pollutants which would result in a significant contribution to existing or projects air quality violations. While MM Incentive District AIR-1a through AIR-1c represent feasible measures to reduce potential impacts associated with construction, impacts would not be reduced to a less-than-significant level if simultaneous development exceed the development levels stated above. Additional feasible mitigation measures cannot be developed without knowing the exact timing or location of the construction projects. Because there is no way to accurately predict the intensity of development projects under the Incentive District or their implementation timing, this impacts related to resulting in an air quality violation is considered significant and unavoidable even with implementation of MM Incentive District AIR-1a through AIR-1c.

The City of Oceanside finds that even with incorporation of MM Incentive District AIR-1a-c and MM Incentive District AIR-2, the significant air quality impacts resulting from construction of simultaneous projects under the Incentive District, which exceed the established development levels, cannot be reduced to a less than significant level. Therefore, air quality impacts are considered significant and unavoidable.

Air Quality Threshold of Significance 3: Cumulatively Considerable Net Increase of Any Criteria Pollutant

Finding: Implementation of the Incentive District would generate pollutant emissions from construction and operation of potential future development under the Incentive District. Future development that could occur as a result of adoption of the Incentive District could result in an increase in density or in the total amount of VMT relative to existing conditions, which may result in an overall increase in building and mobile source emissions, despite the improved energy and transportation efficiency and emissions reductions expected from buildings and mobile sources meeting increasingly stringent energy efficiency and vehicle emissions standards.

The City of Oceanside finds that changes or alterations have been required in, or incorporated into, the project, which lessen these significant environmental effects but not below a level of significance. Specifically, MM Incentive District AIR-1a-c and MM Incentive District AIR-2 will be required. The specific requirements of these measures are provided under Air Quality Threshold of Significance 2 above.

The City of Oceanside further finds that even with incorporation of MM Incentive District AIR-1a-c and MM Incentive District AIR-2, the significant project impacts related to a cumulatively considerable net increase of any criteria pollutant cannot be reduced to a less than significant level. Therefore, cumulative construction and operational air quality impacts are considered significant and unavoidable.

Additional feasible measures beyond the mitigation identified above cannot be developed without knowing the exact nature of the proposed developments, including but not limited to the types and sizes of the proposed uses and associated trip generation rates. Development under the Incentive District would potentially result in a cumulatively considerable net increase of a criteria pollutant for which the project region is nonattainment. Therefore, even with implementation of MM Incentive District AIR-1a-c and MM Incentive District AIR-2, impacts would be significant and unavoidable.

Facts in Support of Findings: MM Incentive District AIR-1a-c and MM Incentive District AIR-2 would reduce construction and operational emissions from future development that could occur as a result of adoption of the Incentive District by requiring that the additional measures listed above be incorporated into future projects proposed under the Incentive District on a project-by-project basis prior to issuance of a building permit. Implementation of MM Incentive District AIR-1a-c and MM Incentive District AIR-2 will ensure that each project proposed under the Incentive District mitigates impacts to air quality during construction and operation to the lowest level of significance feasible on a project-by-project basis. However, while MM Incentive District AIR-1a-c and MM Incentive District AIR-2 will help to reduce environmental effects to air quality, detailed information regarding individual development projects within the Incentive District is not currently available. Thus, it cannot be determined with certainty that the above measures would reduce impacts to a less-than-significant level.

Additional feasible measures beyond the mitigation identified above cannot be developed without knowing the exact nature of the proposed developments, including but not limited to the types and sizes of the proposed uses and associated trip generation rates. Development under the Incentive District would potentially result in a cumulatively considerable net increase of a criteria pollutant for which the project region is nonattainment. Therefore, even with implementation of MM Incentive District AIR-1a-c and MM Incentive District AIR-2, impacts would be significant and unavoidable.

Air Quality Cumulative Threshold of Significance

Finding: The project would result in the emission of criteria pollutants during construction of the Complete Streets improvements and the development of the Incentive District. Operation of the potential development projects under the Incentive District would result in mobile source emissions generated by vehicle trips from future development and population growth.

The City of Oceanside finds that all feasible changes or alterations have been required in, or incorporated into the project, which lessen these significant environmental effects but not below a level of significance. Specifically, MM Incentive District AIR-1a-c and MM Incentive District AIR-2 will be required. The specific requirements of these measures are provided under Air Quality Threshold of Significance 2 above.

The City of Oceanside further finds that even with incorporation of MM Incentive District AIR-1a-c and MM Incentive District AIR-2, the significant project impacts related to a cumulatively considerable air quality impacts cannot be reduced to a less than significant level. Therefore, construction and operational air quality impacts are considered significant and unavoidable.

Facts in Support of Findings: Based on the project-specific level of emissions, the project's cumulative impacts would be potentially significant because its maximum daily construction emissions could potentially exceed the SDAPCD screening level thresholds for maximum daily emissions. As detailed information regarding individual development projects within the Incentive District is not currently available, it cannot be determined with certainty that MM Incentive District AIR-1a through AIR-1c would reduce construction emissions from future development that could occur as a result of adoption of the Incentive District to a less than significant level. Additional feasible measures cannot be developed without knowing the exact timing or location of the construction projects. Because there is no way to

accurately predict the intensity of construction associated with the Incentive District or the construction timing, this impact is considered cumulatively significant and unavoidable.

Per capita operational emissions from development projects under the Incentive District are expected to decline in future years relative to existing conditions, in particular mobile source exhaust pollutants from vehicles (i.e., mobile source volatile organic compound, oxides of nitrogen, and carbon monoxide (CO) emissions), due to improved vehicle emission standards and fuel economy standards that have been adopted by the USEPA and State of California (i.e., emissions standards through vehicle model year 2025). Nonetheless, future development that could occur as a result of adoption of the Incentive District could result in an increase in the total amount of vehicle miles traveled due to increased overall density, which may result in an overall increase in mobile source emissions, despite the improved transportation efficiency and per capita emissions reductions expected from mobile sources meeting increasingly more stringent vehicle emissions standards.

Similarly, there is no way to accurately predict the intensity of development projects associated with the entire project and other non-Incentive District projects. As a result, cumulative operational impacts with respect to non-Incentive District projects are also considered cumulatively significant and unavoidable.

Greenhouse Gas Emissions

Greenhouse Gas Emissions Threshold of Significance 1: Generate Significant Levels of Greenhouse Gas Emissions

Finding: Given the amount of development that could occur with implementation of the Incentive District, it is reasonable to assume that in the aggregate, development projects could eventually result in a net increase in greenhouse gas emissions over current emission levels in excess of the County's proposed screening level threshold which is 900 MT of CO₂e per year.

The City of Oceanside finds that all feasible changes or alterations have been required in, or incorporated into the project, which lessen these significant environmental effects but not below a level of significance. Specifically, MM Incentive District AIR-2 will be required. The specific requirements of these measures are provided under Air Quality Threshold of Significance 2 above.

The City of Oceanside further finds that even with incorporation of MM Incentive District AIR-2, the significant project impacts related to greenhouse gas impacts cannot be reduced to a less than significant level. Therefore, greenhouse gas impacts are considered significant and unavoidable.

Facts in Support of Findings: Given the amount of development that could occur with implementation of the Incentive District, it is reasonable to assume that in the aggregate, development projects could eventually result in a net increase in greenhouse gas emissions over current emission levels in excess of the County's proposed screening level threshold which is 900 MT of CO₂e per year. Therefore, implementation of the Incentive District could result in significant greenhouse gas emissions, and mitigation is required. Compliance with current and future Title 24 standards and MM Incentive District AIR-2 would result in development projects which are more energy efficient than current development, relying on a wide array of strategies such as, possibly, solar water heating and photovoltaic roofs, Energy

Star appliances, etc., resulting a reduction in greenhouse gas emissions as compared to current practices. Even with implementation of MM Incentive District AIR-2, the net increase in greenhouse gas emissions in the aggregate could still exceed the County's greenhouse gas emissions thresholds and therefore, impacts are considered significant and unavoidable. There are no additional feasible mitigation measures available to further reduce this impact.

Greenhouse Gas Emission Cumulative Threshold of Significance

Finding: Cumulative development of the entire project (Complete Streets improvements and the Incentive District) in the aggregate could result in a net increase in greenhouse gas emissions over current emission levels in excess of the County's proposed screening level threshold, which is 900 MT of CO₂e (metric tons of carbon dioxide equivalent) per year. Therefore, implementation of the Complete Streets improvements and Incentive District could result in significant greenhouse gas emissions.

The City of Oceanside finds that all feasible changes or alterations have been required in, or incorporated into the project, which lessen these significant environmental effects but not below a level of significance. Specifically, MM Incentive District AIR-2 will be required. The specific requirements of these measures are provided under Air Quality Threshold of Significance 2 above.

The City of Oceanside further finds that even with incorporation of MM Incentive District AIR-2, the significant project impacts related to greenhouse gas impacts cannot be reduced to a less than significant level. Therefore, greenhouse gas impacts are considered significant and unavoidable.

Facts in Support of Findings: Compliance with current and future Title 24 standards and MM Incentive District AIR-2 would result in development projects which are more energy-efficient than current development, relying on a wide array of strategies such as, possibly, solar water heating and photovoltaic roofs, Energy Star appliances, etc., resulting a reduction in greenhouse gas emissions as compared to current practices. There are no additional feasible mitigation measures available. Even with MM Incentive District AIR-2, the net increase in greenhouse gas emissions in the aggregate could exceed thresholds, and impacts are considered potentially cumulatively significant and unavoidable.

As the entire project would be considered potentially cumulatively significant and unavoidable, and because greenhouse gas impacts are exclusively cumulative in nature, operational impacts with respect to non-Incentive District projects are also considered potentially cumulatively significant and unavoidable.

Noise and Vibration

Noise and Vibration Threshold of Significance 3: Permanent Increase in Ambient Noise Levels

Finding: Traffic noise levels under the Future (2035) with Project traffic scenario would increase in some locations compared to Future (2035) without Project traffic noise levels due primarily to redistribution of traffic volumes that would occur from lane reduction along the Coast Highway corridor.

The City of Oceanside finds that there are no feasible changes or alterations that can be incorporated into the project, which could lessen these significant environmental effects below a level of significance and therefore, impacts related to a permanent increase in noise levels will be significant and unavoidable.

Facts in Support of Findings: Future roadway noise levels without the project were calculated along various arterial segments adjacent to the Coast Highway corridor as compared to calculated 2035 baseline traffic noise levels that would occur with implementation of the project. The Future with Project traffic scenario includes both the Complete Streets improvement and a predicted net traffic change associated with the development under the Incentive District.

Based on the Traffic Impact Analysis, contained in Appendix G of the EIR, the Future with Project scenario generates lower vehicle miles traveled per capita by approximately 11 percent when compared to the baseline Future without Project condition. This result is expected, as the project seeks to promote smart growth with strategies such as encouraging and emphasizing multimodal transportation to increase access and mobility. This would be a benefit to some roadway segments, as it would reduce traffic volumes and traffic noise levels. Traffic noise levels were reduced at 14 roadway segments with the implementation of the Complete Streets improvements.

However, some locations would experience an increase in traffic noise under the Future with Project traffic scenario. Of these increases, the only significant increase would be along the roadway segment of Michigan Avenue east of Coast Highway. In this location, the increase in traffic noise compared to the 2035 Future without project condition is predicted to be as much as 5.1 dBA CNEL, which would be a significant increase in noise levels due to the project because this increase would exceed the significance threshold of a 5 dBA CNEL increase. Because of the configuration of existing land uses in this area, these impacts could not be avoided with implementation of the project. Specifically, vehicles traveling on this roadway segment access driveways of existing residential and commercial uses along this roadway segment. Thus, the addition of sound walls or other attenuation approaches are not feasible in this location. Therefore, noise impacts would be significant and unavoidable along this roadway segment.

Noise and Vibration Threshold of Significance 4: Substantial Temporary or Periodic Increase in Ambient Noise Levels

Finding: Construction activities would increase existing ambient noise levels at noise-sensitive receptors (i.e. residences, schools, daycare centers, senior care facilities) in proximity to the construction activity. Similarly, construction activities could substantially increase ambient noise levels at noise-sensitive receptors (i.e., existing residences and schools) near future construction activity within the Incentive District.

The City of Oceanside finds that all feasible changes or alterations have been required in, or incorporated into the project, which lessen these significant environmental effects but not below a level of significance. Specifically, the project will be conditioned as follows:

MM Complete Streets NOI-1: The following field techniques shall be implemented by the City of Oceanside's construction contractor to reduce construction-related noise at nearby noise-sensitive receptors (residential uses):

- a. Unless safety provisions require otherwise, the Complete Streets construction contractor shall adjust all audible back-up alarms to the lowest volume appropriate for safety purposes (i.e., still maintaining adequate signal-to-noise ratio for alarm effectiveness). The contractor shall consider signal persons, strobe lights, or alternative safety equipment and/or processes as allowed, for reducing reliance on high-amplitude sonic alarms.
- b. The construction contractor shall place stationary noise sources at the construction site, such as generators and air compressors, away from affected noise-sensitive receivers (residential and school uses). Non-noise-producing mobile equipment, such as trailers, shall be located in the direct sound pathways between suspected major noise-producing sources and sensitive receptors.
- c. Noise-producing equipment (e.g., jackhammers and pavement breakers) shall use noise-attenuating shields, shrouds, or portable barriers or enclosures, to reduce operating noise.
- d. Line or cover hoppers, storage bins, and chutes shall include sound-deadening material (e.g., apply wood or rubber liners to metal bin impact surfaces).
- e. To the extent practicable and available, the construction contractor shall use construction equipment manufactured or modified to reduce noise and vibration emissions, such as: electric instead of diesel-powered equipment, hydraulic tools instead of pneumatic tools, and electric saws instead of air- or gasoline-driven saws.

MM Complete Streets NOI-2: Where feasible, the City of Oceanside’s contractor shall install temporary, field-erected noise barriers to block the line of sight between construction equipment and sensitive receptors prior to construction (in the Complete Streets project area these are limited to residential uses). Noise barriers could include sound blankets hanging on existing fences, or the use of freestanding portable sound walls. Noise barriers should be a minimum of 8 feet in height and continuous between the source of noise and adjacent or nearby noise-sensitive receptors. Noise barriers are most effective when placed directly adjacent to either the noise source or receptor.

Barrier construction may include, but is not necessarily limited to, using appropriately thick wooden panel walls (at least 0.5-inch-thick), as shown in Figure 3.10-2, which are tall enough to block the line of sight between the dominant construction noise source(s) and the noise-sensitive receptor. Such barriers can reduce construction noise by 5 to 15 dBA at nearby noise-sensitive receptor locations, depending on barrier height and length, and the distance between the barrier and the noise-producing equipment or activity. Alternatively, field-erected noise curtain assemblies could be installed around specific equipment sites or zones of anticipated mobile or stationary activity, resembling the sample shown in Figure 3.10-3. These techniques are most effective and practical when the construction activity noise source is stationary (e.g., auger or drill operation) and the specific source locations of noise emissions are near the ground, and barriers can be placed as close to the equipment/activity as possible. Barrier layout and other implementation details would vary by construction site.

MM Incentive District NOI-2: For individual development projects proposed under the Incentive District, the following field techniques shall be implemented by the project construction contractor to reduce construction-related noise at noise-sensitive receptors within 100 feet of construction activity:

- a. Unless safety provisions require otherwise, the Incentive District construction contractor shall adjust all audible back-up alarms to the lowest volume appropriate for safety purposes (i.e., still maintaining adequate signal-to-noise ratio for alarm effectiveness). The contractor shall consider signal persons, strobe lights, or alternative safety equipment and/or processes as allowed, for reducing reliance on high-amplitude sonic alarms.
- b. The construction contractor shall place stationary noise sources at the construction site, such as generators and air compressors, as far away as possible from affected noise-sensitive receivers

(residential and school uses). Non-noise-producing equipment, such as trailers, may be located as a sound barrier between suspected major noise-producing sources and sensitive receptors.

- c. Noise-producing equipment (e.g., jackhammers and pavement breakers) shall use noise-attenuating shields, shrouds, or portable barriers or enclosures, to reduce operating noise.
- d. Line or cover hoppers, storage bins, and chutes shall include sound-deadening material (e.g., apply wood or rubber liners to metal bin impact surfaces).
- e. To the extent practicable and available, the construction contractor shall use construction equipment manufactured or modified to reduce noise and vibration emissions, such as: electric instead of diesel-powered equipment, hydraulic tools instead of pneumatic tools, and electric saws instead of air- or gasoline-driven saws.

MM Incentive District NOI-3: Where feasible, temporary, field-erected noise barriers to block the line of sight between construction equipment and sensitive receptors shall be installed prior to construction of the individual development projects under the Incentive District. Noise barriers could include sound blankets hanging on existing fences, or freestanding portable sound walls. Noise barriers should be a minimum of 8 feet in height and continuous between the source of noise and adjacent or nearby noise-sensitive receptors. Noise barriers are most effective when placed directly adjacent to either the noise source or receptor.

Barrier construction may include, but is not necessarily limited to, using appropriately thick wooden panel walls (at least 0.5-inch thick), as shown in Figure 3.10-2, which are tall enough to block the line of sight between the dominant construction noise source(s) and the noise-sensitive receptor. Such barriers can reduce construction noise by 5 to 15 dBA at nearby noise-sensitive receptor locations, depending on barrier height and length, and the distance between the barrier and the noise-producing equipment or activity. Alternatively, field-erected noise curtain assemblies could be installed around specific equipment sites or zones of anticipated mobile or stationary activity, resembling the sample shown in Figure 3.10-3. These techniques are most effective and practical when the construction activity noise source is stationary (e.g., auger or drill operation) and the specific source locations of noise emissions are near the ground, and barriers can be placed as close to the equipment/activity as possible. Barrier layout and other implementation details would vary by construction site.

The City of Oceanside further finds that even with incorporation of MM Complete Streets NOI-1 and NOI-2 and MM Incentive District NOI-2 and NOI-3, the significant project impacts related to causing a substantial temporary or periodic increase in ambient noise levels cannot be reduced to a less than significant level. Therefore, these noise impacts are considered significant and unavoidable.

Facts in Support of Findings: Construction noise would average approximately 80 dBA L_{eq} at 100 feet from a construction activity, which would temporarily increase existing ambient noise levels of approximately 65 dBA L_{eq} at sensitive receptor locations along the project corridor (i.e., an approximate 15 dBA increase). As discussed in the Noise and Vibration Study Technical Report (ESA 2017; 2018), a substantial temporary increase in ambient noise levels is defined as a direct project-related increase of 10 dBA L_{eq} or greater (FTA 2006). Therefore, these impacts would be considered significant.

Construction noise would average approximately 80 dBA L_{eq} at 100 feet from construction activities, which would temporarily increase existing ambient noise levels of approximately 65 dBA, by approximately 15 dBA L_{eq} at existing residences located within the Incentive District. As discussed in the Noise and Vibration Study Technical Report (ESA 2017; 2018), a substantial temporary increase in ambient noise levels is defined as a direct project-related increase of 10 dBA L_{eq} or greater (FTA 2006). Therefore, these impacts would be considered significant.

Barrier material is assumed to be solid and dense enough to demonstrate acoustical transmission loss that is at least 10 dBA greater than the estimated noise level of the equipment or activity. These suggested barrier types do not represent the only ways to achieve the indicated noise reduction in dBA; they represent examples of how such noise attenuation might be attained by an implemented measure under the right conditions.

With the noise reduction achieved with the noise barriers of MM Complete Streets NOI-2, the attenuated construction noise levels at a source would be reduced by 5 to 15 dBA Leq, which would attenuate to a less than substantial increase in daytime ambient noise levels at an adjacent residential uses. However, MM Complete Streets NOI-2 (i.e., barriers) may not be feasible to implement at all locations at all times during construction activities, due to potential physical constraints at a location, which allow for line of sight between a noise source and a residence. For example, existing fences may not be tall enough or sturdy enough to support noise blankets being attached and the placement of temporary barriers could endanger construction crew members and equipment and may restrict removal of impacted materials beneath the barriers. Therefore, impacts will be significant and unavoidable with regard to a temporary substantial increase in ambient noise levels.

Similar to the Complete Streets component of the project, MM Incentive District NOI-3 may not be feasible to implement at all locations at all times during construction activities, due to potential physical constraints at a location, which do not block line of sight between a noise source and a residence. For example, existing fences may not be tall enough or sturdy enough to support noise blankets being attached and the placement of temporary barriers could endanger construction crew members and equipment. Therefore, impacts will be significant and unavoidable with regard to a temporary substantial increase in ambient noise levels.

Noise and Vibration Cumulative Threshold of Significance

Finding: The construction of the project includes the near-term construction of the Complete Streets improvements, and the construction of potential redevelopment under the Incentive District. During construction of the project, due primarily to the dense development of the project area, noise would occur near sensitive receptors, likely resulting in a substantial temporary increase in ambient noise.

Vehicular traffic associated with development within the Incentive District would generate mobile noise. Future traffic noise levels from the project and related projects along two street segment locations, Wisconsin Avenue, between Freeman Street and Ditmar Street and Washington Avenue, west of Coast Highway would be cumulatively significant.

The City of Oceanside finds that there are no feasible changes or alterations that can be incorporated into the project, which could lessen these significant environmental effects below a level of significance and therefore, impacts related to cumulative noise impacts related to roadway traffic along Wisconsin Avenue, between Freeman Street and Ditmar Street and Washington Avenue, west of Coast Highway will be significant and unavoidable.

Facts in Support of Findings: The construction of the project includes the near-term construction of the project-level Complete Streets improvements, and the construction of potential redevelopment under the

Incentive District. The street improvements will occur in specific locations with a scheduled near-term start date and expected end date. The potential development and redevelopment under the Incentive District could occur at any qualifying parcel in the commercial area of the Incentive District at any time. Because the timing or sequencing of individual projects proposed under the Incentive District cannot be determined with any certainty any quantitative analysis to ascertain the daily construction noise levels of multiple, concurrent activities would be speculative. Therefore, it is possible that the Complete Streets project component and individual development projects implemented under the Incentive District could occur simultaneously, as well as in proximity to each other.

Due primarily to the dense development of the project area, project construction noise would occur near sensitive receptors, likely resulting in a substantial temporary increase in ambient noise. Implementation of MM Complete Streets NOI-1 and NOI-2 and MM Incentive District NOI-1 through NOI-3 would reduce construction noise impacts. However, mitigation measures may not be feasible to implement at all locations at all times during construction activities, due to potential physical constraints at a location thereby allowing for line of sight between a noise source and a residence. Therefore, noise levels during construction could exceed the threshold resulting in a potentially significant and unavoidable temporary increase in ambient noise levels. Therefore, project construction noise combined with other nearby projects could in combination result in a cumulatively substantially temporary increase in the ambient noise environment in the project area. Therefore, project construction could contribute to a cumulatively considerable construction noise impact.

Vehicular traffic associated with development within the Incentive District would generate mobile noise. Future noise levels in two street segment locations, along the along Wisconsin Avenue, between Freeman Street and Ditmar Street and along Washington Avenue, west of Coast Highway would be cumulatively significant. In both locations, the project's contribution would be perceptible (greater than 3 dBA) and would exceed the 5 dBA noise significance threshold. Therefore, the project contributes considerably to the significant cumulative impacts for the Future Conditions + Project traffic noise conditions along these two street segments.

Sound walls are often used to minimize roadway noise impacts. However, due to the need for access points (for example, driveways to residences and street access to the Saint Mary Star of the Sea School), a continuous wall could not be provided and the barrier would not effectively shield the noise-sensitive uses from the roadway noise. In addition, the addition of sound walls would not be desirable, as the wall would detract from the community character and visual quality of these neighborhoods. For these reasons, the addition of continuous sound walls to reduce the significant traffic noise impacts would not be desirable or feasible. No other effective mitigation approaches are available. Therefore, the project's contribution to cumulative traffic noise impacts along Wisconsin Avenue (between Freeman Street and Ditmar Street) and Washington Avenue (west of Coast Highway) is considered significant and unavoidable.

Transportation and Traffic

Transportation and Traffic Threshold of Significance 1: Conflict with an Applicable Plan, Ordinance, or Policy Establishing Measures of Effectiveness for the Performance of the Circulation System

Finding: Based on the thresholds established by the City of Oceanside and Caltrans, under the Future Conditions + Project Scenario the following ten study intersections would operate deficiently:

4. Coast Highway & Surfrider Way – LOS F during PM peak-hours
6. Coast Highway & Pier View Way – LOS E during PM peak-hours
15. Seagaze Street & Ditmar Street – LOS E during PM peak-hours
21. Coast Highway & Wisconsin Boulevard – LOS F during PM peak-hours
27. Coast Highway & Oceanside Boulevard – LOS F during PM peak-hours
29. Coast Highway & Morse Street – LOS F during PM peak-hours
35. Coast Highway & Cassidy Street – LOS F during PM peak-hours
42. Vista Way & Ditmar Street – LOS F during PM peak-hours
52. Oceanside Boulevard & I-5 Southbound On-/Off-Ramps – LOS D during AM and PM peak-hours
56. Vista Way & I-5 Southbound On-/Off-Ramps – LOS F during PM peak-hours

The City of Oceanside finds that all feasible changes or alterations have been required in, or incorporated into the project, which lessen these significant environmental effects but not below a level of significance. Specifically, the project will be conditioned as follows:

MM Complete Streets TR-2: In order to mitigate the deficient LOS at the seven degraded study area intersections predicted under the Future Conditions + Project scenario, the City of Oceanside shall implement the following measures to improve intersection operations to an acceptable LOS. The City of Oceanside shall include the project modifications in the Complete Streets construction plans prior to the finalization of the construction plans. The improvements shall be completed either prior to or concurrent with the Complete Streets improvements. The nine mitigation measures for the eight degraded study intersections in the Future Conditions + Project scenario are in the following summary table. The Oceanside Boulevard and I-5 SB On-/Off-Ramps intersection has two specific measures to address both the AM and PM peak hours.

Location	Mitigation Measure	Additional Comments	Mitigated Conditions		Reduced to Less than Significant
			Delay (sec)	LOS	
4. Coast Hwy & Surfrider Way	Maintain Existing Traffic Signal	None	19.6	B	Yes
6. Coast Hwy & Pier View Way	Maintain Existing Traffic Signal	None	8.7	A	Yes

	Location	Mitigation Measure	Additional Comments	Mitigated Conditions		Reduced to Less than Significant
				Delay (sec)	LOS	
15	Seagaze St & Ditmar St	Convert AWSC to Traffic Signal	None	13.2	B	Yes
27	Coast Hwy & Oceanside Blvd	Maintain Existing Traffic Signal	None	47.4	D	Yes
29	Coast Hwy & Morse St	Maintain existing Traffic Signal	None	25.9	C	Yes
35	Coast Hwy & Cassidy Street	Maintain existing Traffic Signal	Implementation of this mitigation measure won't fully mitigate the 's impacts to this intersection	66.4	E	No
42	Vista Way & Ditmar St	Convert SSSC to Traffic Signal	None	18.3	B	Yes
52	Oceanside Blvd & I-5 SB On-/Off-Ramps (AM Peak-Hour)	Southbound configuration will include two left turn lanes and a shared thru-right lane with a storage length of 100 feet	None	33.9	C	Yes
52	Oceanside Blvd & I-5 SB On-/Off-Ramps (PM Peak-Hour)	Southbound configuration will include two left turn lanes and a shared thru-right lane with a storage length of 100 feet	Implementation of this mitigation measure won't fully mitigate the project's impacts to this intersection	44.2	D	No ¹

Note:

¹ Under the Future Conditions without project scenario, Intersection 52 (PM Peak-Hour) would operate at LOS C. Under the Future Conditions + Project scenario, this intersection would be degraded to LOS D, which is considered a significant impact under Caltrans guidelines. While the mitigation measure would reduce delay by 1.8 seconds, this intersection would still operate at LOS D and remain deficient.

SOURCE: IBI 2018

The City of Oceanside finds that even with incorporation of MM Complete Streets TR-2, the significant project impacts as the following four intersections cannot be lessened to a less than significant level and therefore, are considered significant and unavoidable:

- 21. Coast Highway & Wisconsin Boulevard – LOS F during PM peak-hours
- 35. Coast Highway & Cassidy Street – LOS F during PM peak-hours
- 52. Oceanside Boulevard & I-5 Southbound On-/Off-Ramps – LOS D during PM peak-hours
- 56. Vista Way & I-5 Southbound On-/Off-Ramps – LOS F during PM peak-hours

Facts in Support of Findings: While implementation of MM Complete Streets TR-2 would improve operations at seven of the ten study intersections to an acceptable LOS, the four intersections listed above [Intersections 21, 35, 52, and 56 (PM peak-hours)] would continue to operate at a deficient level even with mitigation incorporated.

Mitigation measures were evaluated to in order to improve the capacity at the Coast Highway and Wisconsin Avenue (Intersection 21). In order to improve capacity and reduce impacts to an operating condition that is less than significant under the Future Conditions + Project scenario, the capacity of the proposed single-lane roundabout would need to be increased to a two-lane roundabout. However, the mid-corridor intersection at Coast Highway and Wisconsin Avenue has limited right-of-way, which prevents the installation of a two-lane roundabout. In addition, a signalized intersection is also not a viable solution as this intersection is integral to the continuity of the Complete Streets improvements throughout the corridor. As there are no feasible mitigation measures, impacts to the intersection of Coast Highway and Wisconsin Avenue will remain significant and unavoidable under the Future Conditions + Project scenario with implementation of the project.

In order to improve impacts to Coast Highway and Cassidy Street (Intersection 35) to a better operating condition than under the Future Conditions + Project scenario, this intersection would need to maintain the existing traffic signal. However, doing so would disrupt the flow of traffic along Coast Highway due to the roundabout north of the intersection at Morse Street and immediately south of the intersection at Kelly Street. Even with maintaining the traffic signal, LOS would not be improved to an acceptable level. Furthermore, a signalized intersection is also not a viable solution as this intersection is integral to the continuity of the Complete Streets improvements throughout the corridor. For these reasons, project impacts to the intersection of Coast Highway and Cassidy Street would remain significant and unavoidable under the Future Conditions + Project scenario.

Mitigation measures were evaluated to reduce the significant impacts to Oceanside Boulevard and I-5 Southbound On-/Off-Ramps (PM Peak-Hour) (Intersection 52) under the Future Conditions + Project scenario. New through traffic lanes on Oceanside Boulevard at this location would be necessary to improve capacity. This type of improvement was determined to be infeasible due to the proximity of the roadway to the adjacent Sprinter rail tracks to the south and the proximity of the intersection to the I-5 overpass above Oceanside Boulevard. The roadway right-of-way below the freeway overpass is very constrained by surrounding development and would not accommodate roadway widening. While the intersection is forecast to operate at an unacceptable level of service per Caltrans guidelines, the intersection conditions would not cause significant queuing of vehicles on the southbound off-ramp and would not impact mainline traffic conditions on I-5. For these reasons, project impacts to the intersection of Oceanside Boulevard and I-5 Southbound On-/Off-Ramps (PM Peak-Hour) would remain significant and unavoidable under the Future Conditions + Project scenario.

In order to improve conditions to Vista Way and I-5 Southbound On-/Off-Ramps (Intersection 56) to an operating condition that is less than significant under the Future Conditions + Project scenario, lane modifications would be required to construct new through traffic lanes in either the westbound or eastbound directions on Vista Way/SR 78. The City of Oceanside determined that the addition of a westbound through lane at this location is infeasible due to the limited right-of-way available on Vista Way west of the intersection. Furthermore, with the recent road diet installed by the City of Oceanside along Vista Way east of this intersection, lane modifications would be inconsistent with the vision and goals of the City of Oceanside. Moreover, the City of Oceanside determined that the addition of an eastbound through lane is also infeasible. The configuration of the traffic lanes and bridge to the east of the intersection is not compatible with three eastbound through lanes on Vista Way. Caltrans and SANDAG have plans to reconfigure the I-5/SR 78/Vista Way interchange in the future, where the

proposed reconfiguration would address the significant traffic impact identified for the intersection at Vista Way and I-5 Southbound On-/Off-Ramp (Intersection 56). However, while this is currently in Caltrans and SANDAG's long-term plans, funding is not guaranteed with enough certainty to include the improvements in a CEQA-required future analysis scenario. Therefore, project impacts to the intersection of Vista Way and I-5 Southbound On-/Off-Ramps will remain significant and unavoidable under the Future Conditions + Project scenario.

The City of Oceanside finds that even with incorporation of MM Complete Streets TR-2, the significant project impacts as the four intersections listed above [Intersections 21, 35, 52, and 56 (PM peak-hours)] cannot be reduced to a less than significant level. Therefore, traffic impacts in the Future Conditions + Project scenario are considered significant and unavoidable

X. Findings Regarding Alternatives

CEQA Guidelines Section 15126.6 requires that an EIR consider a range of reasonable alternatives to the proposed project, or to the location of the proposed project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project. Four alternatives to the proposed project were evaluated to consider if they would reduce significant effects identified while still meeting the project objectives. As well, different alternatives were identified during the public scoping and input process based on community desirability. In addition, the No Project Alternative was also analyzed in Chapter 5 of the EIR consistent with the requirements of CEQA (CEQA Guidelines, §15126.6(e)). For a complete summary of comparative impacts of the alternatives relative to the project, see Table 5-22 in Chapter 5, Alternatives, in the EIR.

Because the project will cause significant and unavoidable environmental impacts related to Air Quality, Greenhouse Gas emissions, Noise and Vibration, and Transportation and Traffic, the City of Oceanside must consider a reasonable range of alternatives to the project, evaluating whether these alternatives could avoid or substantially lessen the unavoidable significant effects while achieving most of the objectives of the project.

No Project Alternative

As directed by Section 15126.6(a)(3)(A) of the CEQA Guidelines, when a project is the revision of an existing land use or regulatory plan, policy or ongoing operation, the No Project Alternative will be the continuation of the existing plan, policy, or operation into the future.

CEQA requires that an EIR include an analysis of a No Project Alternative. The No Project Alternative analyzes what would be expected to occur if the proposed project were not approved.

Under the No Project Alternative, the project area would remain as it is under existing conditions. The Complete Streets improvements would not be implemented and Coast Highway would continue to consist of four travel lanes. More specifically, under the No Project Alternative, no roundabouts, mid-block crosswalks, raised medians, continuous bicycle lanes, or enhanced streetscaping would be provided. The amount of public parking would remain the same as under existing conditions. Furthermore, the special management area for the Incentive District would not be established and the project area would continue to be developed and/or redeveloped using the existing land use designations from the City of Oceanside's

General Plan and the existing Zoning Ordinance. Thus, private development would continue to be allowed under the City of Oceanside's existing Municipal Code, Zoning Ordinance, and General Plan policies and regulations.

Finding: The No Project Alternative would not meet any of the project objectives.

Facts in Support of Finding: The No Project Alternative would avoid the significant and unavoidable impacts related to air quality, greenhouse gas emissions, noise and vibration, and transportation and traffic of the project. However, the No Project Alternative would result in greater impacts to cultural resources because the No Project Alternative would not implement the mitigation measures for the project that ensure the protection of cultural resources with future development.

While the No Project Alternative would reduce the majority of environmental impacts of the project, this alternative would not achieve any of the project objectives. Because neither the Complete Streets improvements nor the Incentive District would be implemented, Coast Highway would not be transformed into a "Complete Street". The roadway calming and streetscaping features would not occur and the goals of the Vision Plan would not be implemented through the creation of an optional zoning ordinance aimed at incentivizing development and redevelopment throughout the corridor. Furthermore, development would continue to occur as directed by the City of Oceanside's General Plan/Local Coastal Program and Zoning Ordinance, where the type of future development the City of Oceanside would prefer to see along Coast Highway would not be incentivized.

Therefore, for the reasons stated above, the City of Oceanside rejects the No Project Alternative on the grounds that this alternative fails to achieve any of the project objectives. The City of Oceanside finds that the No Project Alternative should not be adopted in place of the project.

Description of Alternative 1 (Four Lanes between Oceanside Boulevard and Vista Way + Incentive District)

Alternative 1 includes implementation of a modified version of the Complete Streets improvements in conjunction with the Incentive District. The modified Complete Streets improvements under this alternative would limit the Complete Streets improvements from Harbor Drive to Oceanside Boulevard. The modified Complete Streets improvements would convert Coast Highway from four travel lanes to two travel lanes, ergo, one lane of travel in each direction. Coast Highway would transition back to four travel lanes from Oceanside Boulevard to the southern boundary of the city and would provide 7 roundabouts instead of the 12 roundabouts included in the project. This alternative would remove the proposed mid-block crosswalk at the Sprinter Station and Loma Alta Creek. Alternative 1 would also provide streetscaping improvements along Coast Highway from Oceanside Boulevard to Vista Way, which include sidewalk enhancements and parkway landscaping. Additionally, under this alternative, all other components associated with the Incentive District would remain the same as the proposed project.

Alternative 1 was developed to respond to community and City of Oceanside staff recommendations to consider the comparable effects of limiting the geographic area of the Complete Streets improvements to an area closer to the business-oriented uses along the Coast Highway corridor while still implementing the Incentive District throughout the entire corridor. Under Alternative 1, the Complete Streets

improvements were determined to end at Oceanside Boulevard as that intersection was determined to be the midpoint of the Coast Highway corridor.

Finding: Alternative 1 is rejected because this project alternative would result in similar environmental impacts as the project but would not meet all of the project objectives.

Facts in Support of Finding: Alternative 1 (Four Lanes between Oceanside Boulevard and Vista Way + Incentive District) would not avoid or lessen the significant and unavoidable air quality, greenhouse gas emissions, and noise and vibration impacts of the project. However, Alternative 1 would avoid and/or lessen significant impacts to transportation and traffic compared to the project.

Under the Existing Conditions + Alternative 1 scenario, implementation of the modified Complete Streets improvements under Alternative 1 would not cause any of the study area intersections to operate deficiently. Compared to the proposed project, which would cause two intersections to operate deficiently in Existing Conditions + Project, this alternative would avoid significant traffic impacts.

Under the Future Conditions + Alternative 1 scenario, the modified Complete Streets improvements would degrade seven intersections to a deficient LOS, compared to ten intersections under the proposed project. Six of the intersections significantly impacted under Alternative 1 would also occur in the Future Conditions + Project scenario (Intersections 6, 15, 21, 42, 52, and 56); however, an additional intersection (Intersection 47 – Coast Highway & Kelly Street) would also be significantly impacted under this alternative, which differs from the project. Once mitigation specific to Alternative 1 is incorporated, Alternative 1 would reduce significant impacts at four study intersections [Intersections 6, 15, 42, 47, and 52 (AM peak hours only)] but will be unable to fully mitigate significant impacts to three intersections (Intersections 21, 52 (PM peak-hours), and 56). Because there are no feasible mitigation measures to fully reduce significant impacts at these three intersections, traffic impacts in the Future Conditions + Alternative 1 will be significant and unavoidable.

Compared to the proposed project, Alternative 1 would avoid significant impacts at four of the ten study area intersections impacted by the project prior to mitigation (Intersections 4, 27, 29, and 35), where Alternative 1 would avoid the significant and unavoidable impact to Intersection 35 that occurs with implementation of the project. Furthermore, Alternative 1 would eliminate the need for three of the mitigation measures that the proposed project would require for Intersections 4, 27, and 29. While the project would result in significant and unavoidable impacts to four intersections, Alternative 1 would only result in significant and unavoidable impacts to three intersections.

Alternative 1 would fully achieve five of the project objectives (Objectives 2.3, Goal 3 and Objectives 3.1 through 3.3) and would partially achieve seven of the project objectives (Goal 1 and Objectives 1.1 through 1.3, and Goal 2 and Objectives 2.1 and 2.2). Because Alternative 1 would limit the Complete Streets improvements between Harbor Drive and Oceanside Boulevard, this alternative would only partially achieve the objectives related to transforming Coast Highway into a “Complete Street”. While this alternative would provide streetscape improvements along Coast Highway from Oceanside Boulevard to Vista Way, which include sidewalk enhancements and parkway streetscaping, none of the roadway improvements would be implemented in South Oceanside. Without installation of the Complete Streets improvements south of Oceanside Boulevard, Alternative 1 would not aid in reducing traffic speeds, increasing roadway safety for all modes of transportation, and installing roundabouts and midblock

crosswalks in the South Oceanside community. Because the Incentive District would be implemented the same as the project, this alternative would fully achieve the project objectives related to facilitating implementation of the Vision Plan.

For the reasons stated above, the City of Oceanside rejects Alternative 1 on the grounds that this alternative fails to avoid most of the significant and unavoidable impacts of the project and fails to achieve the project objectives to the same extent as the project. Therefore, the City of Oceanside finds that the Alternative 1 should not be adopted in place of the project.

Description of Alternative 2 (Four Lanes between Morse Street and Vista Way + Incentive District)

Alternative 2 also includes implementation of a modified version of the Complete Streets improvements in conjunction with the Incentive District. The modified version of the Complete Streets improvements under this alternative would limit the Complete Streets improvements from Harbor Drive to Morse Street. The modified version of the Complete Streets improvements would convert Coast Highway from four travel lanes to two travel lanes with one lane of travel in each direction from Harbor Drive to Morse Street and then would transition back to four travel lanes from Morse Street to the southern boundary of the city. This alternative would provide 7 roundabouts instead of the 12 roundabouts included in the project. Alternative 2 would also provide streetscaping improvements along Coast Highway from Morse Street to Vista Way, which include sidewalk enhancements and parkway landscaping. Additionally, under this alternative, all other components associated with the Incentive District would remain the same as the proposed project.

Alternative 2 was developed to respond to community and City of Oceanside staff recommendations to consider the comparative effects of a second alternative that limited the Complete Streets improvements to closer to the business-oriented areas of the Coast Highway corridor while still implementing the Incentive District. The Complete Streets improvements were limited to Morse Street under this alternative so that the Complete Streets improvements extended further along the corridor but still allowed for four travel lanes in South Oceanside.

Finding: Alternative 2 is rejected because this project alternative would result in similar environmental impacts as the project but would not meet all of the project objectives.

Facts in Support of Finding: Alternative 2 (Four Lanes between Morse Street and Vista Way + Incentive District) would not avoid or lessen the significant and unavoidable air quality, greenhouse gas emissions, and noise and vibration impacts of the project. However, Alternative 2 would avoid and/or lessen significant impacts to transportation and traffic compared to the project. Under the Existing Conditions + Alternative 2 scenario, implementation of the second version of the modified Complete Streets improvements would not cause any of the study area intersections to operate deficiently. Compared to the proposed project, which would cause two intersections to operate deficiently in Existing Conditions + Project, this alternative would avoid significant traffic impacts and would reduce some delays at certain intersections.

Under the Future Conditions + Alternative 2 scenario, the modified Complete Streets improvements would degrade seven intersections to a deficient LOS, compared to ten intersections under the proposed

project. Six of the intersections significantly impacted under Alternative 2 would also occur in the Future Conditions + Project scenario (Intersections 6, 15, 21, 42, 52, and 56); however, an additional intersection (Intersection 47 – Coast Highway & Kelly Street) would also be significantly impacted under this alternative, which differs from the project. Once mitigation specific to Alternative 2 is incorporated, Alternative 2 would reduce significant impacts at four study intersections [Intersections 6, 15, 42, 47, and 52 (AM peak hours only)] but will be unable to fully mitigate significant impacts to three intersections (Intersections 21, 52 (PM peak-hours), and 56). Because there are no feasible mitigation measures to fully reduce the significant impacts at these three intersections, traffic impacts in the Future Conditions + Alternative 2 will be significant and unavoidable.

Compared to the proposed project, Alternative 2 would avoid significant impacts at four of the ten study area intersections impacted by the project prior to mitigation (Intersections 4, 27, 29, and 35), where Alternative 2 would avoid the significant and unavoidable impact to Intersection 35 that occurs with implementation of the project. Furthermore, Alternative 2 would eliminate the need for three of the mitigation measures that the proposed project would require for Intersections 4, 27, and 29. While the project would result in significant and unavoidable impacts to four intersections, Alternative 2 would only result in significant and unavoidable impacts to three intersections.

Similar to Alternative 1, Alternative 2 would fully achieve 5 of the 12 project objectives (Objectives 2.3, Goal 3 and Objectives 3.1 through 3.3) and would partially achieve seven of the 12 project objectives (Goal 1 and Objectives 1.1 through 1.3, and Goal 2 and Objectives 2.1 and 2.2). Because Alternative 2 would limit the Complete Streets improvements between Harbor Drive and Oceanside Boulevard, this alternative would only partially achieve the objectives related to transforming Coast Highway into a “Complete Street”, which accommodates all roadway users (pedestrians, bicyclists, and automobiles). While this alternative would provide streetscaping improvements along Coast Highway from Oceanside Boulevard to Vista Way, which include sidewalk enhancements and parkway streetscaping, none of the roadway improvements would be implemented in South Oceanside. Without the installation of the Complete Streets improvements in South Oceanside, this Alternative would not aid in reducing traffic speeds, increasing roadway safety for all modes of transportation, and installing roundabouts and midblock crosswalks. Because the Incentive District would be implemented the same as the project, this alternative would fully achieve the project objectives related to facilitating implementation of the Vision Plan.

For the reasons stated above, the City of Oceanside rejects Alternative 2 on the grounds that this alternative fails to avoid most of the significant and unavoidable impacts of the project and fails to achieve the project objectives to the same extent as the project. Therefore, the City of Oceanside finds that the Alternative 2 should not be adopted in place of the project.

Description of Alternative 3 (Complete Streets Improvements and Incentive District to Morse Street and Existing Conditions between Morse Street and Vista Way)

Alternative 3 includes modifying both the Complete Streets improvements and the Incentive District to extend from Harbor Drive to Morse Street, which would reduce the geographic area where the Complete Streets improvements and Incentive District would be implemented compared to the proposed project.

The modified Complete Streets improvements would convert Coast Highway from four travel lanes to two travel lanes with one lane of travel in each direction from Harbor Drive to Morse Street. Coast Highway would transition back to four travel lanes from Morse Street to the southern boundary of the city. With the reduction in the geographic area, this alternative would provide 7 roundabouts instead of the 12 roundabouts included in the project. Alternative 3 would also provide streetscaping improvements along Coast Highway from Morse Street to Vista Way, which include sidewalk enhancements and parkway landscaping. In addition, Alternative 3 would adopt the Incentive District as an optional zoning ordinance for the parcels within the modified boundaries, with the southern boundary ending at Morse Street.

Alternative 3 was developed in response to public comments in favor of considering an alternative that maintained four lanes throughout the southern portion of Coast Highway and removed the Incentive District from the South Oceanside community.

Finding: Alternative 3 is rejected because it would result in similar environmental impacts as the project, with the exception of an additional significant and unavoidable noise impact along Michigan Avenue, west of Coast Highway, in the Future Conditions + Project traffic scenario, and would not meet all of the project objectives.

Facts in Support of Finding: Alternative 3 (Complete Streets Improvements and Incentive District to Morse Street and Existing Conditions between Morse Street and Vista Way) would not avoid or lessen the significant and unavoidable air quality and greenhouse gas emissions impacts of the project. While Alternative 3 would avoid and/or lessen significant impacts to transportation and traffic compared to the project, Alternative 3 would result in greater impacts related to a permanent increase in ambient noise levels compared to the project.

Under the Future Conditions + Alternative 3 scenario, the modified Complete Streets improvements would degrade five intersections to a deficient LOS, compared to ten intersections under the proposed project. Four of the intersections significantly impacted under Alternative 3 would also occur in the Future Conditions + Project scenario (Intersections 6, 15, 21, and 52); however, an additional intersection [Intersection 24 – Wisconsin Boulevard & Ditmar Street (South)] would also be significantly impacted under this alternative, which differs from the project. Once mitigation specific to Alternative 3 is incorporated, Alternative 3 would reduce significant impacts at two study intersections [Intersections 6, 15, and 52 (AM peak hours only)] but will be unable to fully mitigate significant impacts to three intersections [Intersections 21, 24, and 52 (PM peak-hours)]. Because there are no feasible mitigation measures to fully reduce the significant impacts at these three intersections, traffic impacts in the Future Conditions + Alternative 3 will be significant and unavoidable.

Compared to the proposed project, Alternative 3 would avoid significant impacts at six of the ten study area intersections impacted by the project prior to mitigation (Intersections 4, 27, 29, 35, 42, and 56), where Alternative 3 would avoid the significant and unavoidable impact to Intersection 35 that occurs with implementation of the project. Furthermore, Alternative 3 would eliminate the need for four of the mitigation measures that the proposed project would require for Intersections 4, 27, 29, and 42. While the project would result in significant and unavoidable impacts to four intersections, Alternative 3 would only result in significant and unavoidable impacts to three intersections.

Alternative 3 would result in significant impacts related to causing a permanent increase in ambient noise levels along Michigan Avenue, east of Coast Highway, similar to the project and Michigan Avenue, west of Coast Highway, which is an additional roadway segment compared to the project. In addition, Alternative 3 would result in cumulative noise impact along Wisconsin Avenue between Freeman Street and Ditmar Street, and Washington Avenue west of Coast Highway, similar to the project. Similar to the project, Alternative 3 cannot be redesigned to reduce these significant and unavoidable impacts because of the reconfiguration of Coast Highway at these three intersections and the configuration of existing land uses in this area, which make standard noise reduction measures, such as sound walls, infeasible in these locations. Therefore, similar to the project, there are no feasible mitigation measures that could reduce the significant impacts to these four roadway segments and as such, impacts will be significant and unavoidable. Compared to the project, Alternative 3 will result in greater noise impacts due to the additional significant and unavoidable impact along Michigan Avenue, west of Coast Highway.

Alternative 3 would only be able to fully achieve three of the project objectives (Objective 2.3, Objective 3.1 and Objective 3.3). This alternative would achieve these three project objectives as it would provide new mid-block pedestrian crosswalks throughout the corridor, encourage redevelopment and continued investment within the Incentive District by providing development incentives in exchange for community benefits to enhance and revitalize the project area, and foster a built environment along Coast Highway that is pedestrian-scale and pleasurable to walk within, includes architecture that announces gateways, key intersections, and public spaces and is high-quality and provides variation and diversity. However, since Alternative 3 would limit both the Complete Streets improvements and the Incentive District between Harbor Drive and Morse Street, this alternative would only partially achieve nine of the project objectives (Goal 1/Objectives 1.1 through 1.3; Goal 2/Objectives 2.1 and 2.2; Goal 3/Objective 3.2) as this alternative would not implement the project components in South Oceanside.

For the reasons stated above, the City of Oceanside rejects Alternative 3 on the grounds that this alternative does not achieve the project objectives to the same degree as the proposed project and that it results in comparable environmental impacts as the proposed project, with the exception of an additional significant and unavoidable noise impact along Michigan Avenue, west of Coast Highway, in the Future Conditions + Project traffic scenario.

Description of Alternative 4 (Complete Streets Improvements Only, No Incentive District)

Alternative 4 includes only the Complete Streets improvement component of the proposed project and the Incentive District would not be implemented. As with the project, Alternative 4 would convert Coast Highway from four lanes to two lanes (one travel lane in each direction) for the length of the corridor, with segments of two southbound travel lanes between SR 76 and Surfrider Way, and south of Kelly Street to Eaton Street and would implement all other key elements of the Complete Streets improvements. The special management area for the Incentive District would not be established under this alternative. Growth would occur in the project area similar to current trends under existing land use regulations. Similar effects to the development and redevelopment enabled under the Incentive District could occur in the project area under existing growth regulations, but possibly not as quickly as with implementation of the Incentive District.

Finding: Alternative 4 is rejected because this project alternative would generally result in similar environmental impacts as the project and would not be able to meet all of the project objectives.

Facts in Support of Finding: Alternative 4 would provide for the implementation of the Complete Streets improvement component of the project but would not create the Incentive District. Alternative 4 would not reduce the significant and unavoidable air quality and greenhouse gas emissions impacts of the project to a less than significant level. In addition, Alternative 4 would lessen the significant and unavoidable noise and vibration impacts of the project to a less than significant level. However, while Alternative 4 would lessen the transportation and traffic environmental impacts compared to the project, the degree of the reduction to the significant future noise and traffic impacts that would occur under Alternative 4 is not precisely known since modeling was not conducted for the alternatives.⁴ Alternative 4 would continue to allow for development and redevelopment within the project area. However, because the Incentive District is not part of Alternative 4, the cultural resources mitigation measures associated with the Incentive District would not be implemented. The mitigation measures provide additional safeguards for the protection of cultural resources. For these reasons, Alternative 4 would result in a potentially significant impact to cultural resources.

Alternative 4 would only fully achieve 8 of the 12 project objectives (Goal/Objectives 1 and Goals/Objectives 2) and it would not achieve the 4 of the 12 project objectives related to the Incentive District (Goal/Objectives 3). This alternative would implement that Complete Streets improvements throughout the entire Coast Highway corridor and would implement a “Complete Street” with roundabouts and mid-block crosswalks throughout the entire Coast Highway corridor, which would increase safety for all modes of transportation through traffic calming measures, slowing traffic speeds, providing a continuous bicycle lane, and improving the pedestrian environment. However, this alternative would remove the Incentive District component and as such would not achieve any of the project objectives related to the Incentive District and would not help to facilitate the Vision Plan. Furthermore, while development would continue to occur as directed by the City of Oceanside’s General Plan/Local Coastal Program and Zoning Ordinance, the type of future development the City of Oceanside would prefer to see along Coast Highway would not be incentivized.

For the reasons stated above, the City of Oceanside rejects Alternative 4 on the grounds that this alternative fails to avoid the significant and unavoidable impacts of the project and results in an additional significant impact to cultural resources. Furthermore, the City of Oceanside rejects Alternative 4 on the grounds that this alternative fails to achieve the project objectives to the same extent as the project. Therefore, the City of Oceanside finds that the Alternative 4 should not be adopted in place of the project.

⁴ CEQA Guidelines Section 15126.6(d). Evaluation of alternatives, requires that the EIR provide sufficient information about each alternative to allow meaningful evaluation, analysis, and comparison with the project. The Guidelines do not require an equivalent level of analysis for alternatives and the project.

EXHIBIT C

Coast Highway Corridor Final Environmental Impact Report

Statement of Overriding Considerations



STATEMENT OF OVERRIDING CONSIDERATIONS FOR THE OCEANSIDE COAST HIGHWAY CORRIDOR STUDY (SCH NO. 2016051078)

As discussed in the Findings of Fact, the Final Environmental Impact Report for the Oceanside Coast Highway Corridor Study concludes that the project, even with incorporation of all feasible mitigation measures and consideration of alternatives, will nonetheless have significant and unavoidable impacts related to Air Quality, Greenhouse Gas Emissions, Noise and Vibration, and Transportation and Traffic.

As the primary purpose of the California Environmental Quality Act (CEQA) is to fully inform the decision-makers and the public as to the environmental effects of a project and to include feasible mitigation measures and alternatives to reduce any such adverse effects below a level of significance. CEQA nonetheless recognizes and authorizes the approval of projects where not all adverse impacts can be fully lessened or avoided. However, the Lead Agency must explain and justify its conclusion to approve such a project through the statement of overriding considerations setting forth the recommended project alternative's general, social, economic, policy or other public benefits which support the agency's informed conclusion to approve that alternative over another.

Under CEQA, before a project which is determined to have significant environmental effects which cannot be avoided can be approved, the public agency must consider and adopt a statement of overriding considerations pursuant to the CEQA Statutes Section 21081 and CEQA Guidelines 15043 and 15093.

The City Council hereby finds, after consideration of the Final EIR and the evidence in the record, that each of the specific overriding economic, legal, social, technological and other benefits of the project as set forth below independently and collectively outweighs the significant and unavoidable impacts. Any one of the reasons for approval cited below is sufficient to justify approval of the project. Thus, even if a court were to conclude that not every reason is supported by substantial evidence, the City Council will stand by its determination that each individual reason is sufficient.

The City Council further finds that, as part of the process of obtaining project approval, all significant effects on the environment from implementation of the project have been eliminated or substantially lessened where feasible. Furthermore, the City Council has determined that any

remaining significant effects on the environment found to be unavoidable are acceptable due to the following specific overriding economic, technical, legal, social and other considerations.

The Coast Highway Corridor project, which includes both the Complete Streets and Incentive District project elements, has the following benefits:

- In April 2009, the City Council adopted the Coast Highway Vision and Strategic Plan (Vision Plan), which outlined a vision for the Coast Highway that is based on livable communities and Smart Growth principals intended to produce vibrant and economically successful communities. The Coast Highway Corridor project provides a multi-pronged implementation strategy for the Vision Plan, including the Complete Streets improvements and the Incentive District. Adoption of the project will implement these elements of the Vision Plan and will support the development of a vibrant and economically successful Coast Highway Corridor.
- The Coast Highway Corridor is a key mixed-use corridor that provides access to the City's coastal resources, downtown district, and adjacent neighborhoods. However, the area also currently includes vacant and underutilized property. The Coast Highway project will focus public and private investment in this important corridor in order to support additional commercial activity, housing, recreational uses, and gathering spaces.
- The Coast Highway Corridor project is the product of a comprehensive public planning effort that has undergone continual refinements which has resulted in a thoughtful balance of community, business, and environmental interests.
- The Coast Highway Corridor project, in particular the Incentive District, will support the transformation of the built environment within the Coast Highway corridor by incentivizing high quality urban and architectural design, sustainable development, synergistic land uses, and enhancement of environmental resources. Specifically, the Incentive District includes form-based development standards which provide more detail, addressing features such as horizontal and vertical articulation, architectural details, fenestration, finish materials, pedestrian access, parking configuration, and the relationship between buildings and on-site open space areas. The form-based development standards in the Incentive District will not only regulate the massing of buildings but also their visual quality and their effectiveness in accommodating intended uses and activating adjacent public spaces.
- The Coast Highway Corridor project, in particular the Incentive District, will encourage the development of additional housing, public open space and parking, all of which are highly desired in the City of Oceanside. The policies of the Incentive District will also ensure this new development will result in a compatible and synergistic mix of land uses, activation of the street frontage, and an attractive and inviting streetscape.
- The Coast Highway Corridor project promotes pedestrian and bicycle access to the coastline, redevelopment and infill within already urbanized areas, expansion of visitor-

serving commercial uses, and visual enhancement of the Coast Highway corridor, all of which are desired outcomes of the City Council.

- The allowance for standalone residential use in the avenue segments of the corridor, though the Incentive District, responds to strong demand for housing in the San Diego region and the City of Oceanside's need to accommodate its regional fair share of new housing growth.
- The Coast Highway Corridor project will transform Coast Highway into a "Complete Streets" which will be more attractive to pedestrians and alternative means of transportation and will increase foot traffic along the corridor, which will ultimately increase the economic viability of existing and future businesses.
- The Coast Highway Corridor project will provide the incentives for the encouragement of redevelopment and continued investment within the area in exchange for community benefits to enhance and revitalize the area.
- The Coast Highway Corridor project will enhance the local economy and provides opportunities for future commercial development near existing businesses, transportation hubs and walkable residential areas. In addition, the project will also provide residential uses along Coast Highway to provide a new live, work, play atmosphere.
- Considering environmental effects at a programmatic level and identifying mitigation measures for future development within the Incentive District boundaries provides for comprehensive consideration of the cumulative environmental effects. As such, with the adoption of the Incentive District and the mitigation measures outlined in the Mitigation Monitoring and Reporting Program, additional environmental safeguards will be provided for the project area that would not be present without the adoption of the district-wide program.

The public and private investment in and improvement of the Coast Highway Corridor and the betterment of the quality of life for the residents of the community is one of the City's highest priorities. Having considered these benefits, the City Council finds that the benefits of the project outweigh the unavoidable adverse environmental effects, and that the adverse environmental effects are therefore acceptable.

RESOLUTION NO.

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF OCEANSIDE ADOPTING AMENDMENTS TO THE LAND USE ELEMENT AND THE CIRCULATION ELEMENT CONSISTENT WITH THE COAST HIGHWAY CORRIDOR STUDY AND THE COAST HIGHWAY VISION AND STRATEGIC PLAN

(Applicant: City of Oceanside)

WHEREAS, there was filed with this Council a verified petition on the forms prescribed by the Council requesting a General Plan Amendment under the provisions of Article 45 of the Zoning Ordinance of the City of Oceanside for the following:

amendment of the Land Use Element and Circulation Element to incorporate provisions that enable the implementation of the Coast Highway Vision and Strategic Plan, including the establishment of a Special Management Area within the Coast Highway corridor and the re-designation of Coast Highway from a four-lane collector to a two-lane collector with raised medians, roundabouts, and bicycle lanes; and

WHEREAS, Coast Highway is a key mixed-use corridor that provides access to the City's coastal resources, downtown district, and adjacent neighborhoods; and

WHEREAS, there are opportunities to improve pedestrian and bicycle facilities within the Coast Highway corridor; and

WHEREAS, the Coast Highway corridor includes vacant and underutilized property that can support additional commercial activity, housing, recreational uses, and gathering spaces; and

WHEREAS, the City's Circulation Element includes policies and protocols that promote the implementation of "complete streets" throughout Oceanside; and

WHEREAS, on April 15, 2009, the City Council adopted the Coast Highway Vision and Strategic Plan, which calls for the revitalization of the Coast Highway corridor through right-of-way improvements and updated zoning standards; and

WHEREAS, on August 21, 2013, the City Council approved a professional services agreement with IBI and ESA to prepare a Programmatic Environmental Impact Report for

1 proposed roadway improvements and updated zoning standards intended to implement the Coast
2 Highway Vision and Strategic Plan; and

3 WHEREAS, on April 13, 2016, the City Council directed staff to analyze 1) the
4 reconfiguration of Coast Highway from a four-lane collector to a two-lane collector with new
5 raised medians, roundabouts, and bicycle lanes, and 2) the implementation of zoning incentives to
6 promote new mixed-use and residential development within the Coast Highway corridor; and

7 WHEREAS, a Programmatic Environmental Impact Report analyzing the above-noted
8 actions was circulated for public review between July 13, 2017 and August 28, 2017; and

9 WHEREAS, a revised Programmatic Environmental Impact Report with additional
10 analysis of potential traffic, noise, and air quality/GHG impacts was circulated for public review
11 between November 14, 2018 and January 14, 2019; and

12 WHEREAS, the proposed project requires amendment of the City's Land Use Element and
13 Circulation Element; and

14 WHEREAS, on May 22, 2019, the Planning Commission reviewed and proposed
15 amendments to the Land Use Element and Circulation Element and voted 7-0 to recommend City
16 Council adoption of said amendments; and

17 WHEREAS, the City Council, after giving the required notice, did on the 14th day of
18 August, 2019 conduct a duly advertised public hearing as prescribed by law to consider the
19 proposed project; and

20 WHEREAS, pursuant to the California Environmental Act of 1970, the City Council finds
21 that an Environmental Impact Report has been prepared in conformance with the California
22 Environmental Quality Act (CEQA); and

23 WHEREAS, the City Council reviewed and considered information in this document prior
24 to making a decision on the project; and

25 WHEREAS, the documents and other material constituting the record of proceedings upon
26 which the decision is based will be maintained by the City of Oceanside City Clerk's Office, 300
27 North Coast Highway, Oceanside, California 92054; and

28 WHEREAS, studies and investigations made by the City Council and in its behalf reveal
the following facts:

1 FINDINGS:

2 For the General Plan Amendment:

- 3 1. The proposed amendments to the Circulation Element would enable complete street
4 improvements to Coast Highway that accord with Circulation Element policies that call for
5 a multimodal transportation network within the City of Oceanside.
- 6 2. The proposed amendments to the Circulation Element would allow for the installation of
7 enhanced traffic safety features on Coast Highway, improving the corridor for motorists,
8 pedestrians, and bicyclists.
- 9 3. The proposed amendments to the Circulation Element would facilitate visual enhancement
10 of the Coast Highway corridor, providing for upgraded paving, additional landscaping,
11 wayfinding elements, historical markers, and other aesthetic improvements.
- 12 4. The proposed amendments to the Land Use Element would provide for updated zoning
13 standards that incentivize the revitalization of the Coast Highway corridor as promulgated
14 by the Coast Highway Vision and Strategic Plan.
- 15 5. The proposed amendments to the Land Use Element would facilitate the expansion of the
16 City's housing stock, thereby contributing to the availability of housing in the San Diego
17 region.
- 18 6. The proposed amendments to the Land Use Element would promote synergies between
19 commercial and residential land uses and thereby contribute to the expansion of
20 commercial activity within the Coast Highway corridor.
- 21 7. Together, the proposed amendments to the Circulation Element and Land Use Element
22 reflect a coordinated planning effort to align mobility improvements with appropriate land
23 use and development patterns.

24 NOW, THEREFORE, the City Council of the City of Oceanside does resolve as follows:

25 SECTION 1. General Plan Amendment (GPA16-00001) is approved.

26 Notice is hereby given that the time within which judicial review must be sought on this
27 decision is governed by Govt. Code Section 65009(c)(1)(A).

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PASSED AND ADOPTED by the City Council of the City of Oceanside, California,
this 14th day of August, 2019, by the following vote:


- AYES:
- NAYS:
- ABSENT:
- ABSTAIN:

MAYOR OF THE CITY OF OCEANSIDE

ATTEST:

APPROVED AS TO FORM:

City Clerk



City Attorney

EXHIBIT A

General Plan Amendment (GPA16-00001)

Amendments to the Circulation Element (J-1 of FEIR Appendix J)

Amendments to the Land Use Element (J-2 of FEIR Appendix J)

**J-1 2012 Circulation
Element Text
Amendments**

3. MASTER TRANSPORTATION ROADWAY PLAN

The Master Transportation Roadway Plan chapter focuses on providing the guidelines to provide a network of roadways throughout the City which form the transportation network. This is important since the street system is used for vehicular, bicycle, transit, pedestrian, and goods movement. The City of Oceanside strives to create a network of complete streets that creates an environment for all users of the system. The Circulation Element and this Master Transportation Roadway Plan chapter are designed to effectively promote policies and guidelines that support the various forms of transportation available in the City of Oceanside. This chapter primarily focuses on mobility by use of the automobile; however, the design guidelines and policies for a multimodal network of complete streets are considered throughout the chapter.

The information contained within this chapter is intended to encourage design standards that promote efficiency and safety of the circulation network.

3.1 Policies and Implementation Strategies

GOAL 1: *A transportation network that supports safe and efficient travel for all modes of transportation.*

GOAL 2: *A transportation network that is designed to accommodate the existing and future growth of the City of Oceanside.*

Level of Service and Design Standards

OBJECTIVE:

- i. Aim for an acceptable Level of Service (LOS) D or better on all Circulation Element roadways on an average daily basis and at intersections during the AM and PM peak periods.
- ii. Employ Vehicle Miles Traveled (VMT) as the primary metric for determining the traffic impacts for new development projects located within the Smart Growth transit corridors.
- iii. Ensure that all streets within the City achieve the City's mobility goals and design standards as highlighted throughout this chapter.

POLICIES:

- 3.1 In order to achieve the level of service goals, the City shall develop and institute a long-range funding program in which new land development shall bear its share of the associated costs and improvement requirements. Where existing deficiencies occur, the

- Policies and Implementation Strategies
- Roadway Classifications
- Existing Circulation System
- Regional Modeling Process
- Roadway Design Standards
- LOS Analysis Methodology
- Proposed 2030 Master Transportation Plan
- Recommended Overriding Considerations
- Traffic Impact Study Guidelines

- 3.19** For development within the Major Thoroughfare and Traffic Signal Fee area established by the City Council, the City shall collect the Major Thoroughfare Fees as required by Ordinance No. 80-30, the Traffic Signal Fees as required by Ordinance No. 87-19, and the latest City Council resolutions setting such fees.
- 3.20** If the location and traffic generation of a proposed development will result in congestion on major streets or failure to meet the LOS D threshold, or if it creates safety hazards, the proposed development shall be required to make necessary off-site improvements. Such improvements may be eligible for reimbursement from collected impact fees. In some cases, the development may have to wait until financing for required off-site improvements is available. In other cases where development would result in unavoidable impacts, the appropriate findings of overriding consideration will be required to allow temporary undesirable levels of service.
- 3.21** Land Use development near the designated transit corridors or in VMT-efficient (Vehicle Miles Traveled) areas shall be presumed to cause a less than significant transportation impact. Transit, active transportation and rehabilitation projects that do not add motor vehicle capacity should also be presumed to cause a less than significant impact.
- 3.213.22** The City shall require that those responsible for street improvements replant, replace, or install new landscaping pursuant to existing City policy along all new roadways or on those that have been redesigned and reconstructed.
- 3.223.23** Prior to approving any street widening project, the City shall explore all alternatives to adding additional lanes or acquiring additional right-of-way.

3.2 Roadway Classifications

Roadway classification is the process by which streets and highways are grouped into classes according to the type of service they are intended to provide. Fundamental to this process is the recognition that individual streets and highways do not operate independently. Rather, most travel involves movement through an integrated network of roads. It is the City's responsibility to plan, design, and implement a street system that recognizes the importance of the use and function of each hierarchical roadway classification.

The City of Oceanside recognizes the importance of the use and function of each street classification. The class of each road has a certain set of design guidelines and intended functions that are described below.

Expressway – An Expressway is designed to provide express direct travel through a city with several lanes of travel in each direction. It is a multilane roadway that is often a divided highway for through traffic with fully controlled access to intersections and with possible grade separations at most

Prime Arterial – A Prime Arterial is designed to provide regional, sub-regional, and intra-city travel. It includes high design standards with six lanes of travel, raised and landscaped medians, 8-foot shoulders, highly restricted direct access, and on-street parking is not allowed. The cross section for a prime arterial is 104/124 feet (curb-to-curb/total right-of-way width).

Major Arterial – A Major Arterial is designed to provide intra-city and sub-regional service. Direct access is allowed, but selectively restricted to assure proper function of the roadway. Typical design standards include the provision of four or six lanes of travel with a raised and landscaped median, 8-foot shoulders for emergency parking and bike lanes, and left-turn lanes are typically protected along the roadway. The cross section of a major arterial is 80/100 feet or 104/124 feet (curb-to-curb/total right-of-way width).

Secondary Collector – A Secondary Collector is designed for intra-city travel as a link between arterial and collector roadways. It frequently provides direct access to abutting properties; however, that is not its primary purpose. The typical design features include the provision of four travel lanes with a center two-way left-turn lane and includes bike lanes or four lanes with left-turn pockets without a raised median. The cross section of a secondary collector is 64/84 (curb-to-curb/total right-of-way width) with a two-way left-turn lane and 54/74, 60/80 feet without a two-way left-turn lane.

Collector Street – A Collector Street is designed to connect local streets with the adjacent arterial street network. The design standards typically include the provision for two travel lanes and on-street parking is allowed, except in specific locations where parking is removed to provide turn lanes at intersections. Collector streets frequently provide direct access to abutting properties, although the desire is to limit access where possible to reduce conflict. Collectors are generally two lanes with or without a center two-way left-turn lane. The cross section of a collector is 50/70 (curb-to-curb/total right-of-way width) with a two-way left-turn lane and 40/60 or 50/70 feet without a two-way left-turn lane. A Collector can also be a two-lane one-way street, such as the one-way couplet on Mission Avenue between Cleveland Street and Clementine Street with Seagaze Drive. A Collector Street may also be separated by a median with commercial land uses fronting or at an entry way to a residential development. A Collector that is a two-lane one-way street generally has parking on both sides of the street.

Local Street – A Local Street is designed to provide direct access to abutting properties and to provide connection between neighborhood streets and the collector street network. The Local Street may be discontinuous to discourage through trips. The typical design standards include the provision for two travel lanes, parking lanes on both sides of the street, and direct driveway access. The cross section of a local street is 36/56 or 40/60 (curb-to-curb/total right-of-way width).

All major elements of the circulation system have been assigned to one of these classifications to aid in understanding the design configuration and right-of-way needs for any segment of the system.

3.6 LOS Analysis Methodology

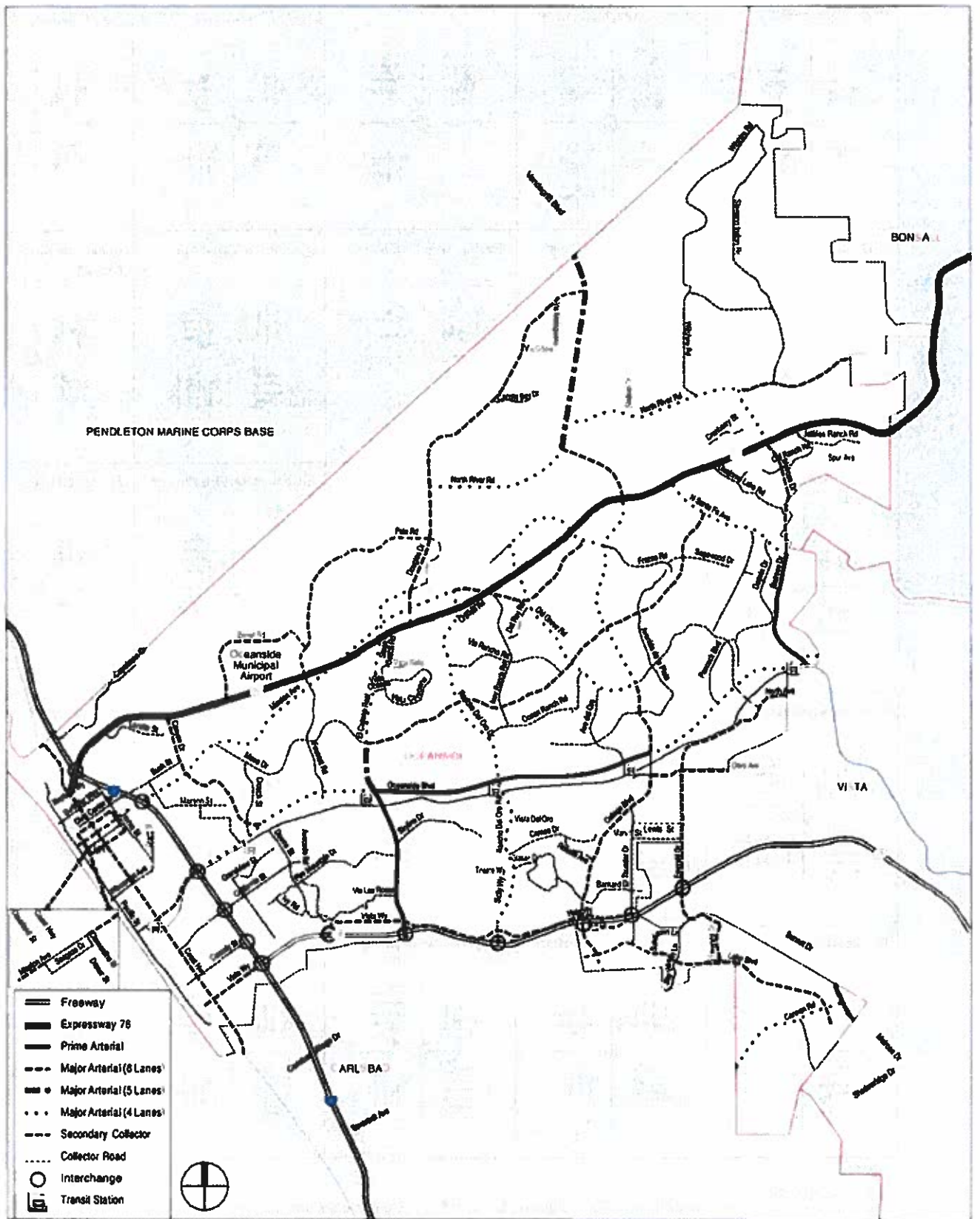
The efficiency of traffic intersection operations and available and utilized capacity of roadways are measured in terms of Level of Service (LOS). The LOS refers to the quality of traffic flow along roadways and at intersections. Evaluation of roadways and intersections involves the assignment of grades from A to F, with LOS A representing the lowest level of congestion, and LOS F representing extremely crowded and restricted operations. Each letter grade corresponds to a range of volume to capacity (V/C) values. The values for roadways are presented as volume to capacity ratios or vehicle demand divided by the roadway capacity. Therefore, as the ratio approaches a 1.00 capacity, the roadway approaches LOS F.

Senate Bill 743 (SB 743) requires public agencies in the State of California to change how transportation impacts are assessed under the California Environmental Quality Act (CEQA), identifying the use of Vehicle Miles Traveled (VMT) as the primary metric for determining the transportation impact of new development projects. Because new metrics and guidelines in response to SB 743 have not yet been adopted by the State, the VMT analysis described in this Circulation Element is for reference purposes. When a final VMT methodology is adopted by the State, the City of Oceanside will implement the new VMT metric in Smart Growth transit corridors for new development projects.

3.6.1 Freeways and Ramps

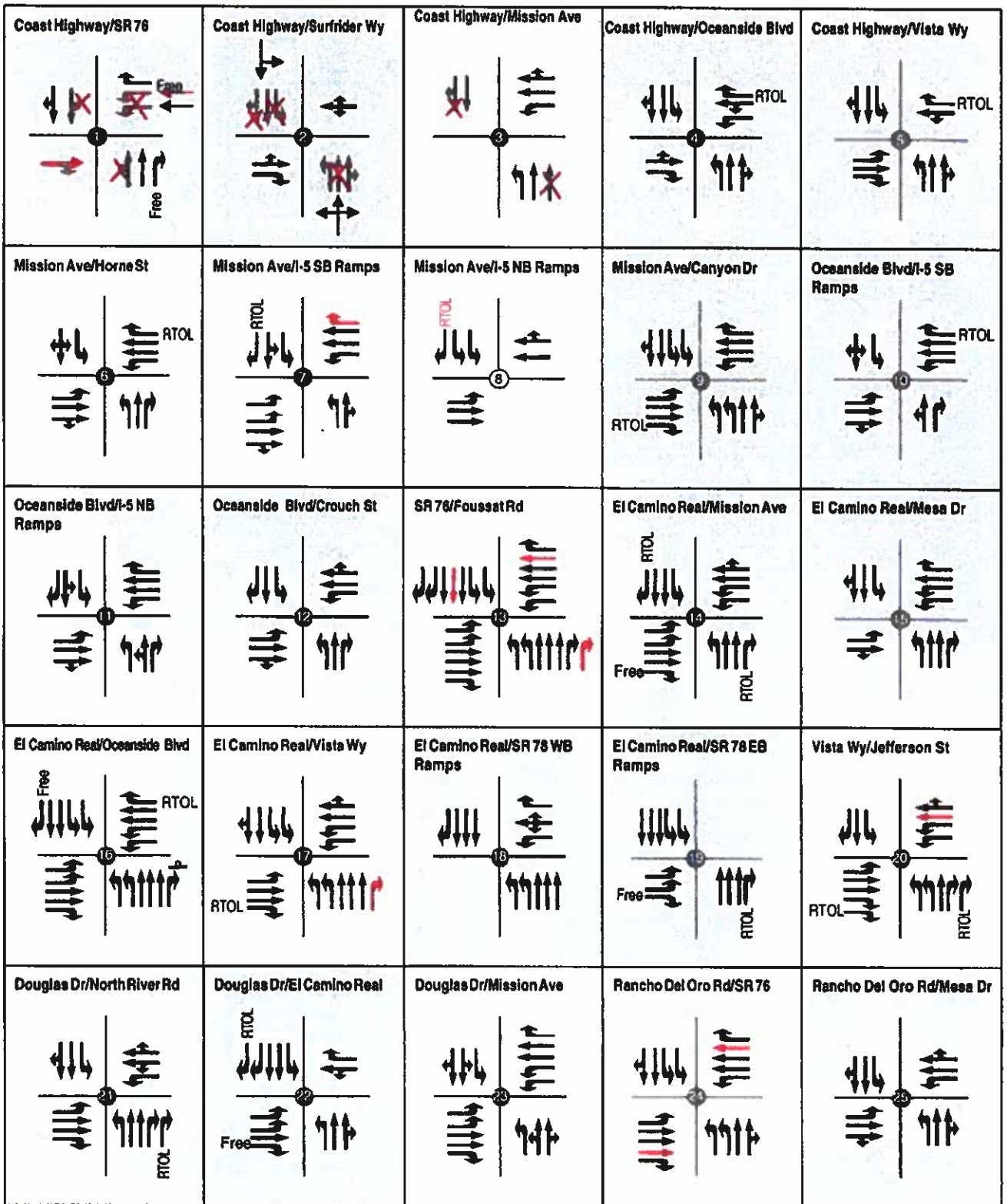
Freeways and associated interchanges must be evaluated since all freeways are on the Congestion Management Program system. All signalized intersections of freeway ramps with arterials should be evaluated using the Highway Capacity Manual (HCM) signalized intersection operational method (see section 3.6.3). For diamond interchanges, the timing and phasing of the two signals must be coordinated to ensure queue clearances. Signal timing sheets should be requested from Caltrans or the City of Oceanside. Freeways impacted by a proposed project should be evaluated for significant impacts. Level of service (generally used by Caltrans) for freeways are categorized in Table 3-2.

If ramp metering is present at a freeway interchange, the effects of the metering should be analyzed. The analysis input and output for ramp metering is explained in the Traffic Impact Study (TIS) component guidelines which are included in Appendix G. In addition, the thresholds for a significant impact to ramps or freeways are included in Appendix G.



Not to Scale





LEGEND

NS Street/EW Street



● Study Intersection - Signalized

Study Intersection Number

↔ Lane Geometry

○ Study Intersection - Unsignalized

RTOL Right-turn overlap

⊥ Stop Sign Control

Unshaded intersection and red coloring indicates a geometric change from the Existing intersection geometry



3.7.1 Unacceptable LOS Roadway Segments

There are several segments in the proposed 2030 Master Transportation Roadway Plan that do not operate at an acceptable LOS D or better and the mitigation measures to improve the operations to an acceptable level of service are not considered feasible. Figure 3.7 (page 38) depicts the 2030 Master Transportation Roadway Plan traffic volumes. Figure 3.8 (page 39) depicts the level of service operations for the roadway network for the 2030 Master Transportation Roadway Plan. As forecast traffic volumes build on the major corridors such as Coast Highway, College Boulevard, El Camino Real and Oceanside Boulevard, traffic should be managed through the Transportation Management Center (TMC). This will not provide full mitigation but simply improve overall traffic flow along these segments. The potential mitigation measures necessary to bring these segments to an acceptable LOS D or better are not always feasible or do not work towards achieving the overall long-term goals for the City of Oceanside. However, it is noted what would be required to fully mitigate these segments. The following are City of Oceanside roadway segments that do not operate at an acceptable LOS D or better under the proposed 2030 Master Transportation Roadway Plan.

Coast Highway between Harbor Drive and Civic Center Drive, Wisconsin Avenue and Oceanside Boulevard and Morse Street.

- Capacity: Four-lane secondary collector with LOS E capacity of 25,000 19,000 (see Table 3-3 for roadway capacities)
- LOS: Forecast 23,600 ADT volumes, LOS E, V/C ratio 0.94
- Mitigation: ~~Widening to a four-lane secondary collector with a center two-way left-turn lane would accommodate forecast traffic volumes~~ Convert to one lane in each direction with roundabouts and bicycle lanes to provide for other mode choices along Coast Highway, such as transit, bicycle and walking.

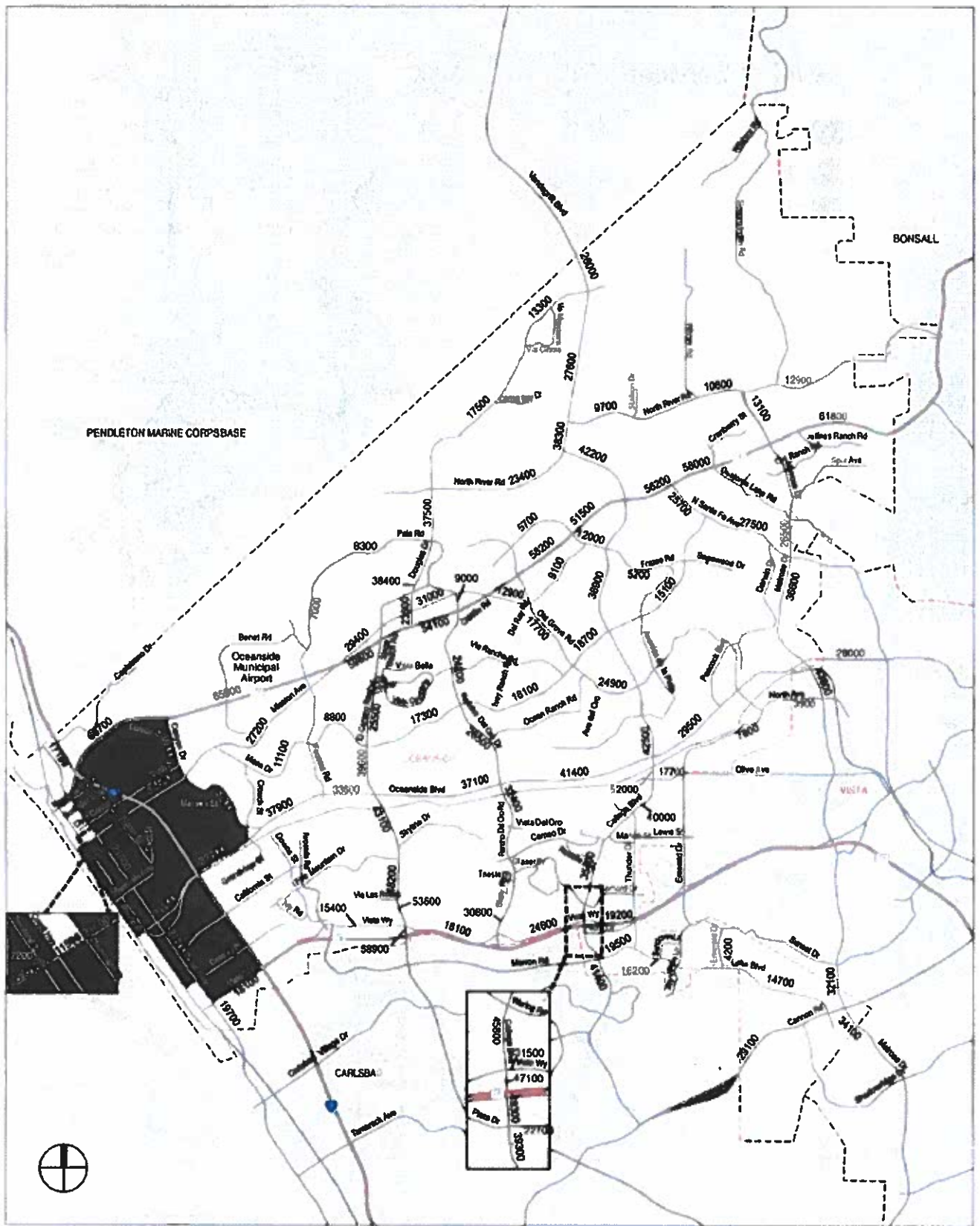
College Boulevard between SR76 and Mesa Drive, Oceanside Boulevard and Olive Drive, Waring Road and Plaza Drive, and Lake Boulevard and Southern City Limits

- Capacity: Four-lane major between SR76 and Mesa Drive, six-lane major between Oceanside Boulevard and Olive Drive, and Waring Road and Plaza Drive, and four-lane major between Lake Boulevard and the Southern City Limits
- Capacity: Four-lane major LOS E capacity of 40,000 and six-lane major LOS E capacity of 50,000
- LOS: Forecast between 36,300-52,000 ADT volumes, LOS E and F, V/C ratio 0.91-1.04
- Mitigation: Widening the four-lane major sections (where there is an impact) to a six-lane major or widening the six-lane major sections (where there is an impact) to a six-lane prime arterial would accommodate forecast traffic volumes; however, residents on certain sections of College Boulevard would be impacted by widening this corridor.

3.7.2 Unacceptable LOS Intersections

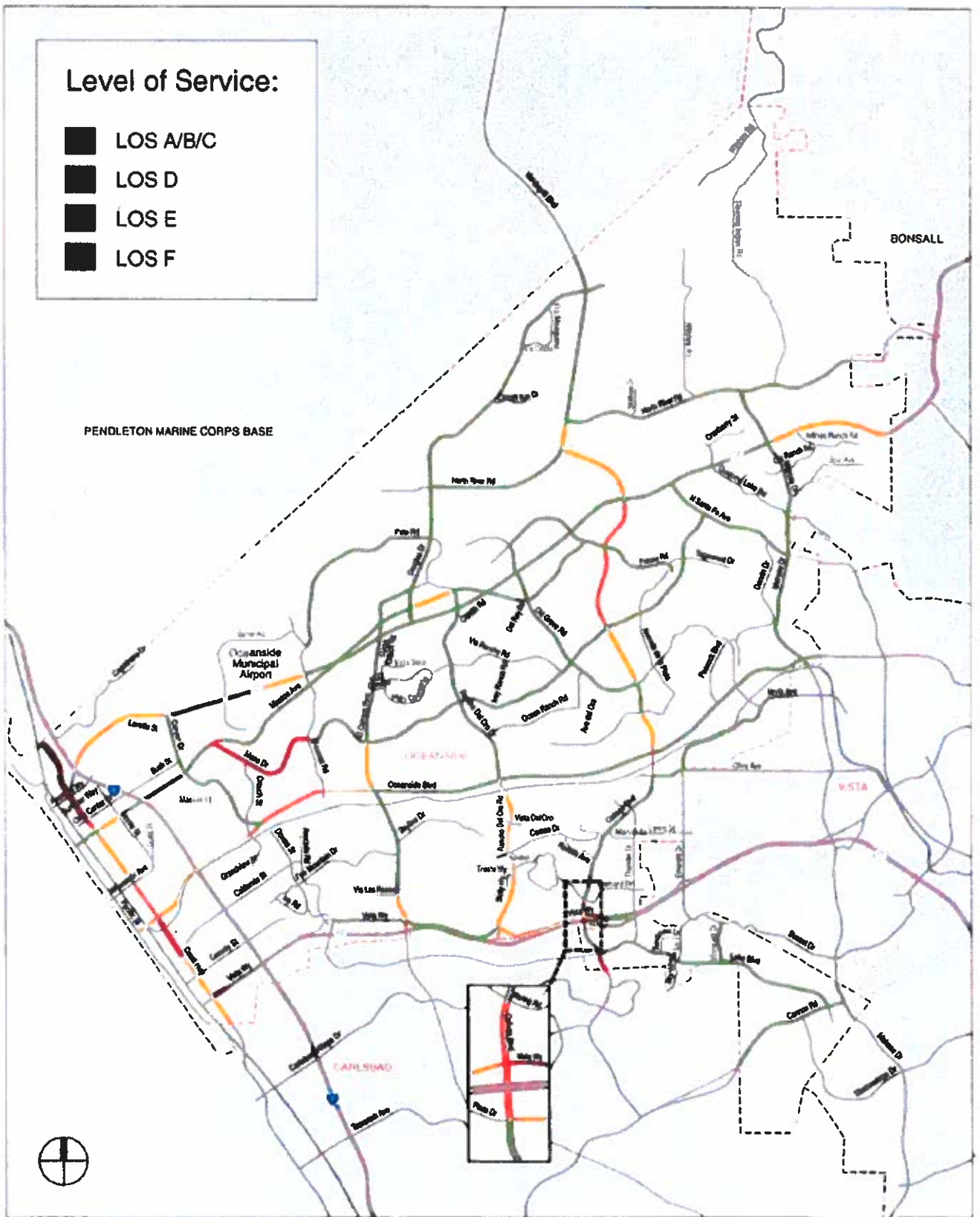
In addition, there are several key intersections in the proposed 2030 Master Transportation Roadway Plan that do not operate at an acceptable level of service during peak hours. Figure 3.9 (page 40) depicts the level of service operations for the key intersections for the proposed 2030 Master Transportation Roadway Plan. Below is a list of intersections that operate at LOS E or F during the AM or PM peak hours. The potential mitigation measures for the intersections that are proposed to operate at LOS E or F include adding an additional thru, left, or right-turn lanes to the necessary leg of the intersection. The intersection level of service, delay, and list of potential mitigation measures for these intersections are included in Appendix F.

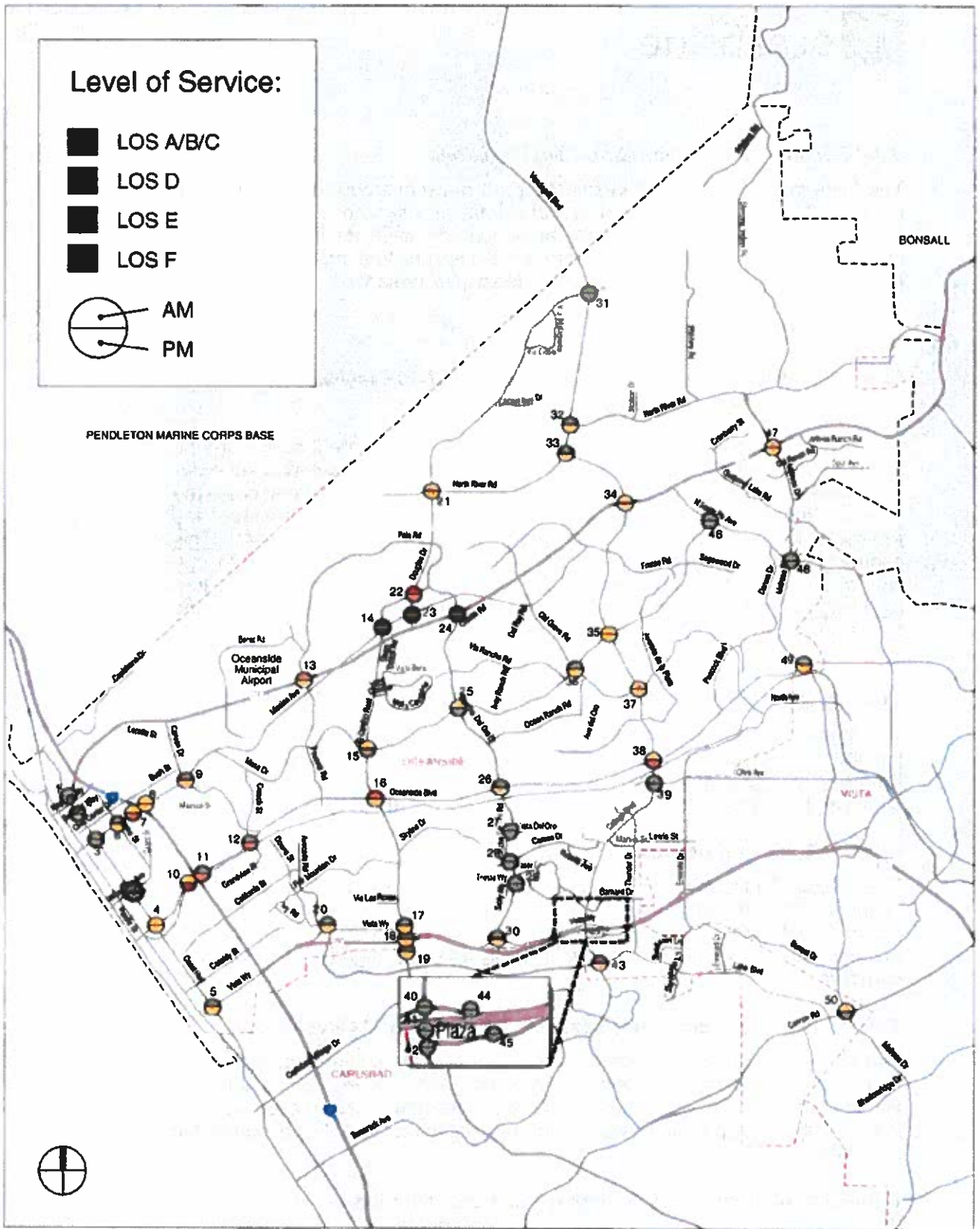
- Mission Avenue and I-5 Southbound Ramps (PM peak hour)
- Oceanside Boulevard and I-5 Northbound & Southbound Ramps ((PM peak hour)
- Oceanside Boulevard and Crouch Street (PM peak hour)
- SR76 and Foussat Road (AM peak hour)
- El Camino Real and Oceanside Boulevard (AM peak hour)
- Douglas Drive and El Camino Real (AM and PM peak hours)
- College Boulevard and Oceanside Boulevard (PM peak hour)
- Coast Highway at Wisconsin Avenue (PM peak hour)



2030 Master Transportation Roadway Plan
Traffic Volumes

Figure 3.7





3.7.3 Recommended Overriding Considerations

The following segments and intersection are recommended for consideration by City Council for an overriding consideration with the adoption of this Circulation Element. The mitigation measures identified for these segments have significant land use or environmental impacts that prohibit a feasible mitigation to bring the facility to an acceptable level of service.

Roadway Segments:

Coast Highway between ~~Wisconsin Avenue and Oceanside Boulevard~~ Harbor Drive to Civic Center Drive

The Master Transportation Roadway Plan shows this segment as a four-lane secondary collector. ~~It would require a four-lane 64/34 secondary collector designation without the existing on-street parking to accommodate the forecast traffic volumes. The new TMC can be used to manage the transportation system through adaptive signals on congested corridors. This segment of Coast Highway will be converted to a complete Street with one lane in each direction separated by a center median island/two-way left turn lane with roundabouts at key intersection locations to facilitate continuous and slow traffic flows and provide for other mode choices.~~

Coast Highway between Oceanside Boulevard and Morse Street

The Master Transportation Roadway Plan shows this segment as a four-lane secondary collector with a two-way left turn lane. This segment of Coast Highway will be converted to a Complete Street with one lane in each direction, two-way left turn lane, bicycle lanes and on-street parking.

College Boulevard between SR76 and Mesa Drive

The Master Transportation Roadway Plan shows this segment as a four-lane major arterial. It would require a six-lane major arterial designation to accommodate the forecast traffic volumes. The new TMC can be used to manage the transportation system through adaptive signals on congested corridors.

College Boulevard between Oceanside Boulevard and Olive Drive

The Master Transportation Roadway Plan shows this segment as a six-lane major arterial. It would require a six-lane prime arterial designation to accommodate the forecast traffic volumes. The new TMC can be used to manage the transportation system through adaptive signals on congested corridors.

College Boulevard between Waring Road and Vista Way

The Master Transportation Roadway Plan shows this segment as a six-lane major arterial. It would require a six-lane prime arterial designation to accommodate the forecast traffic volumes. The new TMC can be used to manage the transportation system through adaptive signals on congested

College Boulevard between Lake Boulevard and the Southern City Limits

The Master Transportation Roadway Plan shows this segment as a four-lane major arterial. It would require a six-lane major arterial designation to accommodate the forecast traffic volumes. The new TMC can be used to manage the transportation system through adaptive signals on congested corridors.

El Camino Real between Vista Way and SR78

The Master Transportation Roadway Plan shows this segment as a six-lane prime arterial. It would require an eight-lane arterial designation to accommodate the forecast traffic volumes on this segment. However, there are land use constraints due to the proximity of existing development that prohibit widening El Camino Real to eight lanes. This segment had a previous overriding consideration from the 1995 Circulation Element. It is recommended that an overriding consideration be adopted for this segment again for this update of the Circulation Element.

Mesa Drive between Mission Avenue and Foussat Road

The Master Transportation Roadway Plan shows this segment as a two-lane collector. It would require widening it to a 50/70 collector street to accommodate forecast traffic volumes.

Oceanside Boulevard between Crouch Street and Foussat Road

The Master Transportation Roadway Plan shows this segment as a four-lane major arterial. It would require a six-lane major arterial designation to accommodate forecast traffic volumes. The new TMC can be used to manage the transportation system through adaptive signals on congested corridors.

Vista Way between College Boulevard and SR78 Ramps

The Master Transportation Roadway Plan shows this segment as a four-lane secondary collector. It would require a four-lane major arterial designation to fully accommodate forecast traffic volumes. However, improvements to the College Boulevard and Vista Way intersection in the form of a westbound dedicated right-turn lane and lengthening the westbound left-turn lanes storage would improve peak hour operations on this segment. Therefore, the westbound improvements at College Boulevard/Vista Way should be implemented.

Vista Way between Coast Highway and Ditmar Street

The Master Transportation Roadway Plan shows this segment as a four-lane secondary collector with a two-way left turn lane. A four-lane secondary collector with two-way left turn lane would be required to accommodate the forecast traffic volumes. This segment of Vista Way has existing residential dwelling units that front Vista Way. This is a history of speeding and safety complaints from the residents. The City reduced the number of lanes on this segment of Vista Way to help slow traffic speeds and improve safety conditions.

adaptive signal controls will fully mitigate this intersection. It would require providing two eastbound right-turn lanes to fully mitigate this intersection.

3.8 Traffic Impact Studies

3.8.1 Traffic Impact Study Guidelines

The SANTEC/ITE Guidelines for Traffic Impact Studies (TIS) in the San Diego Region, March 2000 (or most recent version) shall be followed within the City of Oceanside for any proposed project that would have an impact on the City's roadway network. A TIS can also be required based on the discretion of the City. Early consultation between the developer and the City is required to establish the base study parameters, assumptions, and analysis methodologies for the TIS. The City of Oceanside has a list of required components for TIS reports. All TIS reports should follow the required TIS components guidelines which are included in Appendix G. A TIS can be performed by any consultant on the City approved Traffic Engineering Consultants list to complete development traffic impact studies.

As recommended in the SANTEC/ITE Guidelines, the use of the most recent version of the SANDAG Trip Generation rates should be used or the rates from ITE's latest Trip Generation manual. For smart growth/mixed-use development areas, SANDAG has developed a Trip Generation for Smart Growth: Planning Tools for the San Diego Region to identify trip generation rates associated with smart growth developments. This new method of applying Smart Growth trip generation rates is intended to supplement data in the San Diego Traffic Generators Manual, published by SANDAG in 2000, and the accompanying Not-so-Brief-Guide to Trip Generation, published by SANDAG in 2002. The most recent version of the SANDAG Smart Growth or Trip Generation rates should be used.

In addition, Senate Bill 743 (SB743) requires public agencies in the State of California to change how transportation impacts are assessed under the California Environmental Quality Act (CEQA), identifying the use of Vehicle Miles Traveled (VMT) as the primary metric for determining the transportation impact for new development projects. Because new metrics and guidelines in response to SB 743 have not yet been adopted by the State, the VMT discussion in this Circulation Element is presented for reference purposes only. Traditional intersection and roadway segment levels of service analysis will remain the current metric used by the City of Oceanside to determine transportation impacts under CEQA until the new metrics and guidelines are developed and adopted in the San Diego region through SANDAG.

The Governor's Office of Planning and Research (OPR) released the Revised Proposal on Updates to the CEQA Guidelines of Evaluating Transportation Impacts in CEQA in January 2016. This document outlines OPR's recommendations regarding methodology for conducting VMT analysis and establishing thresholds for determining significant transportation impacts as part of CEQA analysis for new projects. The recommendations include:

- Vehicle Miles Traveled is the primary metric for determining

transportation impacts across the State.

- Land use development near transit or in VMT-efficient areas should be presumed to cause a less than significant transportation impact.
- Transit, active transportation, and rehabilitation project that do not add motor vehicle capacity should also be presumed to cause a less than significant impact.
- Implementation should be phased over time.

Implementation of the new VMT metric in the City of Oceanside will be limited to mixed-use transit corridors. The VMT analysis will be conducted using the SANDAG regional travel model, which considers a variety of factors related to the land use and transportation condition to determine mode of travel choice and VMT. The traditional intersection and roadway level of service analysis remain the current metric until the State releases final guidelines on how VMT is to be modeled.

3.8.2 Traffic Analysis Performance Criteria

The City of Oceanside's minimum acceptable level of service for all roadways is LOS D. The values associated with the LOS D minimum for the different roadway classifications are defined in Table 3-3 (page 27). The minimum acceptable level of service for intersections during the peak periods is LOS D. The values associated with LOS D for intersections during the peak periods were previously defined in Tables 3-4 and 3-5 (pages 28 & 29). These level of service standards are considered acceptable within the San Diego region and have been used in the analysis for the Circulation Element.

3.8.3 Mitigation Measures

Any proposed development project that causes a street segment or intersection to operate worse than LOS D is a significant project impact. If a segment or intersection operates at LOS E or F under pre-project conditions, a significant impact is determined as outlined in the most recent version of the SANTEC/ITE Guidelines. The developer shall propose, prepare and provide feasible mitigation measure(s) for the City to review that would improve the impacted location(s) to an acceptable LOS. Mitigation measures should be

used to construct on- and off-site transportation infrastructure improvements and dedicate right-of-way connected to impacts resulting from new development. The City also may require a developer to provide improvements to pedestrian and/or bicycle facilities as part of the project's mitigation measures. Chapters 6 and 7 focus on the Pedestrian and Bicycle Facility networks in the City of Oceanside.

Any proposed development project that affects a street segment that already operates, or is projected to operate worse than LOS D, regardless of peak hour analysis, the developer shall propose, prepare and provide mitigation measure(s) for the City to review. If there are no feasible mitigation measures that would fully mitigate traffic impacts, the developer shall propose, prepare and provide various mitigation measures, such as Traffic Management Center tools and resources, which may not include physical improvements to the impacted facility. Where various mitigation measures have been prepared, agreed upon by the City, and will be implemented, yet are not sufficient to fully mitigate the traffic impacts, then LOS E during peak hour periods will be considered acceptable. A project's fair share contributions may also be considered by the City for predetermined project improvements (e.g. TMC, adaptive signals) in lieu of prepared and implemented mitigation measures.

3.9 Additional Recommended Transportation Network Guidelines

The following recommended guidelines are for action by the City:

1. Adopt and implement new VMT guidelines from the State Office of Planning and Research (OPR) and regional travel demand modeling for VMT analysis within mix-use transit corridors.
- 1-2. ___ Develop and implement a Traffic Engineering Consultant on-call list for potential developers to utilize for traffic impact studies.
- 2-3. ___ Implement the proposed transportation network and street classification standards as shown on Figure 3.5 (page 32).
- 3-4. ___ As more precise intersection data becomes available, incorporate special design treatments for mitigating potentially unacceptable levels of service at those intersections identified in Section 3.7.2 (page 37) and included in Appendix F.
- 4-5. ___ Continue to impose the City's Thoroughfare Fee and Traffic Signal Fee Ordinances to fund improvements necessary to maintain acceptable levels of service.
- 5-6. ___ Update and revise the City's Thoroughfare Fee and Traffic Signal Fee Ordinances periodically to include road segment and intersection improvements as necessary to maintain acceptable LOS, and make any necessary adjustments in the amount of fees to be imposed.

daily. iCommute offers assistance and tools to commuters and employers through various services such as carpool, vanpool, schoolpool, transit, bike to work, and work from home programs.

Carpool – Carpooling shares the daily commute with another driver. iCommute offers RideMatcher, which is a database of thousands of commuters looking for carpool partners. TripTracker allows the individual commuter to track carpool trips and see the cost and environmental savings. The iCommute Carpool program also offers a Guaranteed Ride Home Program for those who carpool three or more times each week. And, park-and-ride lots are free and available for those who carpool or vanpool. Oceanside currently has four park-and-ride lots at the following locations:

- SR78/College Boulevard South (3700 Haymar Drive)
- Moreno Street (1928 Moreno Street)
- SR78/College Boulevard North (3710 Vista Way)
- Frontier Drive/New Hope Church (Mission Avenue and Frontier Drive)

Vanpool – Vanpools allow groups of 7 to 15 people to share their commute to work. A van can be leased by the commuter or an employer through one of iCommute's contracted vanpool vendors and receive up to \$400 per month to help offset the cost of the lease. The RideMatcher, TripTracker, Guaranteed Ride Home, and Park and Ride Lots are all available to vanpool commuters as well.

SchoolPool – This is iCommute's free carpool matching service for parents of children who attend the same school anywhere in the region. iCommute has helped to establish SchoolPool programs at elementary, middle, and high schools throughout the region.

Transit – Public transportation service is available throughout the region, including Oceanside. iCommute offers a link to 511's Transit Trip Planner to plan a commute using the public transportation available in the San Diego region.

Local Circulator – A local circulator within the Coast Highway corridor such as streetcars like those found in Old Town San Diego, can provide transportation options and connection to popular destinations. Providing local circulation between parking areas and destination points along the coast assists in enhancing the overall parking experience. These systems can promote walkability and can reduce traffic congestion. Funding mechanisms can be secured through public and/or private means and can enhance economic vitality for the Coast Highway corridor. A circulator shuttle operating between the Downtown area and Oceanside Boulevard is recommended, as this segment of Coast Highway typically has the highest concentration of development and parking demand within the corridor.

Bike to Work – iCommute will help a commuter find a ride partner, as well as connect the commuter with a free bike locator and map out the best route of

EXHIBIT C

Coast Highway Corridor Final Environmental Impact Report

Statement of Overriding Considerations



STATEMENT OF OVERRIDING CONSIDERATIONS FOR THE OCEANSIDE COAST HIGHWAY CORRIDOR STUDY (SCH NO. 2016051078)

As discussed in the Findings of Fact, the Final Environmental Impact Report for the Oceanside Coast Highway Corridor Study concludes that the project, even with incorporation of all feasible mitigation measures and consideration of alternatives, will nonetheless have significant and unavoidable impacts related to Air Quality, Greenhouse Gas Emissions, Noise and Vibration, and Transportation and Traffic.

As the primary purpose of the California Environmental Quality Act (CEQA) is to fully inform the decision-makers and the public as to the environmental effects of a project and to include feasible mitigation measures and alternatives to reduce any such adverse effects below a level of significance. CEQA nonetheless recognizes and authorizes the approval of projects where not all adverse impacts can be fully lessened or avoided. However, the Lead Agency must explain and justify its conclusion to approve such a project through the statement of overriding considerations setting forth the recommended project alternative's general, social, economic, policy or other public benefits which support the agency's informed conclusion to approve that alternative over another.

Under CEQA, before a project which is determined to have significant environmental effects which cannot be avoided can be approved, the public agency must consider and adopt a statement of overriding considerations pursuant to the CEQA Statutes Section 21081 and CEQA Guidelines 15043 and 15093.

The City Council hereby finds, after consideration of the Final EIR and the evidence in the record, that each of the specific overriding economic, legal, social, technological and other benefits of the project as set forth below independently and collectively outweighs the significant and unavoidable impacts. Any one of the reasons for approval cited below is sufficient to justify approval of the project. Thus, even if a court were to conclude that not every reason is supported by substantial evidence, the City Council will stand by its determination that each individual reason is sufficient.

The City Council further finds that, as part of the process of obtaining project approval, all significant effects on the environment from implementation of the project have been eliminated or substantially lessened where feasible. Furthermore, the City Council has determined that any

remaining significant effects on the environment found to be unavoidable are acceptable due to the following specific overriding economic, technical, legal, social and other considerations.

The Coast Highway Corridor project, which includes both the Complete Streets and Incentive District project elements, has the following benefits:

- In April 2009, the City Council adopted the Coast Highway Vision and Strategic Plan (Vision Plan), which outlined a vision for the Coast Highway that is based on livable communities and Smart Growth principals intended to produce vibrant and economically successful communities. The Coast Highway Corridor project provides a multi-pronged implementation strategy for the Vision Plan, including the Complete Streets improvements and the Incentive District. Adoption of the project will implement these elements of the Vision Plan and will support the development of a vibrant and economically successful Coast Highway Corridor.
- The Coast Highway Corridor is a key mixed-use corridor that provides access to the City's coastal resources, downtown district, and adjacent neighborhoods. However, the area also currently includes vacant and underutilized property. The Coast Highway project will focus public and private investment in this important corridor in order to support additional commercial activity, housing, recreational uses, and gathering spaces.
- The Coast Highway Corridor project is the product of a comprehensive public planning effort that has undergone continual refinements which has resulted in a thoughtful balance of community, business, and environmental interests.
- The Coast Highway Corridor project, in particular the Incentive District, will support the transformation of the built environment within the Coast Highway corridor by incentivizing high quality urban and architectural design, sustainable development, synergistic land uses, and enhancement of environmental resources. Specifically, the Incentive District includes form-based development standards which provide more detail, addressing features such as horizontal and vertical articulation, architectural details, fenestration, finish materials, pedestrian access, parking configuration, and the relationship between buildings and on-site open space areas. The form-based development standards in the Incentive District will not only regulate the massing of buildings but also their visual quality and their effectiveness in accommodating intended uses and activating adjacent public spaces.
- The Coast Highway Corridor project, in particular the Incentive District, will encourage the development of additional housing, public open space and parking, all of which are highly desired in the City of Oceanside. The policies of the Incentive District will also ensure this new development will result in a compatible and synergistic mix of land uses, activation of the street frontage, and an attractive and inviting streetscape.
- The Coast Highway Corridor project promotes pedestrian and bicycle access to the coastline, redevelopment and infill within already urbanized areas, expansion of visitor-

serving commercial uses, and visual enhancement of the Coast Highway corridor, all of which are desired outcomes of the City Council.

- The allowance for standalone residential use in the avenue segments of the corridor, though the Incentive District, responds to strong demand for housing in the San Diego region and the City of Oceanside's need to accommodate its regional fair share of new housing growth.
- The Coast Highway Corridor project will transform Coast Highway into a "Complete Streets" which will be more attractive to pedestrians and alternative means of transportation and will increase foot traffic along the corridor, which will ultimately increase the economic viability of existing and future businesses.
- The Coast Highway Corridor project will provide the incentives for the encouragement of redevelopment and continued investment within the area in exchange for community benefits to enhance and revitalize the area.
- The Coast Highway Corridor project will enhance the local economy and provides opportunities for future commercial development near existing businesses, transportation hubs and walkable residential areas. In addition, the project will also provide residential uses along Coast Highway to provide a new live, work, play atmosphere.
- Considering environmental effects at a programmatic level and identifying mitigation measures for future development within the Incentive District boundaries provides for comprehensive consideration of the cumulative environmental effects. As such, with the adoption of the Incentive District and the mitigation measures outlined in the Mitigation Monitoring and Reporting Program, additional environmental safeguards will be provided for the project area that would not be present without the adoption of the district-wide program.

The public and private investment in and improvement of the Coast Highway Corridor and the betterment of the quality of life for the residents of the community is one of the City's highest priorities. Having considered these benefits, the City Council finds that the benefits of the project outweigh the unavoidable adverse environmental effects, and that the adverse environmental effects are therefore acceptable.

development in nodal segments of the Coast Highway corridor, as depicted on map exhibits in the Coast Highway Development Incentive District.

Figure LU-5

Special Management Area

Oceanside Small Craft Harbor Precise Plan

(Hardcopy available at the Oceanside Planning Department)

2.2 Commercial Development

OBJECTIVE: To promote and preserve a balance of successful markets and services in aesthetic, people-oriented associations that are compatible and organized to surrounding land uses.

Policies:

A. The City shall preserve and enhance viable, positive commercial developments through the proper allocation of the following commercial land use designations:

- 1) Community Commercial
- 2) Neighborhood Commercial
- 3) General Commercial
- 4) Special Commercial
- 5) Professional Commercial

2.21 Community Commercial

Policies:

- A. This designation shall provide the community with commercial centers containing a wide variety of commercial establishments. Major tenants shall provide larger, low volume, higher cost items, such as home furnishings, apparel, durable goods, and specialty items and generally have citywide market areas. Support facilities such as entertainment establishments and restaurants shall be encouraged.
- B. Development within this designation shall be on sites in excess of thirty (30) acres. Specialized commercial uses requiring less land area may be

considered when of similar intensity, or offering comparative revenue and/or employment generating capacities. Location shall be limited to sites along major arterials or higher rated roads.

C. This designation shall provide for residential use in conjunction with mixed-use development at densities up to 29 dwelling units per acre.

2.22 Neighborhood Commercial

Policies:

A. Neighborhood Commercial shall provide commercial uses which meet the day to day commercial needs of the community. Commercial center development is implicit. Key tenants shall be limited to supermarkets, variety stores, drug stores, specialty stores, and similar businesses. Most retail shops, restaurants and services are permitted as minor tenants and "convenience" businesses may be allowed when well integrated into the center's design.

B. Since Neighborhood Commercial centers will meet the daily shopping needs of the community, they shall be located near residential areas along major arterials or secondary arterials, preferably at their intersections with collector streets. Consequently there shall be limits on their intensity to be compatible with nearby residential areas. Areas shall generally be between ten (10) and thirty (30) acres.

C. This designation shall provide for residential use in conjunction with mixed-use development at densities up to 29 dwelling units per acre.

2.23 General Commercial

Policies:

Land Use Element

- A. General Commercial shall provide retail shops, restaurants and services which meet the "immediate" commercial needs of the community. High intensity, drive-up/drive-through and convenience businesses shall be developed in commercial centers or clustered together in coordinated site or development plans to avoid the proliferation of driveway-cuts and to accommodate their high traffic generation characteristics.
- B. General Commercial shall be designated on small sites of between five (5) and ten (10) acres. General Commercial areas shall be located on streets designated as major arterials or higher or at the intersection of two secondary arterials.
- C. Outside of the Coastal Zone, this designation shall provide for residential use in conjunction with mixed-use development at densities up to 29 dwelling units per acre. Within the Coastal Zone, this designation shall provide for both standalone residential use and residential use in conjunction with mixed-use development at densities consistent with those established in the C-2 zoning standards and the Coast Highway Development Incentive District.

2.24 Special Commercial

Policies:

- A. Special Commercial shall designate commercial sites within and/or adjacent to areas with unique characteristics, such as scenic areas, historic areas, freeway off-ramps, the Coastal Zone, and other unique or special areas.
- B. Signage in Special Commercial developments shall be consistent with any special guidance systems established for the area.
- C. Uses and development standards shall be established through the following special policies and identified guidance systems to best utilize and/or protect the unique characteristics of the externality.

D. Outside of the Coastal Zone, this designation shall provide for residential use in conjunction with mixed-use development at densities up to 29 dwelling units per acre. Within the Coastal Zone, this designation shall provide for both standalone residential use and residential use in conjunction with mixed-use development at densities consistent with those established in the C-2 zoning standards and the Coast Highway Development Incentive Overlay (DIO).

2.241 Coastal Zone

Policy:

- A. Development on property designated Special Commercial within the boundaries of the Coastal Zone or Local Coastal Program (LCP) Area shall provide coastal dependent, recreational and visitor serving uses and facilities as specified by the LCP and California Coastal Act of 1976 (See Figure LU-16).
- B. Residential uses shall be permitted in conjunction with mixed-use development on those properties within the Coastal Zone bearing the General Commercial land use designation. Maximum potential residential densities on these properties are specified in the applicable zoning standards.
- C. Standalone residential use shall be permitted within "avenue" segments of Coast Highway as specified in map exhibits in the Coast Highway Development Incentive Overlay. Maximum potential residential densities for standalone residential use are specified in the applicable zoning standards.

2.242 Interstate 5, State Highway 76, and State Highway 78 Corridors

Policies:

- A. Commercial sites adjacent to freeway off-ramps and expressway intersections shall coordinate site development to provide joint use

ORDINANCE NO.

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF OCEANSIDE AMENDING THE COMPREHENSIVE ZONING ORDINANCE TO INCORPORATE THE COAST HIGHWAY INCENTIVE DISTRICT AS ARTICLE 9

(ZA19-00004)

WHEREAS, the Comprehensive Zoning Ordinance sets forth the purpose and intent, and zoning regulations for properties within the City of Oceanside; and

WHEREAS, certain Articles of said ordinance contain zoning regulations applicable citywide and others are limited to inland or coastal zones; and

WHEREAS, Comprehensive Zoning Ordinance Articles applicable citywide and within the coastal zone have been certified by the California Coastal Commission and constitute part of the Implementation Plan of the Local Coastal Program; and

WHEREAS, amendments to citywide and coastal zone Article regulations require processing of Local Coastal Program amendments and certification by the California Coastal Commission; and

WHEREAS, Coast Highway is a key mixed-use corridor that provides access to coastal resources, the downtown district, and adjacent neighborhoods; and

WHEREAS, the Coast Highway corridor includes vacant and underutilized property that can support additional commercial activity, housing, recreational uses, and gathering spaces; and

WHEREAS, the Planning Division has prepared text amendments to the City's Comprehensive Zoning Ordinance to incorporate the Coast Highway Incentive District as Article 9; and

WHEREAS, the Coast Highway Incentive District is intended to promote the revitalization of the Coast Highway corridor as promulgated by the Coast Highway Vision and Strategic Plan; and

WHEREAS, on May 22, 2019, the Planning Commission conducted a duly-noticed public hearing as prescribed by law and recommended with revisions City Council approval of said zoning text amendment by unanimous vote; and

WHEREAS, said changes require amendment of the implementing document of the City's Local Coastal Program, which must be certified by the California Coastal Commission prior to becoming effective; and

WHEREAS, based upon evidence, testimony, and staff reports, this Council finds that Zone Amendment ZA19-00004 conforms to the General Plan and Local Coastal Program of the City of Oceanside; and

1 WHEREAS, pursuant to the California Environmental Quality Act of the 1970 and State
2 Guidelines, an Environmental Impact Report was prepared for the proposed project;

3 NOW, THEREFORE, the City Council of the City of Oceanside does ordain as follows:

4 **SECTION 1.** Zone Amendment (ZA19-00004), incorporating the Coast Highway Incentive
5 District into the Comprehensive Zoning Ordinance and establishing the amended text as part of the
6 implementing document of the City's Local Coastal Program, as specified in Exhibit A, is hereby
7 adopted.

8 **SECTION 2.** The City Clerk of the City of Oceanside is hereby directed to publish this
9 Ordinance, or the title hereof as a summary, pursuant to state statute, once within fifteen (15) days after
10 its passage in a newspaper of general circulation published in the City of Oceanside.

11 **SECTION 3.** Severability.

12 If any section, sentence, clause or phrase of this Ordinance is for any reason held to be invalid or
13 unconstitutional by a decision of any court of competent jurisdiction, such decision shall not affect the
14 validity of the remaining portions of this Ordinance. The City Council hereby declares that it would have
15 passed this Ordinance and adopted this Ordinance and each section, sentence, clause or phrase thereof,
16 irrespective of the fact that any one or more sections, subsections, sentences, clauses or phrases be
17 declared invalid or unconstitutional.

18 **SECTION 4.** Notice is hereby given that the time within which judicial review must be
19 sought on this decision is governed by Government Code Section 65009(c).

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SECTION 5. This ordinance shall be effective upon certification of LCPA19-00005 by the California Coastal Commission.

INTRODUCED at a regular meeting of the City Council of the City of Oceanside, California, held on the 14th day of August, 2019, and, thereafter,

PASSED AND ADOPTED at a regular meeting of the City Council of the City of Oceanside, California, held on the ___ day of ___, 2019, by the following vote:

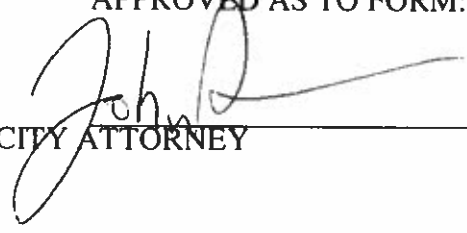
- AYES:
- NAYS:
- ABSENT:
- ABSTAIN:

MAYOR OF THE CITY OF OCEANSIDE

ATTEST:

APPROVED AS TO FORM:

CITY CLERK



CITY ATTORNEY

EXHIBIT A

Zoning Text Amendment (ZA19-00004)

Coast Highway Incentive District (Zoning Ordinance Article 9)

Article 9 **Coast Highway Incentive District (Incentive District).**

Sections:

- 901 Purpose and Intent
- 902 Applicability
- 903 Administration
- 904 Regulating Plan
- 905 Mixed-Use Standards
- 906 Residential Incentive Program
- 907 Land Use Standards
- 908 Urban Standards
- 909 Architectural Standards
- 910 Large Lot Standards
- 911 Parking Standards
- 912 Definitions

901 Purpose and Intent

The specific purposes of the Coast Highway Incentive District (Incentive District) are to:

- A. Incent redevelopment and revitalization of the Incentive District by streamlining the development review process and providing development incentives.
- B. Encourage sustainable, high-quality development consistent with the intent and objectives articulated in the Coast Highway Vision and Strategic Plan.
- C. Create distinct pedestrian-oriented subareas, including:
 - 1. Urbane mixed-use nodal areas featuring relatively intense commercial land use and residential density; development in these nodal areas will generally be taller and more street-adjacent than development in other subareas; commercial uses, including visitor-serving businesses, will provide a wide range of employment opportunities.
 - 2. Commercial Villages featuring neighborhood-serving commercial uses in a suburban main street setting; these villages also allow for mixed-use development, consistent with underlying zoning standards.
 - 3. Transitional Avenue segments featuring a combination of mixed-use, standalone commercial, and standalone residential development with generally less land use intensity and residential density relative to nodal areas; these segments are characterized by more expansive setbacks and landscaping.
- D. Promote high-quality urban and architectural design and variability of massing and height, emphasizing the design of the interface between the private and public realms.
- E. Facilitate the creation of vibrant community places and tourist destinations.

- F. Treat Coast Highway as a complete, multi-modal street that is safe, pedestrian and bicycle friendly, accessible, attractive, visually and functionally engaging for users of all ages and abilities, and well integrated with adjoining neighborhoods along the corridor.

902 Applicability

A. Applicability and Zoning Map Designator

The Incentive District is an *optional* development tool to meet the purpose and intent as described in Section 901, and may be used to develop any parcel within the Incentive District (Map 1). When an applicant voluntarily decides to develop consistent with the Incentive District standards, these standards shall supersede underlying zoning standards.

B. Rules for Interpretation

Section 240 Rules for Interpretation of Article 2 Organization, Applicability, and Interpretation of the 1992 Ordinance shall apply.

C. Severability

Section 220 (L) Severability of Article 2 Organization, Applicability, and Interpretation of the 1992 Ordinance shall apply.

903 Administration

A. Administrative Development Plan review is required for the following:

- 1. New or redevelopment proposals with 43 dwelling units per acre or less, or developments with no residential component.
- 2. Subdivisions and land assemblages.

B. Development Plan review is required for the following:

- 1. Development proposals greater than 43 dwelling units per acre.
- 2. Development proposals greater than 45 feet in height.
- 3. Development proposals subject to Large Lot Standards stipulated in Section 910.

C. Conditional Use Permits are required for the following:

Land uses subject to a Conditional Use Permit per Table 2, Section 907.

D. Overview of Permits Required

Table 1 provides an overview of the permits required, the review authority, and the appeal authority within the Incentive District.

Table 1. Applications and Review Authority

Permit Type	Reviewing Authority	Appealable/Appeal Authority
Administrative Development Plan Permit	City Planner Administrative Decision	Yes/Planning Commission
Development Plan Permit	Planning Commission	Yes/City Council
Conditional Use Permit	Planning Commission	Yes/City Council

E. Administrative Development Plan Review

1. Review Authority

The City Planner shall approve, conditionally approve, or deny applications for Administrative Development Plan Permits based on considerations of the standards of this article.

2. Review and Decision

- a. The City Planner shall approve an Administrative Development Plan Permit if the proposed development complies with applicable development standards, and meets the required findings in Section 903 E (3), below.
- b. The City Planner may require review by any other City department or division or governmental agency deemed necessary.
- c. The City Planner shall prepare a written decision which shall contain the findings of fact upon which such decision is based. A copy of the decision shall be mailed to the applicant at the address shown on the application within 14 calendar days after the decision is made.

3. Required Finding

An Administrative Development Plan Permit may be granted if the City Planner finds that the project, as submitted or modified, is consistent with the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, and the Local Coastal Program and complies with the applicable standards of this article.

4. Conditions

In granting an Administrative Development Plan Permit, the City Planner or the Review Authority on appeal may impose reasonable conditions to protect the public health, safety, and general welfare and secure the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, the Local Coastal Program, and this article, and to support the required findings of approval.

5. Other Standards

Administrative Development Plan Permits shall comply with the standards of Article 43 Development Plan Review of the 1992 Ordinance, specifically: Sections 4303 Initiation of Development Plan Review; 4304 Application for Development Plan; 4305 Notice Administrative Hearing and Public Hearing; and 4308 Effective date, Lapse of Approvals, Time Extension, and Changed Plans.

F. Development Plan Review

1. Review Authority

The Planning Commission shall approve, conditionally approve, or deny applications for Development Plan Permits based on consideration of the standards of this article.

2. Required Findings

A Development Plan Permit shall be granted only if the Planning Commission determines that the project as submitted or modified, complies with the following criteria:

- a. That the proposed project is consistent with the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, and the Local Coastal Program.
- b. That the physical location, size, massing, setbacks, pedestrian orientation, and placement of proposed structures on the site and the location of proposed uses within the project comply with applicable standards and are consistent with the purpose and intent of the Incentive District.
- c. That any proposed extension of rights-of-way and/or onsite circulation can accommodate autos, bicycles, pedestrians, and multi-modal transportation methods, including adequate vehicle and bicycle parking and pedestrian access.
- d. That the area covered by the application can be adequately, reasonably, and conveniently served by existing and/or planned public services, utilities, and public facilities.
- e. That any community benefits have been provided in accordance with Section 904.

3. Conditions

In granting a Development Review Permit, the Planning Commission or the Review Authority on appeal may impose reasonable conditions to protect the public health, safety, and general welfare and secure the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, the Local Coastal Program, and this article, and to support the required findings of approval.

4. Other Standards

Development Plan Permits shall comply with the standards of Article 43 Development Plan Review of the 1992 Ordinance, specifically: Sections 4303 Initiation of Development Plan Review; 4304 Application for Development Plan; 4305 Notice Administrative Hearing and Public Hearing; and 4308 Effective date, Lapse of Approvals, Time Extension, and Changed Plans.

G. Conditional Use Permit

Conditional Use Permits shall be processed in accordance with Article 41 of the 1992 Ordinance.

H. Appeals

1. Rights of Appeal and Review

Rights of appeal and review procedures shall be as prescribed by Article 43 Development Plan Review of the 1992 Ordinance, specifically Section 4309 (A) and (B), except that any filing for an appeal shall include the grounds for appeal and supporting documentation in compliance with Section 903 (H), 2 and 3, of this article.

2. Grounds for Appeal

All appeals shall be filed with the following supporting documentation.

- a. Factual Error. The statements or evidence relied upon by the decision maker when approving, conditionally approving, or denying a permit, map, or other matter were inaccurate.
- b. New Information. New information is available to the applicant or the interested person that was not available through reasonable efforts or due diligence at the time of the decision.
- c. Findings not supported. The decision maker's stated findings to approve, conditionally approve, or deny the permit, map, or other matter are not supported by the information provided to the decision maker.
- d. Conflicts. The decision to approve, conditionally approve, or deny the permit is in conflict with the intent and objectives of the Coast Highway Vision and Strategic Plan, General Plan, Local Coastal Program, and applicable development standards.

3. Insufficient Appeal

An insufficient appeal shall be returned to the appellant.

I. Incentive District Boundary Adjustments

- 1. Minor adjustments to the Incentive District boundary and identified subareas may be approved by the City Planner subject to the following findings:

- a. Parcels must be included within the Coastal Zone boundary.
- b. Parcels are contiguous to the Incentive District or identified subarea and the addition of the parcel(s) would result in a project that equally or better meets the purpose and intent of the Incentive District.
- c. All other adjustments will require an amendment to the Incentive District.

J. Periodic Review and Maintenance

To ensure progress toward achieving the intent and objectives articulated in the Coast Highway Vision and Strategic Plan, General Plan, and Local Coastal Program, the Planning Commission will periodically review the Incentive District boundaries and this Article and recommend changes.

904 Regulating Plan

A. Applicability

The Regulating Plans for this article are depicted in the following maps:

- Map 2 Subarea Plan
- Map 3 Setback Regulating Plan

B. Purpose of the Subarea Plan

The Subarea Plan (Map 2) divides the Incentive District into three subareas, with each subarea assigned place-specific development standards. The purpose of each subarea is described in Section 901(c).

C. Purpose of Setback Plan

The Setback Plan (Map 3) establishes minimum and maximum setbacks along the primary frontage of all parcels within the Nodes and Commercial Village subareas and minimum setbacks along the primary frontage of all parcels within the Avenue subareas.

D. Setback Standards

The primary frontage is provided on Map 3 for guidance purposes only. The City Planner shall make the final determination as to which parcel line(s) serves as the primary frontage of each project site. The City Planner shall use the following criteria in making this determination:

1. Primary frontages shall be applied to properties abutting Coast Highway.
2. For parcels not abutting Coast Highway, primary frontages shall be those abutting streets that parallel Coast Highway.
3. For parcels not abutting Coast Highway with frontages on more than one street running parallel to Coast Highway, the orientation of existing development on the same city block shall determine the primary frontage.

4. For properties on Wisconsin Avenue and south of Coast Highway, primary frontages shall be those abutting Wisconsin Avenue.
5. Front yard setbacks along the primary frontage as shown on Map 3:
 - a. Avenues. Minimum 5 feet for mixed-use/ground-floor commercial and 10-foot minimum for standalone residential; 10-foot maximum for mixed-use/ground-floor commercial and 20-foot maximum for standalone residential.
 - a. Commercial Villages. Minimum 3 feet and 8-foot maximum.
 - b. Nodes. No minimum and 10-foot maximum.
6. Side yard setbacks:
 - a. Corner lots in all sub areas: Consistent with applicable minimum and maximum front yard setbacks.
 - b. Interior lots in all sub areas with at least one frontage along a primary frontage identified on Map 3: No required minimum or maximum.
 - c. For all other interior lots in all sub areas: Minimum 10 percent of lot width, not less than 3 feet and not more than 5 feet.
7. Rear yard setbacks:
 - a. Properties in all sub areas not abutting a residential zoning district: No required minimum or maximum.
 - b. For properties abutting residential zoning districts: Minimum 15 feet, unless abutting an alley, then minimum 5 feet.

905 Mixed-Use Standards

A. Purpose

This section establishes the minimum standards for commercial space within mixed-use developments.

B. Mixed-Use Commercial Floor Area

1. For mixed-use projects on lots 60,000 sf and smaller, the Floor Area Ratio devoted to commercial space shall be a minimum of 0.20.
2. For mixed-use projects on lots greater than 60,000 sf, the Floor Area Ratio devoted to commercial space shall be a minimum of 0.25.

C. Commercial Uses

1. Required commercial spaces shall contain commercial land uses that serve clients and patrons that will visit the site. These spaces may not be used for storage or in the same manner as home occupancy businesses.

2. Commercial land uses in mixed-use projects include the following major land use categories; artisan manufacturing, bars and cocktail lounges, craft breweries and wineries, child care, commercial recreation and entertainment-indoor, cultural institutions, financial services, food and beverage sales, offices business and professional, park and recreation facilities, personal improvement services, personal services, restaurants, retail, studios, and visitor accommodations.

906 Residential Incentive Program

A. Purpose

1. The purpose of this section is to establish regulations that will allow new development to exceed the maximum residential densities of the underlying zoning district in exchange for community benefits, or to implement stand-alone residential use in the Avenue subareas.
2. These regulations require that projects approved above the maximum residential density must implement community benefits within one or more of the categories in Section 906 (C), below.

B. Applicability

1. The provisions of this Chapter shall apply to the nodal subareas as shown in Map 4, Residential Incentive Program Area.
2. Nodes. Residential use in nodal areas is permitted as a component of mixed-use development that meets minimum commercial standards as specified in Section 905. Development in nodal areas may exceed the maximum residential density up to a maximum of 63 dwelling units per acre through the provision of certain community benefits as further described in Section 906 (C), below.
3. Avenue Segments. Development in Avenue segments may not exceed the residential density allowance of the underlying zoning district.

C. Residential Density Incentive for Nodal Development

1. In nodal areas, projects can earn additional density above the underlying residential density allowance in exchange for one or more of the following community benefits provided per unit above 43 dwelling units per acre.
 - a. Public Improvement Fee. Projects can gain additional density up to a maximum of 63 dwelling units per acre through payment of a per-unit public improvement fee. Said fee shall be set and adjusted by the City Council and shall provide for public improvements within the Incentive District.
 - b. Public Open Space. Projects can gain additional density up to a maximum of 63 dwelling units per acre through the provision of public open space at a minimum ratio of 200 square feet (sf) per unit. Public open space provided in exchange for residential density must comply with the following standards:

- i. Projects shall provide a minimum of 1,000 sf of public open space.
 - ii. The minimum dimensions of public open space shall comply with the standards applicable to the building type as provided in Section 908 (D).
 - iii. Open space shall be owned, operated, and maintained by the developer or property manager in accordance with an approved maintenance plan.
 - iv. Open space shall be directly accessible from the public right-of-way.
 - v. Open space shall be accessible to persons with disabilities.
 - vi. Open space shall be on the ground level.
 - vii. No more than 20 percent of open space shall be occupied by above-grade structures.
 - viii. Open space shall be open to the public, without charge, each day of the year from 6:00 a.m. to 10:00 p.m., except for temporary closures for necessary maintenance or compelling public safety concerns identified in coordination with the Oceanside Police Department.
 - ix. At a minimum, the following elements shall be included within the open space:
 - Trees and landscaping.
 - Seating.
 - Refuse and recycling receptacles.
 - Signage identifying the open space as open to the public and specifying hours of operation.
 - x. Open space may be provided off-site, subject to approval by the City Planner.
 - xi. Off-site open space shall comply with the following standards:
 - The open space shall be within 1,320 feet of the project site.
 - The open space shall be dedicated and improved concurrent with the project benefiting from bonus residential density.
 - The open space may be either publicly or privately maintained subject to agreement with the City.
- c. **Public Parking.** Projects can gain additional density up to a maximum of 63 dwelling units per acre through the provision of a minimum number of public parking spaces per unit in compliance with the following standards:
- i. One parking space per unit.

- ii. The total number of additional parking spaces must equal at least 10 percent of the total minimum parking spaces required under Section 911.
 - iii. A public parking easement shall be executed for these facilities to the satisfaction of the City Planner and City Attorney.
 - d. Increased Commercial Floor Area. Projects can gain additional density up to a maximum of 63 dwelling units per acre through the provision of additional ground floor commercial floor area above the minimum commercial floor area standard established in Section 905 (B). A total of 250 sf of additional ground floor commercial space shall be provided per unit.
2. The Residential Incentive Program shall be periodically reviewed by the Planning Commission as described in Section 903 (J) to ensure the public benefits provided in exchange for residential density continue to be equitable and desirable by the community.

D. Standalone Residential in Avenues

Standalone residential development shall be permitted in Avenues in compliance with the maximum density of the underlying zoning district. No community benefit per Section 906(c) above, is required.

907 Land Use Standards

A. Purpose

Allowed land uses are provided in Table 2, Land Use and Permit Standards. The land use regulations shall be reviewed as part of periodic reviews described in Section 903 (J) to ensure a balanced mix.

B. Applicability

Land uses shown in the table are allowed in the subarea specified. Uses not included in the table are considered prohibited unless determined by the City Planner to be substantially similar to another permitted or conditionally permitted use. Each land use in the table corresponds to a definition listed in Section 912.

C. Land Use and Permit Standards

Table 2. Land Use and Permit Standards

Use ⁽¹⁾	Avenue	Commercial Village	Node
Animal Sales and Services	P	P	P
Artisan Manufacturing	P	P	P
Assembly/Meeting Facility	C	C	P
Bars and Cocktail Lounges	C(4)	C(4)	C(4)

Use ⁽¹⁾	Avenue	Commercial Village	Node
Breweries, Craft	Variable (10)	Variable (10)	Variable (10)
Child Care	P	P	P
Commercial Recreation and Entertainment – Indoor	P(4)	P(4)	P(4)
Cultural Institutions	P	P	P
Financial Services	P(2)	P(2)	P(3)
Food and Beverage Sales	P(4)	P(4)	P(4)
Hospitals	C	C	P
Offices, Business and Professional	P(2)	P(2)	P(3)
Parking as a primary use	P(5)	P(5)	P(5)
Park and Recreation Facilities	P	P	P
Personal Improvement Services	P(4)	P(4)	P(4)
Personal Services	P(4)	P(4)	P(4)
Residential	P(6)	P(6)(7)	P(7)(8)
Restaurants, Fast Food	P(2)	P(2)	P(3)
Restaurants, Full Service	P(4)	P(4)	P(4)
Retail	P(2)	P(2)	P(2)(4)
Schools	C	C	C
Studios	P	P	P
Theater	C	C	P
Vehicle/Equipment Sales and Services	-	C	-
Visitor Accommodations	P	P	P
Wineries, Craft	Variable (10)	Variable (10)	Variable (10)
Other ⁽⁹⁾	C	C	C

Notes: “P” denotes that the use is permitted.

“C” denotes that the use is permitted with the approval of a Conditional Use Permit pursuant to Article 41 of the 1992 Ordinance.

“-” denotes that the use is not permitted.

“(numbers, such as ‘1’)” denote that the land use is subject to additional use regulations which are provided after the table.

D. Additional Land Use Regulations

1. All uses must meet City noise and emissions standards per Oceanside Municipal Code Chapter 38. The City Planner may establish project-specific conditions of approval to minimize noise, including conditions to soundproof facilities, limited operating hours, and/or limited facility size.

2. Any drive-through facilities require approval of a Conditional Use Permit in accordance with Article 41 of the 1992 Ordinance.
3. Drive-through facilities are prohibited in Nodes.
4. Certain uses within this land use category are subject to standards in Article 36 Separation of Regulated Uses of the 1992 Ordinance. See Section 912 Definitions of this article for identification of these uses.
5. Parking as a primary use must be made publically available and shall comply with appropriate provisions of Section 911 and applicable provisions of Article 31 of the 1992 Zoning Ordinance, including Section 3120 additional design standards for parking lots and structures.
6. Residential density may not exceed 43 units per acre.
7. Residential shall only be permitted in conjunction with another permitted use.
8. Residential density may not exceed 43 units per acre unless the project complies with the Residential Incentive Program per Section 904 and is subject to approval of a Development Plan Permit per Section 903.
9. Any use that is not listed in Table 2 and defined in Section 912 is considered “Other” and requires a Conditional Use Permit in accordance with Article 41 of the 1992 Ordinance.
10. Definitions, performance standards, and review processes for craft breweries and wineries are provided in Section 414 of Article 4 of the 1992 Ordinance.

908 Urban Standards

A. Purpose

The Urban Standards define the design of the built environment in the Incentive District, placing emphasis on the design of the street frontage where private development meets the public street. The Urban Standards vary by subarea and are further described in Table 3 below.

B. Applicability

This section applies to all areas within the Incentive District.

C. Urban Standards by Subarea

Table 3. Urban Standards by Subarea

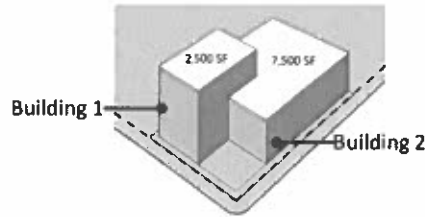
Location	Allowable Building Types	Allowable Frontage Types	Maximum Height	Minimum Frontage Occupancy²
Node	<ul style="list-style-type: none"> • Live/Work • Mixed Use • Courtyard Complex • Podium • Wrap Building • Special Type Buildings 	<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront • Dooryard • Live/Work • Stoop • Forecourt 	45 feet or four stories May exceed height limit subject to additional provisions ¹	90%
Commercial Village	<ul style="list-style-type: none"> • Main St. Retail/Office • Live/Work • Mixed Use • Courtyard Complex • Podium • Wrap Building • Special Type Buildings 	<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront • Live/Work • Dooryard • Stoop • Forecourt 	45 feet or four stories	70%
Avenue	<ul style="list-style-type: none"> • Main St. Retail/Office • Live/Work • Courtyard Apartment • Condo Complex • Podium • Wrap Building • Townhouse • Stacked dwelling • Special Type Buildings 	<ul style="list-style-type: none"> • Arcade/Gallery • Live/Work • Shopfront • Avenue-Commercial • Avenue-Residential • Dooryard • Stoop • Forecourt 	35 feet or three stories May exceed height limit subject to additional provisions ¹	60%

Notes:

- 1) Projects may exceed the height limit up to a maximum of 65 feet, with a maximum average height of 55 feet, subject to approval of a Development Plan Permit pursuant to Section 903 (F).
- 2) An exception to the minimum frontage occupancy standard may be granted by the City Planner where significant urban or architectural features or contributions to the public realm are offered. Such public realm features may include, but are not limited to publicly accessible open space, such as a courtyard or plaza or residential entry alcoves or stoops that contribute to the public realm by adding detail and enhancing the relationship to the street. The following additional standards apply:
 - a. Any open space provided must comply with the open space standards in Section 908.
 - b. The open space may include outdoor dining areas, but the overall space must be accessible to the public and enhance the public realm.
 - c. The urban, architectural, or public realm features must be of exceptional, high-quality design.
 - d. The urban, architectural, or public realm features will not compete with other similar features or spaces on that block so as to detract from an inviting, pedestrian-friendly environment.
- 3) Projects may exceed the height limit up to a maximum of 45 feet, with a maximum average height of 35 feet.

Figure 1. Calculating Maximum Average Height for Nodes

Step One



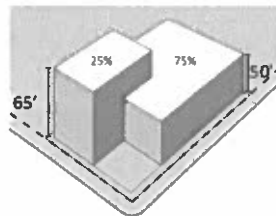
$$\frac{\text{Building 1 Roof Area (SF)}}{\text{Building 1 Roof Area (SF) + Building 2 Roof Area (SF)}} = \text{Building 1 Percentage (B1\%)}$$

Ex: $\frac{2,500}{2,500 + 7,500} = 25\%$

$$\frac{\text{Building 2 Roof Area (SF)}}{\text{Building 1 Roof Area (SF) + Building 2 Roof Area (SF)}} = \text{Building 2 Percentage (B2\%)}$$

Ex: $\frac{7,500}{2,500 + 7,500} = 75\%$

Step Two

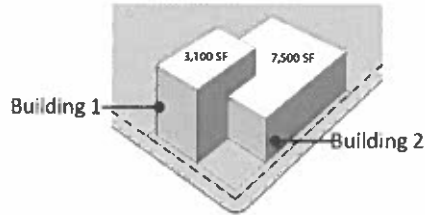


$$(\text{Building 1 Height (FT)} \times \text{B1\%}) + (\text{Building 2 Height (FT)} \times \text{B2\%}) = \text{Average Height (FT)}$$

Ex: $(65' \times .25) + (50' \times .75) = 55'$

Figure 2. Calculating Maximum Average Height for Avenues

Step One



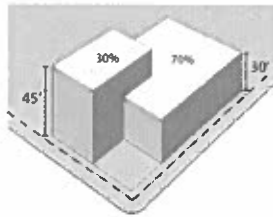
$$\frac{\text{Building 1 Roof Area (SF)}}{\text{Building 1 Roof Area (SF)} + \text{Building 2 Roof Area (SF)}} = \text{Building 1 Percentage (B1\%)}$$

$$\text{Ex: } \frac{3,100}{3,100 + 7,500} = 30\%$$

$$\frac{\text{Building 2 Roof Area (SF)}}{\text{Building 1 Roof Area (SF)} + \text{Building 2 Roof Area (SF)}} = \text{Building 2 Percentage (B2\%)}$$

$$\text{Ex: } \frac{7,500}{3,100 + 7,500} = 70\%$$

Step Two



$$(\text{Building 1 Height (FT)} \times \text{B1\%}) + (\text{Building 2 Height (FT)} \times \text{B2\%}) = \text{Average Height (FT)}$$

$$\text{Ex. } (45' \times .30) + (30' \times .70) = 35'$$

D. Building Types

Building type standards address key building elements as illustrated in Figure 3. Calculating the maximum allowed footprint per story is illustrated in Figure 4.

Figure 3. Key Building Elements

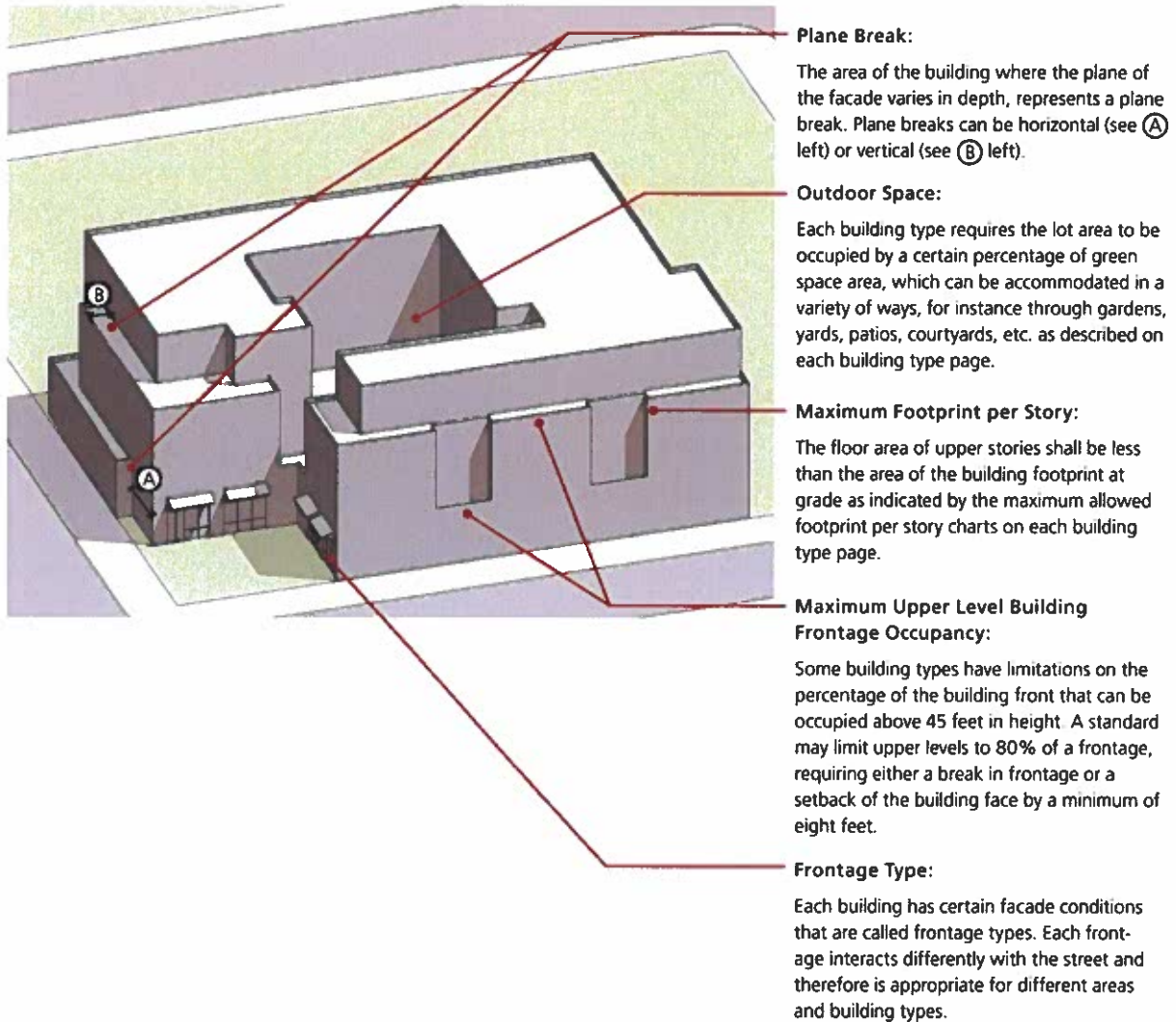
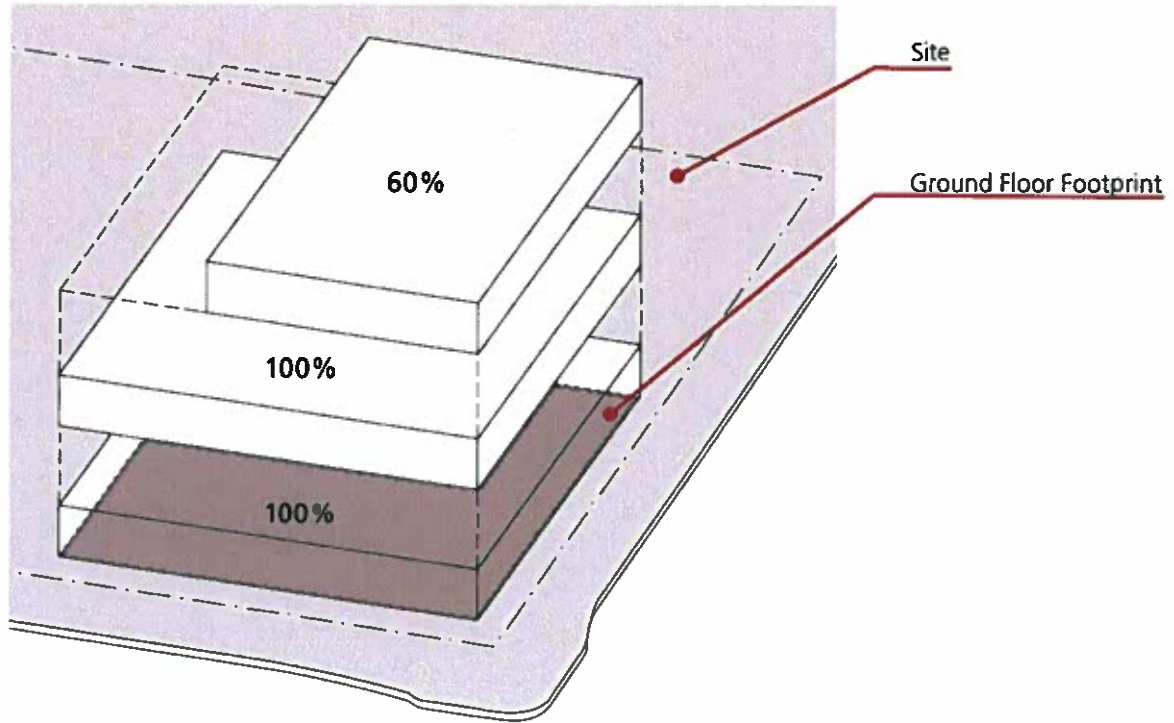
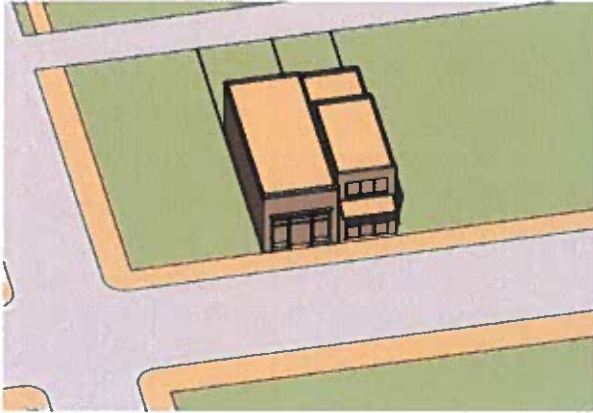


Figure 4. Maximum Allowed Footprint per Story



The maximum footprint per story is computed based on the building's ground floor footprint, not the overall site area.

I. Main Street Retail/Office



Three-dimensional diagram

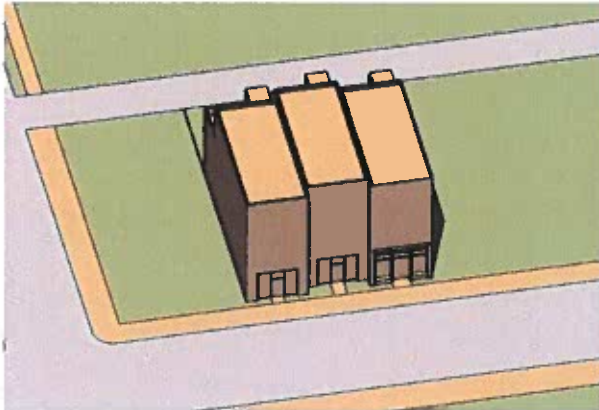


Example of Main Street Retail/Office

Intent Statement
A building designed for occupancy by retail, service, and/or office.
Location
<ul style="list-style-type: none"> Commercial Village Avenue
Building Height
25 feet min. or 1 story with mezzanine
Maximum Facade Width
50 feet
Maximum Upper-Level Frontage Occupancy
Not Applicable
Maximum Allowed Footprint per Story
Not Applicable
Primary Facades
<ul style="list-style-type: none"> Street-facing
Landscaping
<ul style="list-style-type: none"> Hardscape and potted plants, only, for front yards. Landscape materials to be approved by the City.

Open Space		
<ul style="list-style-type: none"> When provided, open space shall be at least 10 feet in length. 		
Primary Frontage Types		
<ul style="list-style-type: none"> Arcade/Gallery Stoops Shopfront 		
Primary Entry Location		
<ul style="list-style-type: none"> Street 		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Concealed from street view behind bldg.
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

2. Live/Work



Three-dimensional diagram

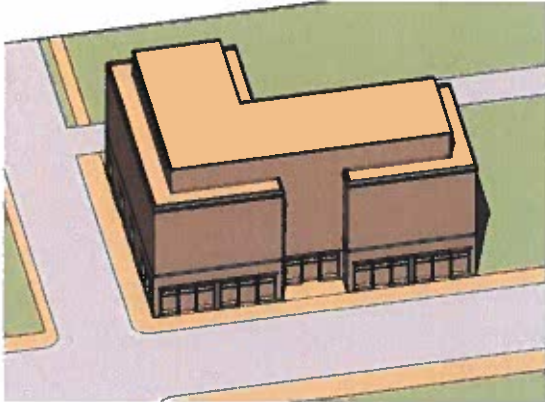


Example of Live/Work buildings

Intent Statement				
<p>An integrated residence and work space, occupied by a single unit. Often two or more such units shall be arranged side by side along the principal frontage that has been designed or structurally modified to accommodate joint residential and work occupancy. Live/work buildings may also wrap the podium of a high-rise building type. To ensure an appropriate ratio of residence to work space, the following standards must be met:</p> <ul style="list-style-type: none"> • The minimum size of a single unit is 470 sf. The maximum size of a single unit is 5,000 sf. • The residential area of a unit may not exceed the lesser of 50% of the total unit size. 				
Location				
<ul style="list-style-type: none"> • Node • Avenue • Commercial Village 				
Building Height				
Min.	25 feet or 2 stories			
Max.	50 feet or 4 stories in Node			
Maximum Facade Width				
Max.	30 feet per unit; 10 units per facade string			
Maximum Upper-Level Frontage Occupancy				
Not Applicable				
Maximum Allowed Footprint per Story				
Stories	1	2	3	4
2	100%	100%	-	-
3	100%	100%	80%	-
4	100%	100%	80%	80%

Primary Facades		
<ul style="list-style-type: none"> • Street-facing 		
Landscaping		
<ul style="list-style-type: none"> • Landscaping may not visually obstruct Shopfront or work space. • Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> • When provided open space shall be at least 10 feet in dimension. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Arcade/Gallery • Live/Work • Shopfront • Stoops 		
Primary Entry Location		
Street; except residential entries may be accessed through work space, through a paseo between units, or from the rear		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Concealed from street view behind bldg.
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

3. Mixed Use



Three-dimensional diagram

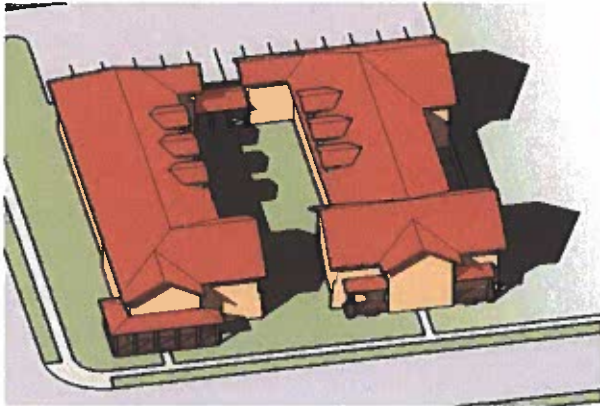


Example of Mixed-Use Building

Intent Statement			
A building designed for occupancy by retail, service, office, and/or residential uses on the ground floor, with upper floors also configured for office and/or residential uses; however two-story retail is permitted as long as the development complies with applicable mixed-use standards in Section 906.			
Location			
• Node		• Commercial Village	
Building Height			
Min.	2 stories		
Façade Width			
Max.	225 feet: >175 feet must have at least 1 facade break of at least 20 feet in length and 10 feet in depth		
Maximum Upper-Level Frontage Occupancy			
Portions of facades above 45 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).			
Maximum Allowed Footprint per Story			
Stories	1-3	4	5
2-3	100%	-	-
4-5	100%	85%	55%
Primary Facades			
Street-facing			

Landscaping		
<ul style="list-style-type: none"> Landscaping may not visually obstruct Shopfront or office or retail space. Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> When provided, open space shall be at least 10 feet in dimension. 		
Primary Frontage Types		
• Arcade/Gallery		• Shopfront
Primary Entry Location		
Street; Primary entrances to upper floors shall be accessed through an interior courtyard or a lobby which is accessed directly from the street		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Concealed from street view behind bldg.
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

4. Courtyard Complex



Three-dimensional diagram

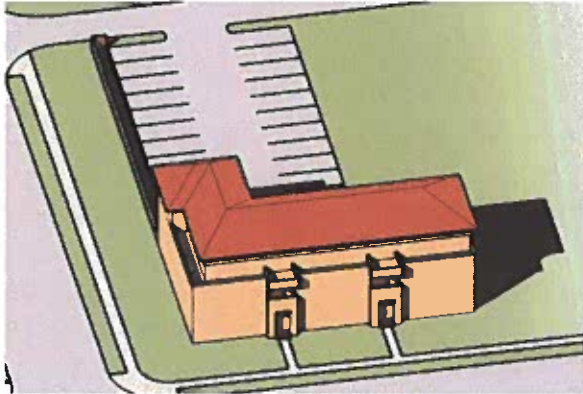


Example of Courtyard Complex

Intent Statement	
A grouping of townhouses or individual units arranged around a central courtyard or series of courtyards at grade. The uses may include residential, retail, office, or lodging.	
Location	
<ul style="list-style-type: none"> • Node • Avenue • Commercial Village 	
Building Height	
Min.	21 feet or 2 stories
Max.	40 feet or 3 stories
Façade Width	
Max.	Not Applicable
Maximum Upper-Level Frontage Occupancy	
Not Applicable	
Maximum Allowed Footprint per Story	
Not Applicable	
Primary Facades	
<ul style="list-style-type: none"> • Street-facing • Inner-Courtyard 	
Landscaping	
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 	

Open Space		
<ul style="list-style-type: none"> • Central courtyard shall average at least 20 feet in width, and at no time may be less than 10 feet in width. • Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 20-foot average width or 10-foot minimum width. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Shopfront • Dooryard 		
Primary Entry Location		
<ul style="list-style-type: none"> • Inner-Courtyard 		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Not permitted
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

5. Multi-Family Complex



Three-dimensional diagram

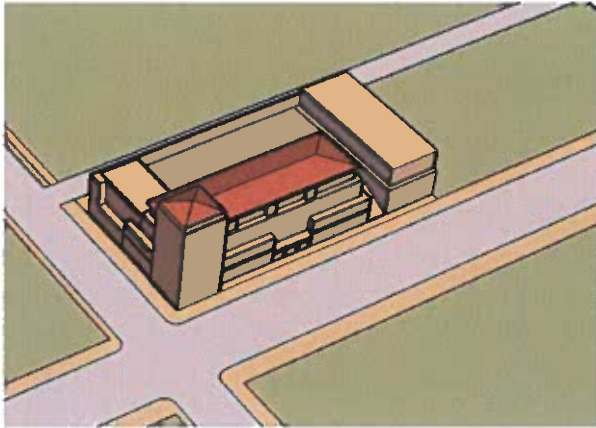


Example of Multi-Family Complex

Intent Statement		
Six units or more configured as one building, one over another, and exclusively residential.		
Location		
<ul style="list-style-type: none"> • Avenue 		
Building Height		
Min.	2 stories	
Maximum Facade Width		
Max.	Not Applicable	
Maximum Upper-Level Frontage Occupancy		
Portions of facades above 40 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).		
Maximum Allowed Footprint per Story		
Stories	1-3	4
2-3	100%	-
4	100%	85%
Primary Facades		
<ul style="list-style-type: none"> • Street-facing 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 		

Open Space		
<ul style="list-style-type: none"> • Where provided, courtyards shall average at least 25 feet in width, and at no time may be less than 10 feet in width. • Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 20-foot average width or the 10-foot minimum width. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Raised terrace • Stoop 		
Primary Entry Location		
<ul style="list-style-type: none"> • Street 		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Integrated or detached
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

6. Podium



Three-dimensional diagram

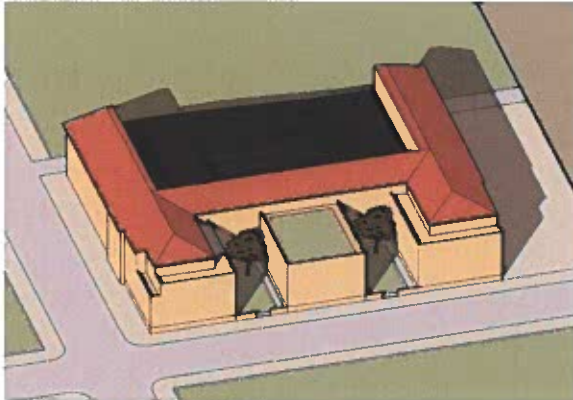


Example of Podium Building

Intent Statement			
Podium parking is typically two levels, one underground and one at grade. At-grade-level parking is wrapped by single-loaded flats or two-story units. Roof of garage is landscaped, providing usable courtyard for residents.			
Location			
• Node		• Avenue	
• Commercial Village			
Building Height			
Min.	3 stories		
Maximum Facade Width			
Max.	300 feet		
>175 feet must have at least one facade break of at least 20 feet in length and 10 feet in depth			
Maximum Upper-Level Frontage Occupancy			
Portions of facades above 45 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).			
Maximum Allowed Footprint per Story			
Stories	1-3	4	5
3	100%	-	-
4-5	100%	85%	50%

Primary Facades		
<ul style="list-style-type: none"> • Street-facing • Parking podiums at ground level and above shall be lined with habitable space along all street facing facades (alleys are excluded from this standard) 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> • Where provided, courtyards must average at least 25 feet in any dimension, and at no time may be less than 10 feet in any dimension • Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 25-foot minimum average dimension or the 10-foot minimum dimension 		
Primary Frontage Types		
• Shopfront		• Stoop
• Dooryard		• Raised terrace
Primary Entry Location		
• Street		• Podium-level Courtyard
Parking		
Type	Surface	Not Permitted
	Above-Grade	Screened by habitable spaces on all street-facing primary facades (with the exception of alleys)
Access	Alley	Permitted

7. "Wrap" Building



Three-dimensional diagram



Example of "Wrap" Building

Intent Statement			
The above-grade parking garage can be fully wrapped with building program or can be left open to a rear alley for access and ventilation. Building against garage is single-loaded corridor, while wings may be served by a double-loaded corridor.			
Location			
• Node		• Avenue	
• Commercial Village			
Building Height			
Min.	3 stories		
Maximum Facade Width			
Nodes	200 feet		
Avenue	No Maximum		
>175 feet must have at least one facade break of at least 20 feet in length and 10 feet in depth			
Maximum Upper-Level Frontage Occupancy			
Portions of facades above 45 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).			
Maximum Allowed Footprint per Story			
Stories	1-3	4	5
3	100%	-	-
4-5	100%	90%	50%

Primary Facades		
• Street-facing		
Landscaping		
• Front yard landscaping shall primarily include hardscapes and potted plants.		
• Trees/vegetation is allowed in all other yards, except front yard.		
Open Space		
• Where provided, courtyards shall be at least 25 feet in any dimension, and at no time may be less than 10 feet in any dimension		
• Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 25-foot average minimum dimension or the 10-foot minimum dimension		
Primary Frontage Types		
• Shopfront		• Stoop
• Forecourt		
Primary Entry Location		
• Street		• Courtyard
Parking		
Type	Surface	Not Applicable
	Above-Grade	Concealed from street view behind building
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

8. Special Type Building – Urban “Large Format”



Three-dimensional diagram

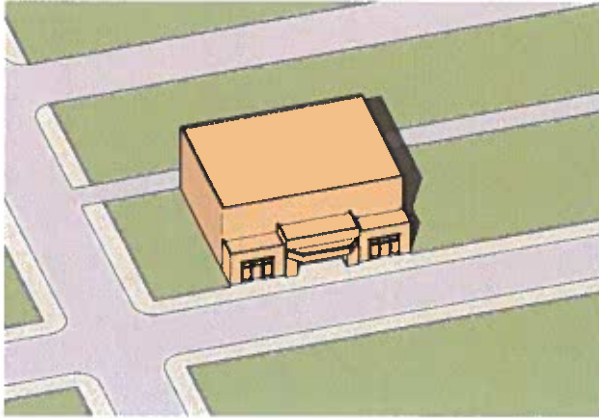


Example of Urban “Large Format” Building

Intent Statement	
<p>A multi-story building defined by its square, “L,” or “U” shape that allows for large-scale retail, office, or entertainment uses of at least 40,000 sf/floor for a single tenant, such as a grocery store or department store. Parking and loading should be designed to avoid vehicular/pedestrian conflict. Unlike “Big Boxes” in suburban areas, these typically have storefronts that create a pedestrian scaled environment and may have other uses such as office and/or residential above.</p>	
Location	
<ul style="list-style-type: none"> • Node • Avenue • Commercial Village 	
Building Height	
Min.	2 stories
Maximum Facade Width	
300 feet	
Primary Facades	
<ul style="list-style-type: none"> • Street-facing 	

Primary Frontage Types		
<ul style="list-style-type: none"> • Arcade/Gallery • Avenue-Commercial • Shopfront 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. • Parking lot landscaping shall be included to ensure that 50% of the total parking area is fully shaded within 5 years of the construction of the building. 		
Open Space		
<ul style="list-style-type: none"> • No minimum open space standard. 		
Primary Entry Location		
<ul style="list-style-type: none"> • Street 		
Parking		
Type	Surface	Not permitted
	Above-Grade	Screened by ground floor uses on all primary facades
Access	Alley	Permitted
	Secondary Street	Permitted

9. Special Type Building – Urban Theater



Three-dimensional diagram



Example of Urban Theater

Intent Statement

A multi-story building that allows for auditorium spaces of different sizes, with a large floor-to-ceiling height, as well as common lobby areas. A distinguishing feature of the theater type is that shop windows and entrances are only utilized at the entry and occasionally in secondary lobbies above the first floor. Unlike suburban-style multiplexes, “Theaters” should be lined with storefronts accommodating other retailers so as not to present a blank wall to pedestrians. While this building type is defined by its internal volumes and primary uses, it is possible to have other uses connected to it or within it, including restaurants and cafes. Where height and FAR permit, additional retail uses may be accommodated below the theater, and/or office and residential may be accommodated above.

Location

- Node
- Commercial Village
- Avenue

Building Height

Min.	2 stories
------	-----------

Maximum Facade Width

300 feet

Primary Facades

- Street-facing

Landscaping

- Front yard landscaping shall primarily include hardscapes and potted plants.

Open Space

- No minimum open space standard.

Primary Frontage Types

- Arcade/Gallery
- Shopfront

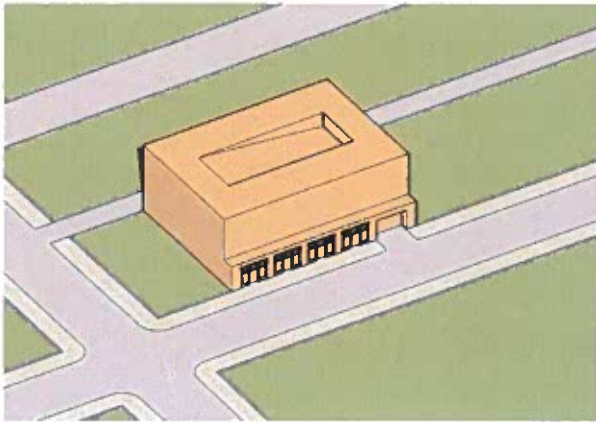
Primary Entry Location

- Street

Parking

Type	Surface	Not permitted
	Above-Grade	Concealed from street view behind bldg.; Integrated
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

10. Special Type Building – Public Parking Structure



Three-dimensional diagram



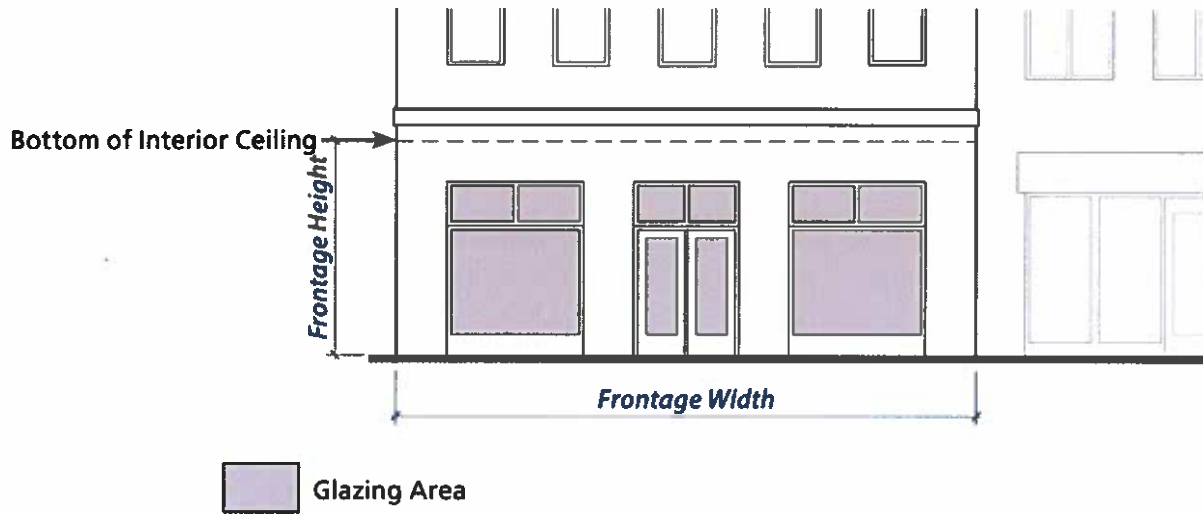
Example of Public Parking Structure

Intent Statement		Primary Facades	
An above-ground structure for vehicle public parking. This building type is designed to accommodate the need for shared public parking. Additional public uses should be considered for the top floor where views become available. Ground floor retail or office space along the edge of the building facing a public street and sidewalk are encouraged.		<ul style="list-style-type: none"> • Street-facing 	
Location		Primary Frontage Types	
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue 		<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront 	
Building Height		Landscaping	
Min.	2 stories	<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 	
Max.	45 feet or 4 stories	Open Space	
Maximum Facade Width		<ul style="list-style-type: none"> • No minimum open space standard. 	
300 feet		Primary Entry Location	
Maximum Upper-Level Frontage Occupancy		Retail	Street
Portions of facades above the first floor shall be set back 10 feet		Parking	Street, or alley when possible
Maximum Allowed Footprint per Story		Required Parking	
Not Applicable		Not Applicable	
		Surface Parking	
		Not Applicable	
		Above-Grade Parking Garage	
		Not Applicable	
		Parking Access	
		<ul style="list-style-type: none"> • Entrances and exits may be no more than 3 lanes wide or 30 feet wide at sidewalk; • Ticket and payment machines should be recessed to allow significant stacking room inside structure. 	

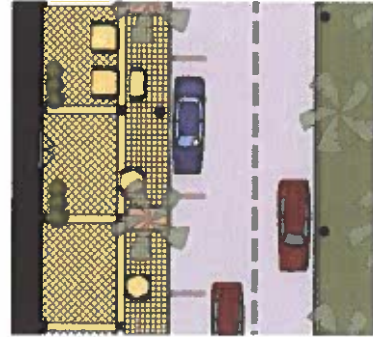
E. Frontage Type Standards

The frontage type standards and guidelines address the building-to-street relationship. Calculating the minimum frontage glazing is illustrated in Figure 5.

Figure 5. Minimum Frontage Glazing Diagram

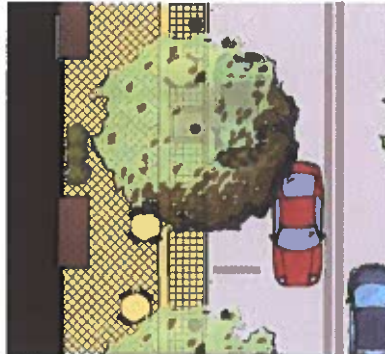


1. Arcade/Gallery



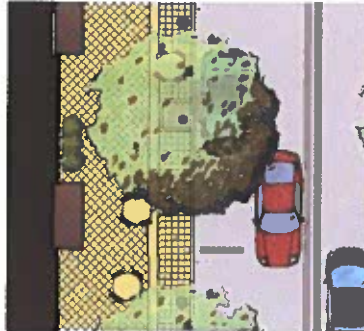
Intent Statement	
<p>A frontage wherein the facade is a colonnade that overlaps the sidewalk, while the facade at sidewalk level remains at the property line. This type is conventional for retail use. Arcades should be in alignment from building to building within a block.</p>	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
<ul style="list-style-type: none"> • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
Height to Underside of Arcade (H)	19 feet min. clear
Arcade Depth (D)	15 feet min.
Distance between Curb and face of Arcade (C)	2-4 feet
Paving and Landscaping	
<p>Area between the property line and building face shall be paved.</p>	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • Locate columns so they do not align with the entry doors of ground floor tenants. • Where an arcade does not wrap around a building's corner, open its end to facilitate the pedestrian path. • Arcades should not be used unless they encompass the primary pedestrian path of travel along the sidewalk. • Shading devices that obstruct views into the arcade from the street may not be used. • At least 60% of the arcade facade area at the ground floor shall be glazed, with at least 70% of the glazing to allow views into the store rather than being shallow window box displays (see Figure 5); Glazing shall be transparent and clear; Opaque, highly reflective, and dark tinting are not permitted. 	

2. Shopfront



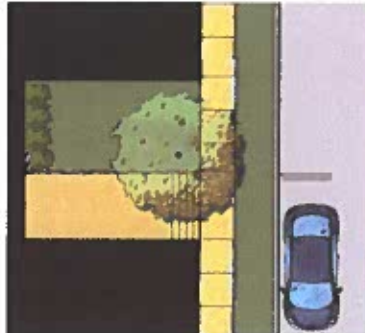
Intent Statement	
Shopfront frontages provide direct access to ground-floor spaces that are located adjacent to the sidewalk. Shopfronts are typically associated with retail uses but may accommodate other uses.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
<ul style="list-style-type: none"> • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
Shopfront Floor-to-Ceiling Height	15-21 feet
Paving & Landscaping	
Area between the property line and the building face shall be paved	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • At least 60% of the shopfront facade area at the ground floor shall be glazed, with at least 70% of the glazing to allow views into the store rather than being shallow window box displays (see Figure 5); Glazing shall be transparent and clear; Opaque, highly reflective, and dark tinting are not permitted. • Storefront window sill height may be no more than 30 inches high measured from the adjacent finished sidewalk • Unoccupied storefronts may be temporarily covered from the inside with white or light color paper, fabric or film, which may contain a graphic image or otherwise permitted sign • Maximum length of blank walls facing the street is limited to 15 horizontal feet for any one stretch 	

3. Live/Work



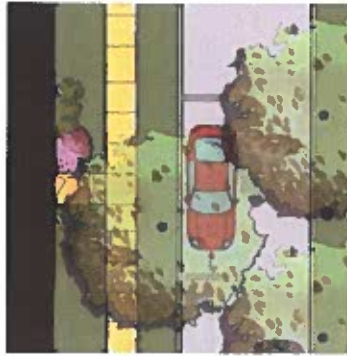
Intent Statement	
Live/Work frontages provide direct access to ground-floor spaces that are located adjacent to the sidewalk. Live/Work frontages may be associated with retail, offices or other work spaces.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue 	
Entries	
<ul style="list-style-type: none"> • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
Live/Work – Floor-to-Ceiling Height	10 feet min.
Paving & Landscaping	
Area between the property line and the building face shall be paved	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • At least 35% of the Live/Work facade area at the ground floor shall be glazed (see Figure 5). Opaque, highly reflective, and dark tinting are not permitted. Glazed garage doors may satisfy this standard. • Live/Work window sill height may be no more than 30 inches high measured from the adjacent finished sidewalk. • Unoccupied Live/Work may be temporarily covered from the inside with white or light color paper, fabric, or film, which may contain a graphic image or otherwise permitted sign. • Maximum length of blank walls facing the street is limited to 15 horizontal feet for any one stretch. 	

4. Forecourt



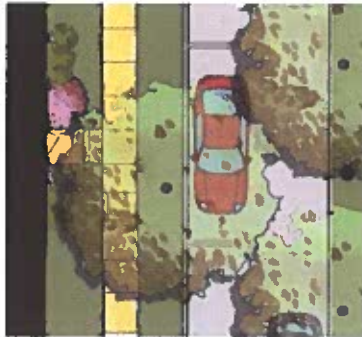
Intent Statement	
Forecourts are open areas located at primary building entrances. They may be designed as gardens or as paved courtyards. Frontages using a forecourt must comply with minimum frontage occupancy standards.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue 	
Entries	
Forecourt	Sidewalk
Building	3 feet max above adjacent forecourt
Dimensions	
Height	18 in. max above adjacent sidewalk
Depth	10-40 feet
Width	20-40 feet
Paving & Landscaping	
Forecourts may be planted with grass, shrubs, or other ground cover or be paved. All walks shall be paved.	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays • Planter pots • Water features 	
Additional Standards	
<ul style="list-style-type: none"> • Forecourts shall be open to the sky; porches are not permitted • Forecourts may be gated • Forecourts are not intended for access by automobiles • Minimum facade occupancy standards are not applicable where forecourts are used 	

5. Stoop



Intent Statement	
Stoops are small staircases leading to the entrance of a building. The stoop elevation provides some privacy between the sidewalk and ground-floor uses. Stoops may be covered.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
<ul style="list-style-type: none"> • Sidewalk 	
Dimensions	
Height	3 feet max above adjacent sidewalk
Depth	4 feet min.
Width	4 feet min.
Paving & Landscaping	
Yards should be planted with grass, shrubs, or other ground cover. Walks shall be paved.	
Furnishing Zone	
Not Permitted	
Additional Standards	
<ul style="list-style-type: none"> • Awnings or canopies may cover stoops. • Elements from dooryard frontages and stoop frontages may be combined. 	

6. Dooryard



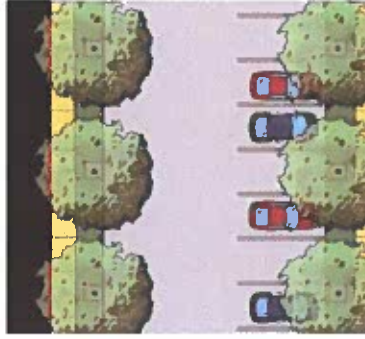
Intent Statement
Dooryard fronts are located in front setbacks and provide small landscaped and paved yards at buildings entrances. Dooryards are often enclosed by low walls, fences or hedges.
Location
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue
Entries
Ground-floor units in multi-family buildings with corridors may have the primary entry from a corridor accessible from a common building lobby, directly from the sidewalk via a dooryard, or both.
Dimensions
Not Applicable
Paving & Landscaping
Dooryards shall be planted with grass, shrubs, or other ground cover. Walks shall be paved. Low retaining walls, fences, or hedges may not exceed 3 feet in height measured from the sidewalk.
Furnishing Zone
<ul style="list-style-type: none"> • Loose Furniture
Additional Standards
Elements from dooryard frontages and stoop frontages may be combined.

7. Avenue – Residential



Intent Statement
A large front yard between a building face and sidewalk that provides a buffer and privacy for the building. A canopy or awning covering the walkway to the sidewalk provides a gracious entry to the building.
Location
<ul style="list-style-type: none"> • Avenue
Entries
Ground-floor units in multi-family buildings with corridors may have the primary entry from a corridor accessible from a common building lobby, directly from the sidewalk via a front yard, or both.
Dimensions
10-20 feet from property line to building face
Paving & Landscaping
Front yards shall be planted with grass, shrubs, or other ground cover. Walks shall be paved. Low retaining walls, fences, or hedges may enclose a front yard. Walls and hedges may not exceed 3 feet in height measured from the adjacent sidewalk.
Furnishing Zone
<ul style="list-style-type: none"> • Loose furniture
Additional Standards
<ul style="list-style-type: none"> • None

8. Avenue – Commercial



Intent Statement	
A street frontage facing a parking lot. Here, the parking lot shall be buffered from the public right of way with landscaping or wall separating the public realm from the private retail	
Location	
<ul style="list-style-type: none"> • Node 	<ul style="list-style-type: none"> • Commercial Village • Avenue
Entries	
<ul style="list-style-type: none"> • Avenue • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
No greater than 75 feet from property line to building face	
Paving & Landscaping	
Area between the property line and the building face shall be paved	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • At least 60% of the shopfront facade area at the ground floor shall be glazed with at least 70% of the glazing to allow views into the store rather than being shallow window box displays (see Figure 5); Glazing shall be transparent and clear; Opaque, highly reflective, and dark tinting are not permitted. • Storefront window sill height may be no more than 30 inches high measured from the adjacent finished sidewalk. • Unoccupied storefronts may be temporarily covered from the inside with white or light color paper, fabric, or film, which may contain a graphic image or otherwise permitted sign. • Maximum length of blank walls facing the street is limited to 15 horizontal feet for any one stretch. 	

909 Architectural Standards

A. Purpose

The Architectural Standards regulate building orientation, access, projections, ceiling heights, and sustainability features.

B. Applicability

This section applies to all areas within the Incentive District.

C. General Architectural Standards

1. Building Orientation

- a. **Primary Facades.** All new or modified buildings shall orient the primary facade to the primary street. Secondary Facades front side and rear yards.
- b. **Corner Buildings.** Buildings at corners shall have primary facades fronting the Primary Street per Map 3 and confirmed by the City Planner. On some sites, corner buildings will front two Primary Streets. In these cases, both facades facing Primary Streets shall be considered primary.

2. Ground-Level Floor-to-Ceiling Heights

When not specified by the applicable frontage type in Section 908 (E), the minimum retail ground-floor-to-floor height shall be 15 feet. Minimum residential ground-floor-to-ceiling height shall be 10 feet.

3. Pedestrian Access

- a. **Primary Pedestrian Access.** Primary pedestrian access to building lobbies shall be provided along the sidewalk or permitted open spaces.
- b. **Mid-Block Paseos or Open Space.** Parcels longer than 300 feet shall be developed with a mid-block pedestrian paseo or open space. Where paseos are utilized, the placement shall emphasize connectivity to other paseos, alleys, or mid-block crossings.
- c. Pedestrian paseos may be no less than 15 feet wide.
- d. If pedestrian paseos are covered, they shall have a floor-to-ceiling height of at least two times their width, but no greater than three times their width.
- e. **Primary Entries.** Primary entries to ground floor tenant spaces or individual residential units shall be directly accessed from the sidewalk and occur at a maximum interval of 60 feet along primary streets.
- f. **Retail Frontages.** All frontages with retail uses shall provide tenant spaces with their own entry from the sidewalk, publically accessible open space or pedestrian paseo.

- g. Multiple entrances shall be incorporated for storefronts along a street front whose length spans the width of more than 90 feet. Where multiple entrances are required by this standard, they should be no more than 60 feet from the centerline of one entrance to the center-line of the other.
- h. Automobile drop-offs and porte cocheres may not be permitted along Primary Frontages where an alley is available. Exceptions may be made subject to the following findings and approval by the City Planner:
 - i. The project provides a community benefit in the form of public parking or public open space.
 - ii. Conflict points with pedestrians and vehicles are reduced to the extent feasible.
 - iii. All parking and service access areas are provided via the alley.
 - iv. Alternative surfacing materials and landscape areas clearly define the driveway entrance from the sidewalk and parking lanes.
 - v. Such street frontage elements do not compromise the viability of street-adjacent landscape areas.
- i. Parking is not permitted along any Primary Frontage.

4. Building Walls at the Ground Floor

When not specified by the applicable frontage type in Section 908 (E), along primary frontages, blank ground floor walls (defined as having no active use, glazing, or doorway) are limited to 20 percent of the linear building frontage, but in no case exceeding 40 feet of linear frontage.

5. Loading and Service

- a. Trash enclosures and retail loading areas shall be provided off of an alley where practical.
- b. Service, utility and mechanical equipment that is visible from the street should be screened from view with landscaping or enclosures.
- c. Back flow and standpipes, along with utility box transformers shall be screened.
- d. Chain link fencing with slats or mesh screen and unpainted wood are not permitted as screening materials.
- e. Mechanical equipment shall vent to an alley where practical.

6. Fenestration

- a. Ground Floor

- i. Provide large windows and window display boxes along ground floor commercial spaces to activate the street and allow pedestrians to view the merchandise for sale, restaurant interiors, dance classes, art galleries, artists at work, etc. At least 60 percent of Storefront Frontage shall be glazed.
 - ii. Use glazing that has limited UV tinted glazing so as to provide views into the building from the street. Glazing at ground-level storefronts and offices shall have a Visible Light Reflected Out value of 12 percent or less, and a Visible Transmittance value of 0.65 or more. Developers seeking relief from this standard based on Title 24 calculations shall provide those calculations to the City Planner as evidence that all other alternative measures to comply with Title 24 requirements, such as the use of shading devices, are insufficient.
 - iii. The ground floor window sill height may be no higher than 30 inches above the segment of sidewalk immediately adjacent to the window.
 - iv. Storefront windows shall be recessed at least 6 inches from building face.
 - v. Interior floor displays shall not obstruct views into the store.
 - vi. Exterior store displays against storefronts may not exceed 36 inches above adjacent sidewalk so as to not obstruct views into the store.
- b. Upper Level
- i. Upper-level glazing shall have a Visible Light Reflected Out value of 15 percent or less and a Visible Transmittance value of 50 percent or more.

7. Roofs

- a. Rooftop mechanical equipment, including roof vent penetrations, shall be screened from public view.

8. Projections

No projection may extend into the public right-of-way except as provided below. Projections into the public right-of-way shall be limited as follows:

- a. Awnings/Canopies. Awnings and canopies may project no more than 8 feet from the building face or 33 percent of the distance between the building face and the curb, whichever is less. The minimum vertical clearance for awnings and canopies shall be 8 feet if removable or retractable and 12 feet if fixed or permanent.

Awning and canopy heights on a building shall be designed to be consistent along the facade and/or frontage line so as to maintain a consistent street edge.

- b. Theater Marquees. Theater marquees may not project over the public sidewalk within 6 feet of the curb. Signage may not project more than 3 feet from the building face along block frontages where the distance from the curb to the building face is less than 18 feet in width and no more than 4 feet along block frontages where the distance between the curb and the building face is over 18 feet in width. If signage sits on the ledge of a canopy, then the guidelines for canopies shall apply (see above).
- c. Balconies. Balconies may not project into required setbacks by more than 7 feet from the building façade and may not extend beyond the property line.
- d. Projecting Habitable and Non-Habitable Space. The minimum vertical clearance below permitted Projecting Habitable and Non-Habitable Space shall be equivalent to the floor-to-floor ground floor height standard. The combined length of Projecting Habitable and Non-Habitable Spaces along the building face may not exceed 67 percent of the total length of the building face to which they are attached.

9. Encroachments

Encroachments shall be limited as follows:

- a. Stoops. Stoops are permitted for ground floor residences only, setbacks from the property line, and the grade change for the first floor residents as doors and windows are raised from the sidewalk grade.
 1. Stoops may not extend more than 8 feet from the building face and may not extend beyond the property line.
 2. The finished floor of the stoop may be a maximum of 42 inches above the adjacent sidewalk grade.
 3. Fences or walls defining the stoop may not exceed the height required by the building.
 4. Stoops may be parallel or perpendicular to the sidewalk from which they are accessed.
 5. Where they extend beyond the front setback line and run parallel to the sidewalk, the total length of the stoop or stoops may not exceed 40 percent of the total building frontage.
 6. Where they are perpendicular to the sidewalk, they may be combined with non-encroaching raised planters or terraces.
- b. Outdoor Seating, Dining and Temporary Display. These areas may encroach into the front setback, but may not encroach into the public right of way unless a clear area of travel is maintained of at least 6 feet in width not encroached upon by tree grates or street furniture.

- c. Raised Terraces. Raised terraces adjacent to storefronts with dining areas shall be a minimum of 7 inches in height. Raised terraces may be no taller than 42 inches as measured from the sidewalk grade. Raised terraces are permissible for restaurant and hotel uses located mid-block and behind the required minimum front yard setback line.
- d. Railings. Any railings that may be required by building code or desired windscreens shall be at least 80 percent transparent.
- e. Underground Parking. Underground parking may extend past the minimum front yard setback line, up to the property line.

10. Architectural Lighting

- a. Building Lighting and Fixtures
 - i. Frontages, entrances, arcades pathways, corner plazas, and adjacent public sidewalks shall be illuminated for pedestrian safety. Lighting may be freestanding or attached to the building. Lighting provided for pedestrian safety shall be mounted between 10 and 14 feet above the ground. Lighting along alleys shall be connected to a separate circuit.
 - ii. Lighting sources shall be shielded so that light is aimed downward or back at the building wall.
 - iii. Incandescent and high-pressure sodium exterior lights are prohibited.

910 Large Lot Standards

A. Purpose

- 1. The purpose of this section is to provide standards for the development and redevelopment of large parcels or assemblages.

B. Applicability

Any parcel or parcel assemblage with a contiguous area of 60,000 sf or more shall be developed according to these Large Lot Standards.

C. Submittal Standards

An application for a Development Plan review qualifying under this section shall include, at a minimum, a plan sheet with diagrams and text which identifies proposed individual building sites and their dimensions, existing adjacent streets, proposed new streets, proposed building types, proposed frontage types, and the relationship of the project site to its surroundings.

D. Relationship to Land Use Standards, Urban Standards, and Architectural Standards

- 1. Each building within a large lot development shall comply with the applicable standards in the Land Use Standards, Urban Standards, and Architectural Standards. However, standards shall be amended as follows:

- a. **Primary Frontage Line:** For buildings not abutting an existing street, the plan shall introduce a new street, alley or pedestrian paseo that is open to the sky for at least 50 percent of its length and accessible to the public at least during daylight hours and a proposed Primary Frontage Line along that street or paseo. Buildings proposed to front the new street or pedestrian paseo are not subject to any setback standards from the new street or paseo.
- b. Development on parcels or parcel assemblages exceeding 90,000 sf shall be subject to Section 910 (C) (1) a, above, and be composed of at least two distinct buildings (not connected via interior circulation, except as part of a subterranean parking structure) which may be the same or different building types as allowed by the applicable subarea.
- c. Frontage Types shall be permitted based upon the applicable subarea in which the project is located.

E. Access

- 1. In cases where parcel sizes exceed 90,000 sf, development shall provide for new streets and/or pedestrian paseos, according to the rules below and as shown in Figure 6.
 - a. Vehicular and pedestrian access shall include new internal streets, alleys, or driveways. Cul-de-sacs and dead-end streets are prohibited except where public utility constraints (e.g., railroad right-of-ways) prohibit through streets. Alleys or woonerfs (i.e., roads in which devices for reducing or slowing the flow of traffic have been installed) may be dead-end if they allow for future connection to adjacent parcels. Garage doors shall face alleys, woonerfs, or driveways. Flag lots are prohibited.
 - b. Parcels over 90,000 sf shall require at least one publicly accessible bisecting street, alley, or woonerf. Where such parcels border an existing public street and an existing alley sufficient to service the entire parcel, then the site may be partially bisected by a pedestrian paseo (minimum 15 feet wide between primary building walls) that is open to the sky for at least 50% of its length and accessible to the public at least during daylight hour in lieu of a street, alley, or woonerf.
 - c. Lots over 125,000 sf shall require at least one publicly accessible street. If the resulting subdivision includes parcels over 125,000 sf, they too shall require one publicly accessible bisecting street. If the resulting subdivision includes parcels over 90,000 sf, they too shall require one publicly accessible bisecting street, alley, or woonerf or, where conditions apply, may be partially bisected by a pedestrian paseo (see above for allowable conditions) as per Section 910 (D) (1) b, above.

- d. New streets shall be designed to emphasize the pedestrian experience. Roadway widths shall be the minimum deemed acceptable by the City Engineer and Fire Department.

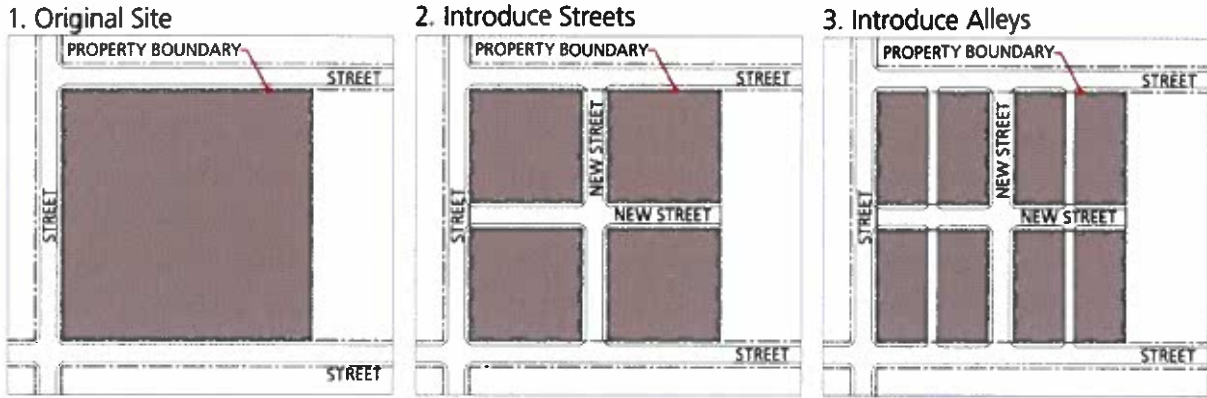


Figure 6. Large Lot Development

911 Parking Standards

A. Applicability

All development and redevelopment established under the Incentive District shall comply with the parking standards contained in Article 31 Off-Street Parking and Loading Regulations of the 1992 Ordinance, with the exception of the provisions contained in subsections (B) and (C) below.

B. Minimum Parking Standards

Table 4. Required Parking Spaces

Use	Parking Spaces Required
Nonresidential Uses ¹	1 for each 500 sf of gross floor area
Artisan Manufacturing	1 for each 800 sf of gross floor area
Eating and Drinking Establishment ²	1 for each 125 sf of seating area
Visitor Accommodations	1 per unit
Personal Services	1 for each 600 sf of gross floor area
Residential ⁴	1 parking space per 1,500 sf of habitable space ³
Notes:	
1) The nonresidential use category shall apply to all nonresidential uses within the Incentive District except those explicitly listed in Table 4 or in this footnote.	
2) Eating and Drinking Establishments shall include; Restaurant, Full Service; Restaurant, Fast Food; Bars and Cocktail lounges; Food and Beverage Sales; Breweries, Craft; Wineries, Craft as defined in Section 912.1.	
3) The minimum parking standard is calculated from a total aggregate square footage of habitable space for the residential project.	
4) No parking spaces shall be required for housing units dedicated to affordable housing units, senior housing units, or SROs.	

C. Additional Parking Provisions

1. No parking spaces shall be required to be individually accessible. However, parking spaces may not be more than two-deep (tandem), unless they are served by mechanical systems or robotic parking that allows for independent access.
2. Shared on-site parking among land uses with different periods of peak parking demand shall be allowed for all uses in the plan area. Shared on-site parking shall be allowed to satisfy 100 percent of the parking standard for each use, so long as documentation can be provided that the existing and anticipated land uses will have different periods of peak parking demand and the shared parking can accommodate the parking demand for both uses.
3. Off-site parking within 1,250 feet shall be allowed for all uses in the plan area. Off-site parking further than 1,250 feet should be allowed at the discretion of the City Planner as long as documentation that a shuttle bus service or valet parking service will be provided.
4. Upon establishment of an in-lieu parking fee, payment of the in-lieu fee may be used to 100 percent satisfy the required interim parking standard for each parking space that is not provided.

912 Definitions

912.1 Land Use Classifications

The use classifications defined in this section are listed in Section 907 (C) of this article. Definitions identified as “New” are unique to the Coast Highway Incentive District. Definitions identified as “Existing” are defined by Article 4 Use Classifications of the 1992 Ordinance. Definitions identified as “Existing/Modified” include slight modifications from the Article 4 Use Classifications of the 1992 Ordinance.

A. Animal Sales and Service (Existing)

1. Animal Boarding. Provision of shelter and care for animals on a commercial basis. This classification includes activities such as feeding, exercising, grooming, and incidental medical care. (Existing)
2. Animal Grooming. Provision of bathing and trimming services for animals on a commercial basis. This classification includes boarding of domestic animals for a maximum period of 48 hours. (Existing)
3. Animal Hospitals. Establishments where animals receive medical and surgical treatment. This classification includes only facilities that are entirely enclosed, soundproofed, and air-conditioned. Grooming and temporary (30 days) boarding of animals is included if incidental to the hospital use. (Existing)
4. Animals: Retail Sales. Retail sales and boarding of animals provided such activities take place within an entirely enclosed building. This classification

includes grooming if incidental to the retail use, and boarding of animals not offered for sale for a maximum period of 48 hours. (Existing)

- B. Artisan Manufacturing. A facility accommodating manufacturing processes involving and/or producing: apparel; food and beverage products; electronic, optical and instrumentation products; jewelry; and musical instruments. Artisan manufacturing also includes other establishments engaged in the assembly, fabrication, and conversion of already processed raw materials into products, as well as the incidental storage, sales and distribution of such products, where the operational characteristics of the manufacturing processes and the materials used are unlikely to cause impacts on surrounding land uses or the community. Typical artisan manufacturing uses include, but are not limited to; food and beverage product manufacturing such as breweries, wholesale bakeries, and coffee roasting; printing and publishing; art and jewelry making, clothing and fabric production; metal work; furniture making; glass or ceramic production; photo/film processing; and paper product manufacturing. (New)
- C. Assembly/Meeting Facility. Meeting, recreational, or social facilities of a private or nonprofit organization primarily for use by members or guests. Includes clubs, lodges, private meeting halls, religious assembly facilities (e.g., churches, mosques, synagogues, etc.), private auditoriums, union halls, etc. This use classification excludes private schools as defined in this section. (Existing/Modified – Clubs and Lodges and Religious Assembly)
- D. Bars and Cocktail Lounges. Any premises designed, used or intended to be used for the selling or serving of alcoholic beverages to the public for consumption on the premises, and in which food is not sold or served to the public as in a bona fide restaurant. (Existing)
- E. Child Care. Non-medical care and supervision on a less than 24-hour basis in any care facility of any capacity, and not within a licensee’s home for persons under the age of 18. (Existing)
- F. Commercial Recreation and Entertainment – Indoor. Provision of participant or spectator recreation or entertainment. This classification includes bowling alleys, billiard parlors, pool rooms, dance halls, ice/roller skating rinks, scale-model courses, shooting galleries, arcades and game centers having five or more coin-operated game machines and card rooms. This does not include outdoor facilities such as amusement parks, batting cages, go-cart tracks, golf-driving ranges, and miniature golf courses. (Existing/Modified).
1. Dance Establishment. Any premises wherein a public dance, as defined in the Oceanside City Code, is held. (Existing)
 2. Pool Rooms, Billiard Rooms, and Shooting Galleries. Pool rooms, billiard rooms, and shooting galleries as defined in the Oceanside City Code. (Existing)

3. Arcades and Game Centers. Any place having five or more coin-operated, slug-operated, or any type of amusement or entertainment machines for which payment is necessary for operation. These include, but are not limited to, pinball machines and video games, but do not include merchandise vending machines or mini-jukeboxes (see Article 36 Separation of Regulated Uses of the 1992 Ordinance). (Existing)
- G. Brewery, Craft. A small-scale beer manufacturing facility that includes designated floor area (comprising no less than 20 percent and no more than 40 percent of the total floor area) for product sampling and/or retail sales of beer conforming to state requirements. Retail sales of craft brewery merchandise including mugs, pint glasses, growlers, tap handles, coasters, apparel, signs, bottle openers, and books are also permitted. A craft brewery shall produce a minimum of 250 barrels annually and not exceed a maximum output of 15,000 barrels annually (with a barrel of beer being equal to 31 U.S. gallons). Facilities housing a craft brewery shall not exceed 15,000 square feet of floor area. The category of a craft brewery (based upon floor area and scale of operation) shall determine the appropriate review and approval process, as specified below. Such facilities shall be subject to the following limitations:
1. Other than business name and/or logo, such uses shall not display exterior signage (including advertising directed to the exterior from interior spaces) that promotes the availability of alcoholic beverages.
 2. There shall be no amusement or video machines maintained on the premises.
 3. There shall be no “happy hour” or regular periods of reduced-priced alcoholic beverages.
 4. Amplified live entertainment shall require issuance of an Administrative Use Permit.
 5. Ancillary food service involving outdoor equipment (e.g., food trucks) shall require issuance of an Outdoor Facilities Permit.
 6. Delivery/distribution activities involving Class 4 or higher vehicles shall not occur between the hours of 10 p.m. and 7 a.m.
 7. Hours of operations shall not extend beyond 10 p.m., unless extended hours of operation are approved through a Conditional Use Permit.

Craft breweries shall be regulated under the following tiered review and approval process:

1. Tier 1: Produces at least 250 and no more than 10,000 barrels annually and does not exceed 10,000 square feet of floor area. Tier 1 facilities are subject to approval of an Administrative Conditional Use Permit (ACUP) when not abutting residential zoning districts, schools, and/or churches. Tier 1 facilities abutting

residential zoning districts, schools, and/or churches are subject to approval of a Conditional Use Permit (CUP).

2. Tier 2: Produces at least 750 and no more than 15,000 barrels annually and does not exceed 15,000 square feet of floor area. Tier 2 facilities are subject to approval of a Conditional Use Permit (CUP).
- H. Cultural Institutions. Nonprofit institutions displaying or preserving objects of interest in one or more of the arts or sciences. This classification includes libraries, museums, and art galleries. (Existing)
- I. Financial Services. Financial institutions that provide retail banking services to individuals and businesses. This classification is limited to institutions engaged in the on-site circulation of cash money including businesses offering check-cashing facilities. (Existing/Modified)
1. Drive-Through/Drive-Up Service. Institutions providing self-service banking facilities that are not associated with a primary banking or savings and loan building located on the same site. (Existing)
 2. Self-Service Facilities (ATMs). Institutions providing self-service banking facilities that are not associated with a primary banking or savings and loan building located on the same site. (Existing)
- J. Food and Beverage Sales. Retail sales of food and beverages for off-site preparation and consumption. Typical uses include groceries, liquor stores, or delicatessens. Establishments at which 20 percent or more of the transactions are sales of prepared food for on-site or take consumption shall be classified as Catering Services. (Existing/Modified)
- K. Hospitals. Facilities providing medical, surgical, psychiatric, or emergency medical services to sick or injured persons, primarily on an inpatient basis. This classification includes incidental facilities for outpatient treatment, as well as training, research, and administrative services for patients and employees. (Existing)
- L. Offices, Business and Professional. Offices of firms or organizations providing professional, executive, management, or administrative services, such as architectural, engineering, real estate, insurance, investment, legal, and medical/dental offices. This classification includes medical/dental laboratories incidental to an office use, but excludes banks and savings and loan associations. (Existing)
- M. Parking as a Primary Use Land Use. Any lot or space where vehicles may be kept, sometimes for a fee, and includes public parking areas. (New)

- N. Park and Recreation Facilities. Noncommercial parks, playgrounds, recreation facilities and open spaces. Includes squares, plazas, outdoor theaters, skateparks, dog parks, trails, beaches, playgrounds, sports fields, greenbelts and other park, recreation and open space areas, including buildings and facilities that comprise the parks and recreation system of the city under the management and control of the Neighborhood Services Department. (Existing/Modified)
- O. Personal Improvement Services. Provision of instructional services or facilities, including; photography, fine arts, crafts, dance or music studios, driving, business or trade schools, diet centers or reducing salons, health and fitness studios, spas or clubs. (Existing)
- P. Health/Fitness Studios, Spas or Clubs. A fitness center, gymnasium, health and athletic club or studio, which may include any of the following: sauna, spa or hot tub facilities; indoor tennis, handball, racquetball, archery and shooting ranges, rock climbing and other indoor sports activities. Outdoor areas may be used as accessory to the primary use held indoors. (New)
1. Massage Establishments. Establishments providing massage service. (Existing)
- Q. Personal Services. Any premises primarily engaged in the provision of non-medical services for the enhancement of personal appearance, cleaning, alteration or reconditioning of garments and accessories, and similar non-business-related or nonprofessional services. These uses may also include accessory retail sales of products related to the services provided. Personal services include barber shops or beauty parlors, dry cleaning, self-service laundromats, locksmiths, tailors and shoe repair shops, tattoo parlors, and similar establishments catering directly to consumers. (Existing/Modified)
1. Tattooing Establishment. Any establishment or business engaged in “tattooing” as defined in the Oceanside City Code, excluding “micropigmentation” or “permanent cosmetic makeup” typically provided at beauty salon or day spas.
 2. Body Piercing Establishment. Any establishment or business engaged in “body piercing.” Body piercing means the creation of an opening in the body of a human being for the purpose of inserting jewelry or other decoration. This includes, but is not limited to, piercing of an ear, lip, tongue, nose, eyebrow, naval, genitals, breasts. “Body piercing” does not include piercing an ear with a disposable, single-use stud or solid needle that is applied using a mechanical device to force the needle or stud through the ear.
- R. Residential. Any building or portion thereof which is used for residential purposes, including multi-family dwelling units, live/work units, group houses, rest homes, residential care facilities, and rest homes, and accessory buildings and structures. (New)

1. Group Residential. Shared living quarters without separate kitchen or bathroom facilities for each room or unit. This classification includes roomhouse/boardinghouse, dormitories, fraternities, sororities, and private residential clubs, but excludes residential hotels (see Single-Room Occupancy (SRO) Residential Hotels). (Existing)
 2. Live/work. A mixed-use unit consisting of an office land use or retail land use with a residential land use. The habitable area may be anywhere in the unit and is intended to be occupied by a business operator who lives in the same structure that contains the office or retail activity. (New)
 3. Multifamily Residential. Two or more dwelling units on a site. This includes two-family dwelling units, three-family dwelling units, four-family dwelling units, zero lot line development, townhomes, and apartments. (New)
 4. Convalescent Facilities. Establishments providing care on a 24-hour basis for persons requiring regular medical attention, but excluding facilities providing surgical or emergency medical services. (Existing)
- S. Restaurants, Fast Food. A bona fide restaurant establishment where the principal business is the sale of prepared or rapidly prepared food and beverages to guests via counter, walk up, or window service for consumption on or off the premises. The sale of beer and wine for on-site consumption is permitted. As used in this definition, a “bona fide” restaurant shall have suitable kitchen facilities for cooking and/or preparation of meals. The word “meals” means the assortment of food commonly ordered at various hours of the day. (Existing)
1. Restaurants, Fast Food with Drive-thru or Drive-up. A restaurant establishment providing service from a building to patrons in vehicles through an outdoor service window (Drive-thru) or delivery service to vehicles parked in designated parking spaces (Drive-up). The sale and consumption of alcoholic beverages at a restaurant with a Drive-thru or Drive-up window is prohibited.
- T. Restaurant Full Service. A bona fide restaurant establishment where the principal business is the sale of food and beverages to guests via table service for consumption on the premises. The sale of beer and wine for on-site consumption shall be considered incidental to the full service restaurant. Delivery service to vehicles parked in designated parking spaces (i.e. drive-up) is allowed as an ancillary service to the Restaurant Full Service. As used in this definition, a “bona fide” full service restaurant shall have suitable kitchen facilities for cooking of complete meals. The word “meals” means the assortment of foods commonly ordered at various hours of the day; the service of only such foods as sandwiches or salads does not meet the bona fide restaurant definition.
1. Restaurants Full Alcohol. A bona fide restaurant establishment authorized to sell distilled spirits for consumption on licensed premises. The sale of liquor is

included as an appurtenant use to full service restaurants having table seating and service for more than 50 guests.

2. Restaurants Full Service with Live Entertainment – (Small Scale). Restaurant establishments providing live entertainment to patrons with 5 or fewer performers at restaurant facilities with no dance floor during typical lunch and dinner hours (11:00 a.m. – 11:00 p.m.) and having 75 percent food sales compared to alcohol sales.

U. Retail. Premises available for the sale of many lines of merchandise and food service. These include, but are not limited to: art galleries, art supplies, bakeries, book or stationary shops, cameras and photographic supplies, collectibles, dress or millinery shops, drug stores, dry goods, florist shops, gifts, sundries and souvenir shops, grocery stores, hardware stores, jewelry stores, meat markets or delicatessen stores, recreation equipment rental and sales, shoe stores convenience food store, liquor store, and secondhand store. (New)

1. Convenience Market. Retail sales of food, beverage and small convenience items typically found in establishments with long or late hours of operation. This definition excludes delicatessens and other specialty food shops having a sizeable assortment of fresh fruits and vegetables, and fresh-cut meat or fish. (Existing)
2. Drive-in facilities. Any place of business, excluding gasoline service stations, which transact any part or all of its business directly with customers within a vehicle. (New)
3. Liquor store. A place or business engaged in the primary business of off-sale alcoholic beverages. For the purposes of article, primary business shall mean 25 percent or more of the shelf area of a business. (Existing)
4. Pawnshops. Establishments engaged in the buying or selling of new or secondhand merchandise and offering loans secured by personal property and subject to Chapter 22 of the Municipal Code. (Existing)
5. Recreational equipment rental and sales. Shall mean any recreational facilities operated as a business and open to the general public for a fee. This includes bicycles, commercial fishing, diving, and sportfishing establishments including supplies and services. (Existing/Modified)
6. Secondhand Furniture, Appliances, “Collectibles,” and Clothing Sales. The retail sale of used furniture, appliances, “collectibles,” and clothing, and secondhand dealers who are subject to Chapter 22 of the Municipal Code. This classification excludes antique shops primarily engaged in the sale of antique furniture and accessories. (Existing)

- V. Schools. A public or private academic institution including school providing specialized education/training. Example include: Elementary, middle and high school, art schools, computer and electronic school, professional school (law, medicine, etc.) Also includes facilities, institutions and conference centers that offer specialized programs for personal growth and development. Does not include child care facilities (see Child Care) or Studios used for smaller-scale facilities offering specialized instruction. (New)
- W. Studios. Small-scale facilities, typically accommodating one group of students at a time, in no more than one instructional space. Larger facilities are included under the definition of Schools. Examples of these facilities include: individual and group instruction and training in the arts; production rehearsal; photography, and the processing of photographs produced only by users of the studio facilities; martial arts training studios; gymnastics instruction, and aerobics and gymnastics studios with no other fitness facilities or equipment. Also includes production studios for individual musicians, painters, sculptors, photographers, and other artists. (New)
- X. Theaters. An indoor facility for group entertainment. Examples include movie theaters and facilities for live theater and concerts. (New)
- Y. Vehicle/Equipment Sales and Services. This does not include commercial parking facilities (see Parking as a primary use). (Existing/Modified)
1. Automobile Washing. Washing, waxing, or cleaning of automobiles or similar light vehicles. (Existing)
 2. Service Stations. Establishments engaged in the retail sale of gas, diesel fuel, lubricants, parts, and accessories. This classification includes incidental maintenance and repair of automobiles and light trucks, but excludes body and fender work or repair of heavy trucks or vehicles. (Existing)
 3. Vehicle/Equipment Repair. Repair of automobiles, trucks, motorcycles, mobile homes, recreational vehicles, or boats, including the sale, installation, and servicing of related equipment and parts. This classification includes auto repair shops, body and fender shops, wheel and brake shops, and tire sales and installation, but excludes vehicle dismantling or salvage and tire retreading or recapping. (Existing)
 4. Vehicle/Equipment Sales and Rentals. Sale or rental of automobiles, motorcycles, trucks, tractors, construction or agricultural equipment, mobile homes, and similar equipment, including storage and incidental maintenance. (Existing)
 5. Vehicle Storage. Storage of operative or inoperative vehicles. This classification includes storage of parking tow-aways, impound yards, and storage lots for automobiles, trucks, buses, and recreational vehicles, but does not include vehicle dismantling. (Existing)

- Z. Visitor Accommodations. A facility that offers transient lodging accommodations to the general public, and includes campgrounds and recreational vehicle parks, hotels, motels, tourist cottages, and accessory uses. (Existing/Modified)
1. Bed and Breakfast Inns. Establishments offering lodging on a less than weekly basis in a converted single-family or multi-family dwelling, with incidental eating and drinking service for lodgers only provided from a single kitchen. (Existing)
 2. Hotels, Motels and Time-Share Facilities. Establishments offering commercial lodging on a less than monthly basis. This classification includes incidental eating, drinking, and banquet services intended for the convenience of guests. (Existing)
 3. Single-Room Occupancy (SRO) Residential Hotels. Buildings with six or more guest rooms without kitchen facilities in individual rooms, or kitchen facilities for the exclusive use of guests, and which are also the primary residences of the hotel guests. (Existing)
 4. Vacation Club. Prepaid or credit based establishments offering lodging on a less than weekly basis and having kitchens. This classification includes eating, drinking and banquet services. (Existing)
- AA. Winery, Craft. A small-scale winemaking facility that includes designated floor area (comprising no less than 20 percent and no more than 40 percent of the total floor area) for product sampling and/or retail sales of wine conforming to state requirements. Retail sales of craft winery merchandise including wine glasses, wine bottles, decanters, corkscrews, home décor, apparel, signs, books, and other wine paraphernalia are also permitted. A craft winery shall produce a minimum of 300 cases annually and not exceed a maximum output of 10,000 standard cases annually (with a standard case of wine containing twelve 750 ml bottles or nine liters of wine). Facilities housing a craft winery shall not exceed 15,000 square feet of floor area. The category of a craft winery (based upon floor area and scale of operation) shall determine the appropriate review and approval process, as specified below. Such facilities shall be subject to the following limitations:
1. Other than business name and/or logo, such uses shall not display exterior signage (including advertising directed to the exterior from interior spaces) that promotes the availability of alcoholic beverages.
 2. There shall be no amusement or video machines maintained on the premises.
 3. There shall be no “happy hour” or regular periods of reduced-priced alcoholic beverages.
 4. Amplified live entertainment shall require issuance of an Administrative Use Permit (ACUP).

5. Ancillary food service involving outdoor equipment (e.g., food trucks) shall require issuance of an Outdoor Facilities Permit.
6. Delivery/distribution activities involving Class 4 or higher vehicles shall not occur between the hours of 10 p.m. and 7 a.m.
7. Hours of operations shall not extend beyond 10 p.m., unless extended hours of operation are approved through a Conditional Use Permit.

Craft wineries shall be regulated under the following tiered review and approval process:

1. Tier 1: Produces at least 300 and no more than 7,000 standard cases annually and does not exceed 10,000 square feet of floor area. Tier 1 facilities are subject to approval of an Administrative Conditional Use Permit (ACUP) when not abutting residential zoning districts, schools, and/or churches. Tier 1 facilities abutting residential zoning districts, schools, and/or churches are subject to a Conditional Use Permit (CUP).
2. Tier 2: Produces at least 900 and no more than 10,000 standard cases annually and does not exceed 15,000 square feet of floor area. Tier 2 facilities shall be subject to approval of a Conditional Use Permit (CUP).

912.2 Architectural and Development Standards

- A. Arcade. A frontage type typical for retail uses wherein the facade is a colonnade supporting habitable space that overlaps the sidewalk, while the facade at sidewalk level remains at the primary frontage line.
- B. Bay System. The spaces between posts, columns, or buttresses in the length of a building.
- C. Blockfronts. The aggregate of private lots, passages, alleys, circumscribed by streets.
- D. Colonnade. A row of columns supporting a roof or arcade.
- E. Encroachments. An encroachment is defined as either a fixed or moveable architectural or furnishing element that projects beyond the building face at the ground level or below ground.
- F. Façade Compositional Strategy. The combining of distinct parts or elements of building facades to form a unified whole.
- G. Habitable Projecting Space. Habitable Projecting Space is a portion of a building enclosed by walls and a roof that extends beyond the building face (i.e., bay windows and other architectural projections).
- H. Massing. The perception of the general shape or form as well as the size of a building.

- I. Mixed-Use Development. Development that includes two or more land uses.
- J. Non-Habitable Projecting Space. Non-habitable Projecting Space is a space used by occupants that are not enclosed by walls and a roof, such as balconies. They should not extend more than 6 feet from the building face and should not extend beyond the property line.
- K. Porte Cochere. A vehicular passageway leading through a building or screen wall into an interior courtyard. A porte cochere can also refer to a roofed structure extending from the entrance of a building over the adjacent driveway and sheltering those getting in or out of vehicles.
- L. Pedestrian Paseo. A pathway, open or roofed, that serves as an extension of the sidewalk as it passes from the primary frontage line onto private property, often between buildings, to courtyards, parking areas, or civic spaces.
- M. Primary Frontage Line. A setback, parallel to the primary street, which marks the location from which the principle vertical plane of the building façade must be erected.
- N. Primary Street. Streets with key circulation, mix of intensities, more pedestrian and vehicular accommodation than secondary streets, most developed street, with a mix of uses and access to transit.
- O. Secondary Street. Street with single use development as opposed to mixed-use development that are fed from primary streets, have less circulation than primary street, have less mix of intensities than primary streets, and have less of pedestrian and vehicular traffic than the main streets.
- P. Standalone Residential. A residential development where residential is the primary use and other land uses such as retail or office are not included as a primary or secondary use. A standalone residential development may include open space and other uses considered incidental in a residential development project.
- Q. Stoops. Stoops are elevated entry porches with stairs paced close to the front setback line.
- R. Transoms. A transverse horizontal structural beam or bar, or a crosspiece separating a door from a window above it.
- S. Underlying Zoning. The underlying zoning district per the 1986 zoning ordinance.
- T. Visible Light Reflected Out. A value describing the percentage of total visible light that is reflected by a window glass and film system.
- U. Visible Transmittance. A value describing the percentage of visible light that can pass through a window glass and film system.
- V. Woonerfs. A road in which devices for reducing or slowing the flow of traffic have been installed.

912.3 Frontage and Building Type Terms

- A. Allowable Building Types. Defines which building types are permitted within each subarea. Allowable building types are provided in 908 (C) and (D) and the following describes the standards provided for each building type.
1. Building Intent Statement. This statement describes the intent of each building type.
 2. Locations. List of subareas where specified building types are permissible.
 3. Maximum Building Height. The maximum building height for the building type.
 4. Maximum Façade Width. The maximum dimension along a frontage of any single façade.
 5. Maximum Upper-Level Frontage Occupancy. The maximum linear dimension of building face above a specified height that may be set along the building frontage.
 6. Maximum Allowed Footprint Per Story. The maximum percentage of the site's buildable area (after excluding required setbacks) that may be constructed per story.
 7. Primary Facades. The primary orientation of the building facades.
 8. Landscaping. Landscaping standards by building type.
 9. Open Space. Any open space standards applicable to the building type.
 10. Primary Frontage Types. The primary frontage types that may be used for each building type.
 11. Primary Entry Locations. The location of the building's front door.
 12. Parking. Parking orientation standards by building type.
- B. Allowable Frontage Types. Defines which frontage types are permitted within each subarea. In some cases, the permitted frontage types may also be regulated according to the street on which they face and must also comply with the permitted frontage types of the selected building type. Allowable frontage types are provided in Section 908 (E) and the following describes the standards provided for each frontage type.
1. Frontage Intent Statement. This statement describes the building-to-street relationship that each frontage type is meant to achieve.
 2. Locations. List of subareas where specified frontage types are permissible.
 3. Entries. These standards address entries at the blockfronts, not those that are internal to the site.

4. Dimensions. Specific dimensions of features like massing, entry height, openings, and setbacks are delineated here.
 5. Paving and Landscaping. This standard addresses the area between the property line and building face.
 6. Furnishing Zone. This standard addresses furnishing within front setbacks.
 7. Additional Standards and Guidelines. These standards and guidelines provide additional direction in shaping the appropriate building-to-street relationship. They address glazing at the ground floor, frontages, and entries (see Figure 5).
- C. Maximum Building Height. Identifies the maximum height buildings in each subarea are not allowed to exceed.
 - D. Maximum Average Building Height. Requires buildings to create a varied “street wall” by requiring portions of a development to have different heights to meet the average standard. In no case can any portion of the development exceed the Maximum Building Height standard. Figures 1 and 2 identify how average building height shall be calculated to determine compliance with the standard.
 - E. Minimum Frontage Occupancy. Is the minimum percentage of a blockfront at which a building frontage is set either at or within the zone established by the minimum and maximum Setback Lines, as shown in the Setback Regulating Plan, Map 3. The Minimum Frontage Occupancy is measured as a linear distance parallel to the property line. The remaining frontage length may be set behind the Maximum Setback Line or may be left unoccupied.



SOURCE: ESA, 2016; City of Oceanside, 2016; NAIP, 2014

City of Oceanside Coast Highway Corridor Stud. 130217

Map 2
Coast Highway Incentive District
 Sub-Area Plan



SOURCE: ESA, 2016; City of Oceanside, 2016; NAIP 2014

City of Oceanside Coast Highway Corridor Stud. 130217

Map 3
Coast Highway Incentive District
 The Setback Regulating Plan



SOURCE: ESA, 2016; City of OceanSide 2016; NAIP, 2014

City of OceanSide Coast Highway Corridor Stud. 1302.17

Map 4
Coast Highway Incentive District
Residential Incentive Program Area

RESOLUTION NO.

**A RESOLUTION OF THE CITY COUNCIL AND
COMMUNITY DEVELOPMENT COMMISSION OF THE
CITY OF OCEANSIDE APPROVING LCPA19-00005 TO
INCORPORATE THE COAST HIGHWAY INCENTIVE
DISTRICT INTO THE IMPLEMENTING DOCUMENT OF
THE CITY'S LOCAL COASTAL PROGRAM**

**(City of Oceanside –Applicant)
(ZA19-00004/LCPA19-00005)**

WHEREAS, the California Coastal Act (Public Resources Code §30000, et seq.) (the "Coastal Act") requires that the City adopt a Local Coastal Program (LCP) which meets the requirements of the Coastal Act at the local level and implements its provisions and policies; and

WHEREAS, on January 25, 1985, the California Coastal Commission ("Commission") approved with suggested modifications, the City's Land Use Plan ("LUP") and, pursuant to Public Resources Code §30512.2, found the City's LUP to be consistent with the policies and requirements of Chapter 3 of the Coastal Act and to meet the basic stated goals specified in Public Resources Code §30001.5; and

WHEREAS, on December 8, 2008, the Commission established with the City of Oceanside that development proposals in those portions of the Coastal Zone located outside of the Downtown Redevelopment Area would be reviewed for consistency under the standards of the City's 1986 Zoning Ordinance, in light of the fact that the previously applicable 1992 Zoning Ordinance had never received CCC certification; and

WHEREAS, on May 11, 2009, the City acknowledged in correspondence to the Commission an obligation to use the 1986 Zoning Ordinance as the standard for review of development proposals within those portions of the Coastal Zone located outside of the Downtown Redevelopment Area; and

1 WHEREAS, pursuant to the California Environmental Quality Act of 1970 and the
2 State Guidelines thereto, an Environmental Impact Report was prepared for the proposed
3 project; and

4 WHEREAS, on May 22, 2019, the Planning Commission conducted a duly-noticed public
5 hearing as prescribed by law and recommended City Council approval of said zoning text
6 amendment and Local Coastal Program amendment by unanimous vote; and

7 WHEREAS, the City Council and Community Development Commission conducted a
8 joint duly-noticed public hearing on August 14, 2019, to consider Zone Amendment (ZA19-
9 00004) and Local Coastal Program Amendment (LCPA19-00005), and the recommendation of
10 the Planning Commission thereon, and heard and considered written and oral testimony
11 regarding the proposed amendments; and

12 WHEREAS, the City Council/Community Development Commission finds the proposed
13 modification to the Local Coastal Program to be adequate to carry out the land use plan of the
14 Local Coastal Program.

15 NOW, THEREFORE, the City Council/Community Development Commission of the
16 City of Oceanside DOES RESOLVE as follows:

- 17 1. Pursuant to Public Resources Code §30510(a), the Oceanside City Council hereby
18 certifies that the Local Coastal Program Amendment (LCPA19-00005) is intended to be
19 carried out in a manner fully in conformity with the Coastal Act, and is hereby adopted.
- 20 2. Pursuant to Coastal Commission Local Coastal Program Regulations §13551(b), this
21 Local Coastal Plan Amendment shall take effect upon Commission concurrence.
22

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1 3. Notice is hereby given that the time within which judicial review must be sought on the
2 decision is governed by Public Resources Code §30801.

3 PASSED AND ADOPTED by the City Council/Community Development Commission
4 of the City of Oceanside, California, this 14th day of August, 2019, by the following vote:

5 AYES:

6 NAYS:

7 ABSENT:

8 ABSTAIN:

9
10 _____
MAYOR OF THE CITY OF OCEANSIDE

11
12 ATTEST:

APPROVED AS TO FORM:

13
14 _____
CITY CLERK

15
16 _____
CITY ATTORNEY

J-3 Local Coastal Program Land Use Plan Amendments

Chapter Three-

LAND USE PLAN and
PERMIT REGULATIONS

I. COASTAL ZONE LAND USE CLASSIFICATIONS

The City's Land Use Element of the General Plan provides a basic framework for the City's LCP Land Use Plan Map (see Exhibit A). Several modifications to land use have been proposed in response to Coastal Act requirements. These include:

-Shorefront properties from The Strand south to Cassidy Street are shown as "Mixed High Density/Transient Residential". This designation was previously applied to portions of The Strand by the Redevelopment Agency, and is intended to allow a mixture of both permanent residential and transient residential uses (such as seasonal rentals, tourist cottages, hotels and motels).

-As required by the Coastal Act, the Land Use Plan makes a distinction between "general commercial" uses and "coastal dependent, recreation and visitor serving commercial" uses. The latter specialized commercial uses are concentrated near the pier (as proposed in the Redevelopment Neighborhood Plan), the Harbor and the San Luis Rey River area.

-A specialized transportation and utility designation is proposed for the AT&SF Railroad right-of-way and the La Salina Treatment Plant. This is more reflective of existing uses than the current "institutional open space" designation.

-Several modifications to residential densities have been proposed based on Coastal Project Committee recommendations, and findings of the draft LCP Housing Study. The most significant of these changes is an increase in densities on portions of the San Luis Rey River Specific Plan area.

The proposed Coastal Zone Land Use designations consist of both a map (Exhibit A) and the narrative text, which follows. A breakdown of land use categories by acreage is provided in Appendix D.

A. COMMERCIAL LAND USES

1. General Commercial – The ~~general~~ General Commercial ~~commercial~~ category allows a variety of retail, service, and office uses. This land use category also allows for residential uses consistent with base zoning standards and the Coast Highway Incentive District (ID). Residential uses are allowed on all General Commercial properties in conjunction with minimum commercial floor area as specified in the ID or as determined through the discretionary review process. As indicated in the ID, General Commercial properties within "avenue" segments of the Coast Highway corridor may accommodate standalone residential uses. Visitor uses, such as restaurants, hotels and motels may be located in this designation, especially on sites with good freeway access and exposure. The major general commercial corridor in the Coastal Zone is along Hill Street.
2. Coastal Dependent, Recreation, and Visitor Serving Commercial – This land use category encompasses specialized commercial uses which are directly dependent, supportive or

related to the coast. Such uses provide services or goods for coastal industries or recreationists, and include boat sales, supplies, and service; diving, commercial fishing, and sportfishing establishments; restaurants, snack bars and convenience markets; gift, sundries, and novelty shops; transient accommodations such as hotels, motels, tourist cottages, campgrounds and recreational vehicle parks; and recreational equipment rentals (such as bicycles, roller skates, surfboards). Within the Coast Highway Incentive District (ID), properties bearing this land use designation may accommodate residential uses in conjunction with minimum commercial floor area as specified in the ID or as determined through the discretionary review process.

The majority of coastal dependent, recreation and visitor serving commercial areas in the Coastal Zone are in three locations: in the Harbor area, near the San Luis Rey River, and east of the municipal pier.

3. Harbor – The Harbor classification applies to all land and water areas governed by the Oceanside Small Craft Harbor District. This is a multiple use category, primarily for boating and Harbor-dependent uses, and secondarily for harbor-related and support services such as open space, recreation, public facilities, visitor-serving commercial, and residential/transient accommodations. The intent of this classification is to ensure that the limited land and water areas in the Harbor are assigned to highest priority uses. Implementation of this land use classification is achieved through development of a specific plan. The Harbor Precise Plan, which was approved by the Harbor District on October 25, 1979, was prepared to serve that function.

B. INDUSTRIAL LAND USES

1. Light Industrial – Industrial uses have generally been phased out of the Coastal Zone due to land use compatibility problems and a lack of sites large enough to successfully develop and buffer light industrial uses.

Only one light industrial site of 11 acres remains in the Coastal Zone. This site is bordered by the Loma Alta Creek Channel on the south, the AT&SF Railroad and La Salina Sewage Treatment Plant on the west, the Escondido railroad spur line on the north and the Hill Street commercial corridor on the east.

First priority for use of this area would be small Coastal-dependent or related industries such as boat building, sail making or a boat repair yard. If, because of the site's small size and isolated location, such coastal dependent uses are not possible, light industrial uses should be allowed.

Any development on this site should be designed to be visually unobtrusive and compatible with the surrounding area.

2. Transportation and Utility – This classification encompasses the two major public utilities in the Coastal Zone: the La Salina Sewage Treatment Plant and the Atchison, Topeka and Santa Fe Railroad.

The treatment plant site includes the plant itself (with area available for possible expansion) and the open space necessary to buffer the plant from surrounding land uses. In the future, land excess to those needs may be considered for conversion to public recreation use, such as beach parking.

The railroad corridor serves as a link in the major passenger and freight line between Los Angeles and San Diego. The corridor includes open space which buffers the railroad from surrounding noise-sensitive land uses and also serves as a reserve corridor for future transportation needs. Air rights or multiple-use of the rail corridor in the downtown area may be possible in the future, but only if future transportation options are not foreclosed and substantial public benefits can be gained.

The railroad corridor also includes a site designated for a possible multi-modal transportation facility. This facility would interface rail, local bus, intercity bus, and taxi service in a single facility and, as such, is strongly supported in the LCP policies.

C. RESIDENTIAL LAND USES

1. Low Density Residential – The low density residential classification of 0-7 dwelling units per acre has been applied to neighborhoods which are predominantly built-out with single-family residences. This designation is intended to preserve existing single-family residences in neighborhoods which have basically sound stock with a substantial remaining economic life. Three neighborhoods which possess special character are within this designation: the single-family neighborhood above Buena Vista Lagoon between Hill Street and I-5; the portion of the Eastside north of Laurel Street; and, the St. Malo area.
2. Medium Density Residential – The medium density classification allows up to 15 units per acre. It is proposed in areas which are generally inappropriate for traditional single-family construction, but lack the infrastructure, physical characteristics, or access necessary for high density development. Medium density offers a transition between single-family and high density development.

A medium density designation is proposed for the mobile home parks located near Loma Alta Creek, as a means to protect that existing development.

3. High Density Residential – The need for affordable housing, energy considerations and the goal of protecting agricultural and rural areas all dictate that the City reserve areas for high density residential development. High density uses have been proposed for flat, accessible sites where community facilities and public services are available to serve the higher numbers of people.

High density development should not be confused with overcrowding. The City should insist upon good design and site planning to ensure that new high density development does not detract from the attractiveness and “liveability” of the urban environment. In addition, the City should ensure that high density areas are served nearby by higher levels

of amenities and services --- such as public transportation, shopping areas, parks, churches, etc. --- than are normally provided for low and medium density areas.

The density range for this classification is 15 units per acre and up with the upper limit set by the Zoning Ordinance, ~~and~~ Redevelopment Design Guidelines, and the Coast Highway Incentive District (ID). The density for any given project in this category should be based upon site characteristics, compatibility with the surrounding neighborhood, project type, and service availability. For instance, a senior citizen project in the downtown area may be more appropriate for a higher density than a family-oriented project in South Oceanside.

The high density areas in the Coastal Zone have generally been proposed for the lands west of Hill Street. In addition, high density use is also proposed for the one large vacant “unconstrained” parcel left in the Coastal Zone, which is located above Lawrence Canyon.

High-density residential properties within the Coast Highway Incentive District (ID) can accommodate commercial land uses in accordance with the provisions of the ID, which include form-based development standards and additional building height and residential density in exchange for specified public benefits.

4. Mixed High Density/Transient Residential – This category is intended to allow both high density residential use and transient accommodations, such as hotels, motels, tourist cottages, and seasonal rentals. Also, limited office or commercial uses which are incidental or ancillary to transient residential uses ---- such as seasonal rental or property management offices -- may be allowed in this designation. Uses in this classification should be designed to be compatible with surrounding development and should not overcrowd public recreational amenities.

The mixed high density/transient residential classification is proposed for shorefront properties from Ninth Street south to Cassidy Street.

5. Cluster Overlay – The cluster overlay is intended to allow grouping of residential units on the least constrained portions of sites which have some environmental limitations. For example, on a one-acre site designated for low density which is about half flat and half hilly, the cluster overlay would allow up to seven units to be built on the flat portion of the site, with the remainder left in open space.

The cluster overlay is used to augment the underlying land use designation. It has been applied to two properties in the San Luis Rey River area which are affected by steep terrain.

D. OPEN SPACE

The open space classification encompasses several distinct types of land use. Significant habitat areas such as the San Luis Rey River and Buena Vista Lagoon are to be left in a natural state, with only limited passive recreation use allowed. Public parks, beaches, and coastal accessways are intended to support active public recreation uses. The only “institutional” open space use in the Coastal Zone is Laurel School, which is used primarily for education purposes

but does offer limited recreation benefits to the surrounding neighborhood. The final class of open space is constrained lands which are to remain undeveloped because of public health and safety concerns. These lands include the Loma Alta Creek Channel and steep slopes in the San Luis Rey River area.

II. LOCAL COASTAL PERMIT REGULATIONS

This Local Coastal Program Land Use Plan contains many policies which are to be implemented at the time of development permit issuance. For example, one of the access policies states that major new developments on Pacific Street, south of Cassidy Street, should be required to dedicate and build public accessways if adequate access is not available nearby. Other policies require that buffers be maintained adjacent to sensitive habitat areas, drainage improvements be designed to protect water quality, and projects in certain neighborhoods incorporate special design themes.

The overall goal of this section is that permitting procedures for coastal development applicants be consolidated and simplified. Rather than having to obtain several separate permits from the City, Redevelopment Agency, Harbor District and/or Coastal Commission, the LCP proposes that applicants would have to obtain only one permit at the local level, with only limited appeal authority retained by the State Coastal Commission.

In order to uniformly apply all LCP Land Use Plan policies, it is recommended that the City amend its Zoning Ordinance and other regulatory provisions to establish consolidated local discretionary review and approval procedures for the Coastal Zone.

1. Coastal Development Permits would be required of all projects which because size, intensity or location require special review and approval by one or more of the following decision-making bodies: the City Council, Redevelopment Agency, Harbor District Board, or Planning Commission. Where other discretionary permit requirements already exist (such as major subdivisions, development plans, conditional use permits, Redevelopment permits, or Harbor District permits), the Local Coastal Program requirements should be incorporated into those existing procedures.
2. Design Review would be required for all projects where aesthetic issues (such as compatibility with the neighborhood, landscaping adequacy and conformance to design "themes") are a concern. In instances where other discretionary permits (described above) are required, design review should be consolidated with those permit processes. In instances where no other permits are required, design review should be accomplished by either an administrator (such as the Planning Director) or a professional review board, with possibility for appeals to the Planning Commission and City Council.
3. Discretionary Demolition Permits would be required for any demolitions of sound buildings in the permit appeals area and for demolition of sound residences only in the remainder of the Coastal Zone. Under State law, the Building Director has sole authority for determining whether a building should be demolished for health and safety reasons. Therefore, the demolition of condemned buildings, as defined in the State Building Code,

would not require any discretionary permits. Regulation of the demolition of sound structures will enable the City to enforce policies concerning Coastal-dependent, recreation, and visitor serving uses, as well as low and moderate cost housing.

Table 2 provides a matrix delineating the type of discretionary approvals which are required to implement the Local Coastal Program. Most of these discretionary approvals are already required by the City. Additional costs encumbered in administering these procedures should be recovered through permit application fees or State reimbursement. The level of review proposed for different types of projects varies according to the size, location or intensity of the project. For example, single-family residences outside the Permit Appeals Area would require no discretionary permits (unless subdivision of land or a zoning variance is involved). Small multi-family or commercial projects outside the Appeal Area would require only administrative design review. Large projects or projects in sensitive areas will require full local hearings for approval. Conditions of approval may be applied to such projects to fully meet the requirements of the LCP.

Table 2 (continued)

LOCAL DISCRETIONARY REVIEW REQUIREMENTS

TYPE OF PROJECT	COASTAL DEVELOPMENT PERMIT	DESIGN REVIEW	DEMOLITION PERMIT (where applicable)
All developments within the San Luis Rey River Specific Plan Area	X	X	* Residential Demolitions Only
Any other uses requiring Conditional Use Permits pursuant to the City's Zoning Ordinance	X	X	"
Sign Permits			
Public Projects such as parks, community buildings, and recreational facilities	X	X	
Shoreline structure projects, such as seawalls, revetments, jetties, groins, etc.			
Mining or extraction of materials	X		

X Indicates projects for which local permits are already required.

* Indicates projects which are currently regulated by the Coastal Commission, but not by the City. The City would assume permit authority for these types of projects upon certification of the LCP.

Tab. 2

LOCAL DISCRETIONARY REVIEW REQUIREMENTS

TYPE OF PROJECT	COASTAL DEVELOPMENT PERMIT	DESIGN REVIEW	DEMOLITION PERMIT (where applicable)
Projects within Coastal Appeal Area:			
All developments	X	X	*
Projects Outside the Coastal Appeal Area:			Residential Demolitions Only
Single Family Residences			*
Multi Family Projects of 2 to 20 units			"
Multi Family Projects of 20 or more units	X		"
Commercial Uses on sites of less than 2½ acres		X	"
Commercial Uses on sites greater than 2½ acres	X	X	"
Industrial Uses	X	X	"
Subdivisions pursuant to the State Subdivision Map Act	X		
All developments within the Redevelopment Area pursuant to the Redevelopment Design Guidelines	X	X	"
All developments in the Harbor Precise Plan Area pursuant to the Harbor Design Guidelines and Application Procedures	X	X	X

III. COASTAL PERMIT APPEALS

Once Oceanside's LCP is certified, the City will have sole discretion over the issuance of Coastal Development Permits, except for limited permit and appeal authority to be retained by the State Coastal Commission. Appeals will be allowed only in the following instances:

- For any projects within the permit appeal area, shown on Exhibit B;
- For any major public works projects, costing in excess of \$25,000*.

As an LCP Implementation Measure, the City will adopt permit notification, hearing and appeals procedures pursuant to the Local Coastal Program Implementation Regulations recently adopted by the Coastal Commission.

*The Coastal Commission is contemplating a change to their regulations which would raise this amount to \$50,000.

Article 9 **Coast Highway Incentive District (Incentive District).**

Sections:

- 901 Purpose and Intent
- 902 Applicability
- 903 Administration
- 904 Regulating Plan
- 905 Mixed-Use Standards
- 906 Residential Incentive Program
- 907 Land Use Standards
- 908 Urban Standards
- 909 Architectural Standards
- 910 Large Lot Standards
- 911 Parking Standards
- 912 Definitions

901 Purpose and Intent

The specific purposes of the Coast Highway Incentive District (Incentive District) are to:

- A. Incent redevelopment and revitalization of the Incentive District by streamlining the development review process and providing development incentives.
- B. Encourage sustainable, high-quality development consistent with the intent and objectives articulated in the Coast Highway Vision and Strategic Plan.
- C. Create distinct pedestrian-oriented subareas, including:
 - 1. Urbane mixed-use nodal areas featuring relatively intense commercial land use and residential density; development in these nodal areas will generally be taller and more street-adjacent than development in other subareas; commercial uses, including visitor-serving businesses, will provide a wide range of employment opportunities.
 - 2. Commercial Villages featuring neighborhood-serving commercial uses in a suburban main street setting; these villages also allow for mixed-use development, consistent with underlying zoning standards.
 - 3. Transitional Avenue segments featuring a combination of mixed-use, standalone commercial, and standalone residential development with generally less land use intensity and residential density relative to nodal areas; these segments are characterized by more expansive setbacks and landscaping.
- D. Promote high-quality urban and architectural design and variability of massing and height, emphasizing the design of the interface between the private and public realms.
- E. Facilitate the creation of vibrant community places and tourist destinations.

- F. Treat Coast Highway as a complete, multi-modal street that is safe, pedestrian and bicycle friendly, accessible, attractive, visually and functionally engaging for users of all ages and abilities, and well integrated with adjoining neighborhoods along the corridor.

902 Applicability

A. Applicability and Zoning Map Designator

The Incentive District is an *optional* development tool to meet the purpose and intent as described in Section 901, and may be used to develop any parcel within the Incentive District (Map 1). When an applicant voluntarily decides to develop consistent with the Incentive District standards, these standards shall supersede underlying zoning standards.

B. Rules for Interpretation

Section 240 Rules for Interpretation of Article 2 Organization, Applicability, and Interpretation of the 1992 Ordinance shall apply.

C. Severability

Section 220 (L) Severability of Article 2 Organization, Applicability, and Interpretation of the 1992 Ordinance shall apply.

903 Administration

A. Administrative Development Plan review is required for the following:

- 1. New or redevelopment proposals with 43 dwelling units per acre or less, or developments with no residential component.
- 2. Subdivisions and land assemblages.

B. Development Plan review is required for the following:

- 1. Development proposals greater than 43 dwelling units per acre.
- 2. Development proposals greater than 45 feet in height.
- 3. Development proposals subject to Large Lot Standards stipulated in Section 910.

C. Conditional Use Permits are required for the following:

Land uses subject to a Conditional Use Permit per Table 2, Section 907.

D. Overview of Permits Required

Table 1 provides an overview of the permits required, the review authority, and the appeal authority within the Incentive District.

Table 1. Applications and Review Authority

Permit Type	Reviewing Authority	Appealable/Appeal Authority
Administrative Development Plan Permit	City Planner Administrative Decision	Yes/Planning Commission
Development Plan Permit	Planning Commission	Yes/City Council
Conditional Use Permit	Planning Commission	Yes/City Council

E. Administrative Development Plan Review

1. Review Authority

The City Planner shall approve, conditionally approve, or deny applications for Administrative Development Plan Permits based on considerations of the standards of this article.

2. Review and Decision

- a. The City Planner shall approve an Administrative Development Plan Permit if the proposed development complies with applicable development standards, and meets the required findings in Section 903 E (3), below.
- b. The City Planner may require review by any other City department or division or governmental agency deemed necessary.
- c. The City Planner shall prepare a written decision which shall contain the findings of fact upon which such decision is based. A copy of the decision shall be mailed to the applicant at the address shown on the application within 14 calendar days after the decision is made.

3. Required Finding

An Administrative Development Plan Permit may be granted if the City Planner finds that the project, as submitted or modified, is consistent with the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, and the Local Coastal Program and complies with the applicable standards of this article.

4. Conditions

In granting an Administrative Development Plan Permit, the City Planner or the Review Authority on appeal may impose reasonable conditions to protect the public health, safety, and general welfare and secure the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, the Local Coastal Program, and this article, and to support the required findings of approval.

5. Other Standards

Administrative Development Plan Permits shall comply with the standards of Article 43 Development Plan Review of the 1992 Ordinance, specifically: Sections 4303 Initiation of Development Plan Review; 4304 Application for Development Plan; 4305 Notice Administrative Hearing and Public Hearing; and 4308 Effective date, Lapse of Approvals, Time Extension, and Changed Plans.

F. Development Plan Review

1. Review Authority

The Planning Commission shall approve, conditionally approve, or deny applications for Development Plan Permits based on consideration of the standards of this article.

2. Required Findings

A Development Plan Permit shall be granted only if the Planning Commission determines that the project as submitted or modified, complies with the following criteria:

- a. That the proposed project is consistent with the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, and the Local Coastal Program.
- b. That the physical location, size, massing, setbacks, pedestrian orientation, and placement of proposed structures on the site and the location of proposed uses within the project comply with applicable standards and are consistent with the purpose and intent of the Incentive District.
- c. That any proposed extension of rights-of-way and/or onsite circulation can accommodate autos, bicycles, pedestrians, and multi-modal transportation methods, including adequate vehicle and bicycle parking and pedestrian access.
- d. That the area covered by the application can be adequately, reasonably, and conveniently served by existing and/or planned public services, utilities, and public facilities.
- e. That any community benefits have been provided in accordance with Section 904.

3. Conditions

In granting a Development Review Permit, the Planning Commission or the Review Authority on appeal may impose reasonable conditions to protect the public health, safety, and general welfare and secure the intent and objectives of the Coast Highway Vision and Strategic Plan, the General Plan, the Local Coastal Program, and this article, and to support the required findings of approval.

4. Other Standards

Development Plan Permits shall comply with the standards of Article 43 Development Plan Review of the 1992 Ordinance, specifically: Sections 4303 Initiation of Development Plan Review; 4304 Application for Development Plan; 4305 Notice Administrative Hearing and Public Hearing; and 4308 Effective date, Lapse of Approvals, Time Extension, and Changed Plans.

G. Conditional Use Permit

Conditional Use Permits shall be processed in accordance with Article 41 of the 1992 Ordinance.

H. Appeals

1. Rights of Appeal and Review

Rights of appeal and review procedures shall be as prescribed by Article 43 Development Plan Review of the 1992 Ordinance, specifically Section 4309 (A) and (B), except that any filing for an appeal shall include the grounds for appeal and supporting documentation in compliance with Section 903 (H), 2 and 3, of this article.

2. Grounds for Appeal

All appeals shall be filed with the following supporting documentation.

- a. Factual Error. The statements or evidence relied upon by the decision maker when approving, conditionally approving, or denying a permit, map, or other matter were inaccurate.
- b. New Information. New information is available to the applicant or the interested person that was not available through reasonable efforts or due diligence at the time of the decision.
- c. Findings not supported. The decision maker's stated findings to approve, conditionally approve, or deny the permit, map, or other matter are not supported by the information provided to the decision maker.
- d. Conflicts. The decision to approve, conditionally approve, or deny the permit is in conflict with the intent and objectives of the Coast Highway Vision and Strategic Plan, General Plan, Local Coastal Program, and applicable development standards.

3. Insufficient Appeal

An insufficient appeal shall be returned to the appellant.

I. Incentive District Boundary Adjustments

1. Minor adjustments to the Incentive District boundary and identified subareas may be approved by the City Planner subject to the following findings:

- a. Parcels must be included within the Coastal Zone boundary.
- b. Parcels are contiguous to the Incentive District or identified subarea and the addition of the parcel(s) would result in a project that equally or better meets the purpose and intent of the Incentive District.
- c. All other adjustments will require an amendment to the Incentive District.

J. Periodic Review and Maintenance

To ensure progress toward achieving the intent and objectives articulated in the Coast Highway Vision and Strategic Plan, General Plan, and Local Coastal Program, the Planning Commission will periodically review the Incentive District boundaries and this Article and recommend changes.

904 Regulating Plan

A. Applicability

The Regulating Plans for this article are depicted in the following maps:

- Map 2 Subarea Plan
- Map 3 Setback Regulating Plan

B. Purpose of the Subarea Plan

The Subarea Plan (Map 2) divides the Incentive District into three subareas, with each subarea assigned place-specific development standards. The purpose of each subarea is described in Section 901(c).

C. Purpose of Setback Plan

The Setback Plan (Map 3) establishes minimum and maximum setbacks along the primary frontage of all parcels within the Nodes and Commercial Village subareas and minimum setbacks along the primary frontage of all parcels within the Avenue subareas.

D. Setback Standards

The primary frontage is provided on Map 3 for guidance purposes only. The City Planner shall make the final determination as to which parcel line(s) serves as the primary frontage of each project site. The City Planner shall use the following criteria in making this determination:

1. Primary frontages shall be applied to properties abutting Coast Highway.
2. For parcels not abutting Coast Highway, primary frontages shall be those abutting streets that parallel Coast Highway.
3. For parcels not abutting Coast Highway with frontages on more than one street running parallel to Coast Highway, the orientation of existing development on the same city block shall determine the primary frontage.

4. For properties on Wisconsin Avenue and south of Coast Highway, primary frontages shall be those abutting Wisconsin Avenue.
5. Front yard setbacks along the primary frontage as shown on Map 3:
 - a. Avenues. Minimum 5 feet for mixed-use/ground-floor commercial and 10-foot minimum for standalone residential; 10-foot maximum for mixed-use/ground-floor commercial and 20-foot maximum for standalone residential.
 - a. Commercial Villages. Minimum 3 feet and 8-foot maximum.
 - b. Nodes. No minimum and 10-foot maximum.
6. Side yard setbacks:
 - a. Corner lots in all sub areas: Consistent with applicable minimum and maximum front yard setbacks.
 - b. Interior lots in all sub areas with at least one frontage along a primary frontage identified on Map 3: No required minimum or maximum.
 - c. For all other interior lots in all sub areas: Minimum 10 percent of lot width, not less than 3 feet and not more than 5 feet.
7. Rear yard setbacks:
 - a. Properties in all sub areas not abutting a residential zoning district: No required minimum or maximum.
 - b. For properties abutting residential zoning districts: Minimum 15 feet, unless abutting an alley, then minimum 5 feet.

905 Mixed-Use Standards

A. Purpose

This section establishes the minimum standards for commercial space within mixed-use developments.

B. Mixed-Use Commercial Floor Area

1. For mixed-use projects on lots 60,000 sf and smaller, the Floor Area Ratio devoted to commercial space shall be a minimum of 0.20.
2. For mixed-use projects on lots greater than 60,000 sf, the Floor Area Ratio devoted to commercial space shall be a minimum of 0.25.

C. Commercial Uses

1. Required commercial spaces shall contain commercial land uses that serve clients and patrons that will visit the site. These spaces may not be used for storage or in the same manner as home occupancy businesses.

2. Commercial land uses in mixed-use projects include the following major land use categories; artisan manufacturing, bars and cocktail lounges, craft breweries and wineries, child care, commercial recreation and entertainment-indoor, cultural institutions, financial services, food and beverage sales, offices business and professional, park and recreation facilities, personal improvement services, personal services, restaurants, retail, studios, and visitor accommodations.

906 Residential Incentive Program

A. Purpose

1. The purpose of this section is to establish regulations that will allow new development to exceed the maximum residential densities of the underlying zoning district in exchange for community benefits, or to implement stand-alone residential use in the Avenue subareas.
2. These regulations require that projects approved above the maximum residential density must implement community benefits within one or more of the categories in Section 906 (C), below.

B. Applicability

1. The provisions of this Chapter shall apply to the nodal subareas as shown in Map 4, Residential Incentive Program Area.
2. Nodes. Residential use in nodal areas is permitted as a component of mixed-use development that meets minimum commercial standards as specified in Section 905. Development in nodal areas may exceed the maximum residential density up to a maximum of 63 dwelling units per acre through the provision of certain community benefits as further described in Section 906 (C), below.
3. Avenue Segments. Development in Avenue segments may not exceed the residential density allowance of the underlying zoning district.

C. Residential Density Incentive for Nodal Development

1. In nodal areas, projects can earn additional density above the underlying residential density allowance in exchange for one or more of the following community benefits provided per unit above 43 dwelling units per acre.
 - a. Public Improvement Fee. Projects can gain additional density up to a maximum of 63 dwelling units per acre through payment of a per-unit public improvement fee. Said fee shall be set and adjusted by the City Council and shall provide for public improvements within the Incentive District.
 - b. Public Open Space. Projects can gain additional density up to a maximum of 63 dwelling units per acre through the provision of public open space at a minimum ratio of 200 square feet (sf) per unit. Public open space provided in exchange for residential density must comply with the following standards:

- i. Projects shall provide a minimum of 1,000 sf of public open space.
 - ii. The minimum dimensions of public open space shall comply with the standards applicable to the building type as provided in Section 908 (D).
 - iii. Open space shall be owned, operated, and maintained by the developer or property manager in accordance with an approved maintenance plan.
 - iv. Open space shall be directly accessible from the public right-of-way.
 - v. Open space shall be accessible to persons with disabilities.
 - vi. Open space shall be on the ground level.
 - vii. No more than 20 percent of open space shall be occupied by above-grade structures.
 - viii. Open space shall be open to the public, without charge, each day of the year from 6:00 a.m. to 10:00 p.m., except for temporary closures for necessary maintenance or compelling public safety concerns identified in coordination with the Oceanside Police Department.
 - ix. At a minimum, the following elements shall be included within the open space:
 - Trees and landscaping.
 - Seating.
 - Refuse and recycling receptacles.
 - Signage identifying the open space as open to the public and specifying hours of operation.
 - x. Open space may be provided off-site, subject to approval by the City Planner.
 - xi. Off-site open space shall comply with the following standards:
 - The open space shall be within 1,320 feet of the project site.
 - The open space shall be dedicated and improved concurrent with the project benefiting from bonus residential density.
 - The open space may be either publicly or privately maintained subject to agreement with the City.
- c. Public Parking. Projects can gain additional density up to a maximum of 63 dwelling units per acre through the provision of a minimum number of public parking spaces per unit in compliance with the following standards:
- i. One parking space per unit.

- ii. The total number of additional parking spaces must equal at least 10 percent of the total minimum parking spaces required under Section 911.
 - iii. A public parking easement shall be executed for these facilities to the satisfaction of the City Planner and City Attorney.
 - d. Increased Commercial Floor Area. Projects can gain additional density up to a maximum of 63 dwelling units per acre through the provision of additional ground floor commercial floor area above the minimum commercial floor area standard established in Section 905 (B). A total of 250 sf of additional ground floor commercial space shall be provided per unit.
2. The Residential Incentive Program shall be periodically reviewed by the Planning Commission as described in Section 903 (J) to ensure the public benefits provided in exchange for residential density continue to be equitable and desirable by the community.

D. Standalone Residential in Avenues

Standalone residential development shall be permitted in Avenues in compliance with the maximum density of the underlying zoning district. No community benefit per Section 906(c) above, is required.

907 Land Use Standards

A. Purpose

Allowed land uses are provided in Table 2, Land Use and Permit Standards. The land use regulations shall be reviewed as part of periodic reviews described in Section 903 (J) to ensure a balanced mix.

B. Applicability

Land uses shown in the table are allowed in the subarea specified. Uses not included in the table are considered prohibited unless determined by the City Planner to be substantially similar to another permitted or conditionally permitted use. Each land use in the table corresponds to a definition listed in Section 912.

C. Land Use and Permit Standards

Table 2. Land Use and Permit Standards

Use ⁽¹⁾	Avenue	Commercial Village	Node
Animal Sales and Services	P	P	P
Artisan Manufacturing	P	P	P
Assembly/Meeting Facility	C	C	P
Bars and Cocktail Lounges	C(4)	C(4)	C(4)

Use ⁽¹⁾	Avenue	Commercial Village	Node
Breweries, Craft	Variable (10)	Variable (10)	Variable (10)
Child Care	P	P	P
Commercial Recreation and Entertainment – Indoor	P(4)	P(4)	P(4)
Cultural Institutions	P	P	P
Financial Services	P(2)	P(2)	P(3)
Food and Beverage Sales	P(4)	P(4)	P(4)
Hospitals	C	C	P
Offices, Business and Professional	P(2)	P(2)	P(3)
Parking as a primary use	P(5)	P(5)	P(5)
Park and Recreation Facilities	P	P	P
Personal Improvement Services	P(4)	P(4)	P(4)
Personal Services	P(4)	P(4)	P(4)
Residential	P(6)	P(6)(7)	P(7)(8)
Restaurants, Fast Food	P(2)	P(2)	P(3)
Restaurants, Full Service	P(4)	P(4)	P(4)
Retail	P(2)	P(2)	P(2)(4)
Schools	C	C	C
Studios	P	P	P
Theater	C	C	P
Vehicle/Equipment Sales and Services	-	C	-
Visitor Accommodations	P	P	P
Wineries, Craft	Variable (10)	Variable (10)	Variable (10)
Other ⁽⁹⁾	C	C	C
<p>Notes: “P” denotes that the use is permitted. “C” denotes that the use is permitted with the approval of a Conditional Use Permit pursuant to Article 41 of the 1992 Ordinance. “-” denotes that the use is not permitted. “(numbers, such as ‘1’)” denote that the land use is subject to additional use regulations which are provided after the table.</p>			

D. Additional Land Use Regulations

1. All uses must meet City noise and emissions standards per Oceanside Municipal Code Chapter 38. The City Planner may establish project-specific conditions of approval to minimize noise, including conditions to soundproof facilities, limited operating hours, and/or limited facility size.

2. Any drive-through facilities require approval of a Conditional Use Permit in accordance with Article 41 of the 1992 Ordinance.
3. Drive-through facilities are prohibited in Nodes.
4. Certain uses within this land use category are subject to standards in Article 36 Separation of Regulated Uses of the 1992 Ordinance. See Section 912 Definitions of this article for identification of these uses.
5. Parking as a primary use must be made publically available and shall comply with appropriate provisions of Section 911 and applicable provisions of Article 31 of the 1992 Zoning Ordinance, including Section 3120 additional design standards for parking lots and structures.
6. Residential density may not exceed 43 units per acre.
7. Residential shall only be permitted in conjunction with another permitted use.
8. Residential density may not exceed 43 units per acre unless the project complies with the Residential Incentive Program per Section 904 and is subject to approval of a Development Plan Permit per Section 903.
9. Any use that is not listed in Table 2 and defined in Section 912 is considered “Other” and requires a Conditional Use Permit in accordance with Article 41 of the 1992 Ordinance.
10. Definitions, performance standards, and review processes for craft breweries and wineries are provided in Section 414 of Article 4 of the 1992 Ordinance.

908 Urban Standards

A. Purpose

The Urban Standards define the design of the built environment in the Incentive District, placing emphasis on the design of the street frontage where private development meets the public street. The Urban Standards vary by subarea and are further described in Table 3 below.

B. Applicability

This section applies to all areas within the Incentive District.

C. Urban Standards by Subarea

Table 3. Urban Standards by Subarea

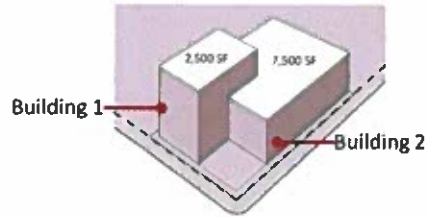
Location	Allowable Building Types	Allowable Frontage Types	Maximum Height	Minimum Frontage Occupancy ²
Node	<ul style="list-style-type: none"> • Live/Work • Mixed Use • Courtyard Complex • Podium • Wrap Building • Special Type Buildings 	<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront • Dooryard • Live/Work • Stoop • Forecourt 	45 feet or four stories May exceed height limit subject to additional provisions ¹	90%
Commercial Village	<ul style="list-style-type: none"> • Main St. Retail/Office • Live/Work • Mixed Use • Courtyard Complex • Podium • Wrap Building • Special Type Buildings 	<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront • Live/Work • Dooryard • Stoop • Forecourt 	45 feet or four stories	70%
Avenue	<ul style="list-style-type: none"> • Main St. Retail/Office • Live/Work • Courtyard Apartment • Condo Complex • Podium • Wrap Building • Townhouse • Stacked dwelling • Special Type Buildings 	<ul style="list-style-type: none"> • Arcade/Gallery • Live/Work • Shopfront • Avenue-Commercial • Avenue-Residential • Dooryard • Stoop • Forecourt 	35 feet or three stories May exceed height limit subject to additional provisions ³	60%

Notes:

- 1) Projects may exceed the height limit up to a maximum of 65 feet, with a maximum average height of 55 feet, subject to approval of a Development Plan Permit pursuant to Section 903 (F).
- 2) An exception to the minimum frontage occupancy standard may be granted by the City Planner where significant urban or architectural features or contributions to the public realm are offered. Such public realm features may include, but are not limited to publicly accessible open space, such as a courtyard or plaza or residential entry alcoves or stoops that contribute to the public realm by adding detail and enhancing the relationship to the street. The following additional standards apply:
 - a. Any open space provided must comply with the open space standards in Section 908.
 - b. The open space may include outdoor dining areas, but the overall space must be accessible to the public and enhance the public realm.
 - c. The urban, architectural, or public realm features must be of exceptional, high-quality design.
 - d. The urban, architectural, or public realm features will not compete with other similar features or spaces on that block so as to detract from an inviting, pedestrian-friendly environment.
- 3) Projects may exceed the height limit up to a maximum of 45 feet, with a maximum average height of 35 feet.

Figure 1. Calculating Maximum Average Height for Nodes

Step One



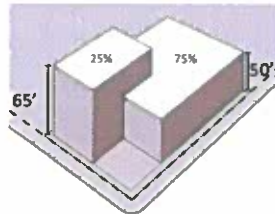
$$\frac{\text{Building 1 Roof Area (SF)}}{\text{Building 1 Roof Area (SF)} + \text{Building 2 Roof Area (SF)}} = \text{Building 1 Percentage (B1\%)}$$

Ex: $\frac{2,500}{2,500 + 7,500} = 25\%$

$$\frac{\text{Building 2 Roof Area (SF)}}{\text{Building 1 Roof Area (SF)} + \text{Building 2 Roof Area (SF)}} = \text{Building 2 Percentage (B2\%)}$$

Ex: $\frac{7,500}{2,500 + 7,500} = 75\%$

Step Two

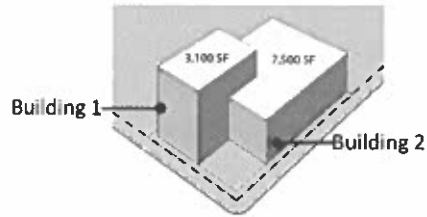


$$(\text{Building 1 Height (FT)} \times \text{B1\%}) + (\text{Building 2 Height (FT)} \times \text{B2\%}) = \text{Average Height (FT)}$$

Ex: $(65' \times .25) + (50' \times .75) = 55'$

Figure 2. Calculating Maximum Average Height for Avenues

Step One



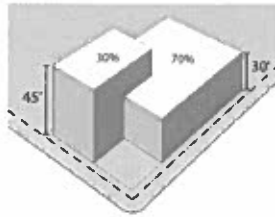
$$\frac{\text{Building 1 Roof Area (SF)}}{\text{Building 1 Roof Area (SF)} + \text{Building 2 Roof Area (SF)}} = \text{Building 1 Percentage (B1\%)}$$

$$\text{Ex: } \frac{3,100}{3,100 + 7,500} = 30\%$$

$$\frac{\text{Building 2 Roof Area (SF)}}{\text{Building 1 Roof Area (SF)} + \text{Building 2 Roof Area (SF)}} = \text{Building 2 Percentage (B2\%)}$$

$$\text{Ex: } \frac{7,500}{3,100 + 7,500} = 70\%$$

Step Two



$$(\text{Building 1 Height (FT)} \times \text{B1\%}) + (\text{Building 2 Height (FT)} \times \text{B2\%}) = \text{Average Height (FT)}$$

$$\text{Ex. } (45' \times .30) + (30' \times .70) = 35'$$

D. Building Types

Building type standards address key building elements as illustrated in Figure 3. Calculating the maximum allowed footprint per story is illustrated in Figure 4.

Figure 3. Key Building Elements

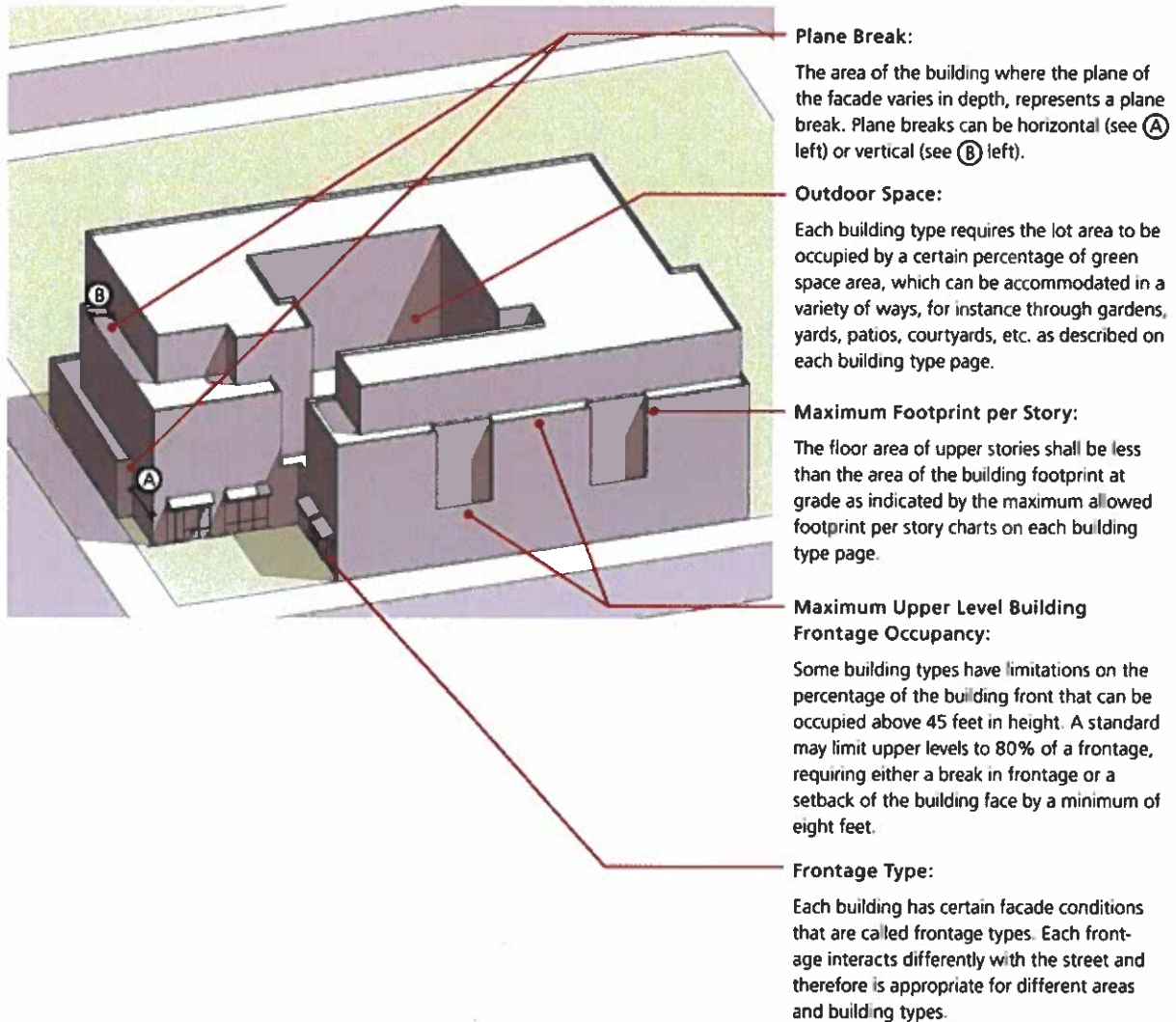
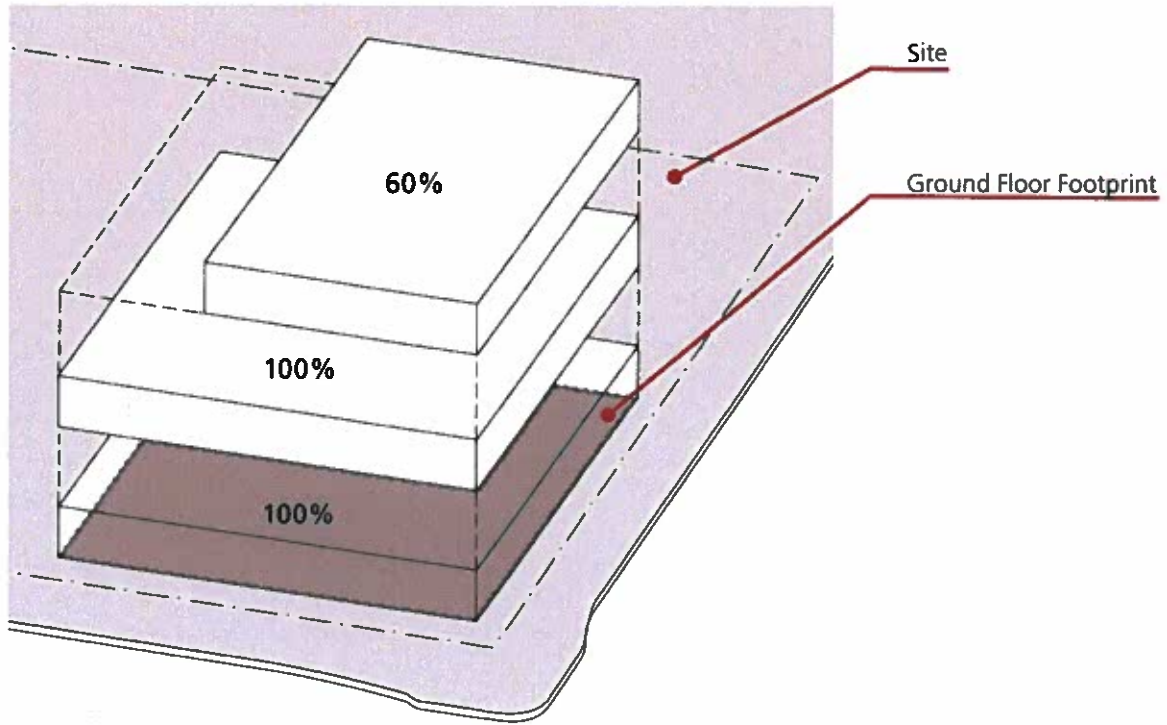
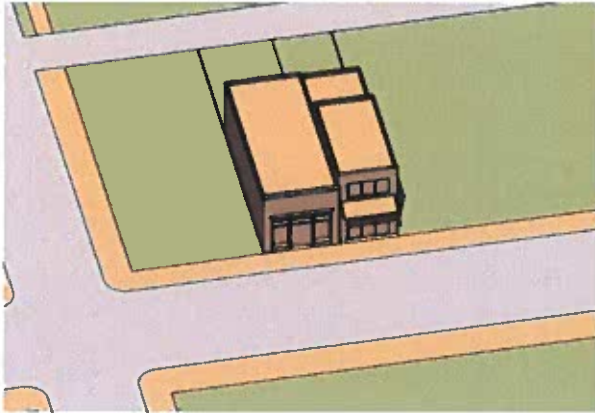


Figure 4. Maximum Allowed Footprint per Story



The maximum footprint per story is computed based on the building's ground floor footprint, not the overall site area.

1. Main Street Retail/Office



Three-dimensional diagram

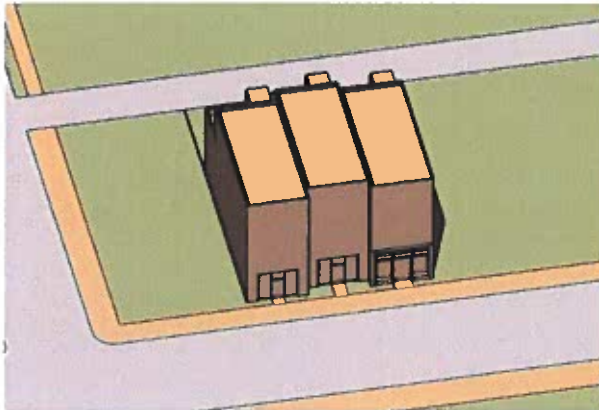


Example of Main Street Retail/Office

Intent Statement
A building designed for occupancy by retail, service, and/or office.
Location
<ul style="list-style-type: none"> Commercial Village Avenue
Building Height
25 feet min. or 1 story with mezzanine
Maximum Facade Width
50 feet
Maximum Upper-Level Frontage Occupancy
Not Applicable
Maximum Allowed Footprint per Story
Not Applicable
Primary Facades
<ul style="list-style-type: none"> Street-facing
Landscaping
<ul style="list-style-type: none"> Hardscape and potted plants, only, for front yards. Landscape materials to be approved by the City.

Open Space		
<ul style="list-style-type: none"> When provided, open space shall be at least 10 feet in length. 		
Primary Frontage Types		
<ul style="list-style-type: none"> Arcade/Gallery Stoops Shopfront 		
Primary Entry Location		
<ul style="list-style-type: none"> Street 		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Concealed from street view behind bldg.
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

2. Live/Work



Three-dimensional diagram

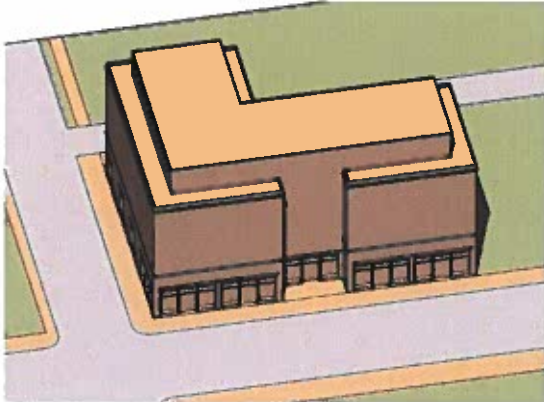


Example of Live/Work buildings

Intent Statement				
<p>An integrated residence and work space, occupied by a single unit. Often two or more such units shall be arranged side by side along the principal frontage that has been designed or structurally modified to accommodate joint residential and work occupancy. Live/work buildings may also wrap the podium of a high-rise building type. To ensure an appropriate ratio of residence to work space, the following standards must be met:</p> <ul style="list-style-type: none"> • The minimum size of a single unit is 470 sf. The maximum size of a single unit is 5,000 sf. • The residential area of a unit may not exceed the lesser of 50% of the total unit size. 				
Location				
<ul style="list-style-type: none"> • Node • Avenue • Commercial Village 				
Building Height				
Min.	25 feet or 2 stories			
Max.	50 feet or 4 stories in Node			
Maximum Facade Width				
Max.	30 feet per unit; 10 units per facade string			
Maximum Upper-Level Frontage Occupancy				
Not Applicable				
Maximum Allowed Footprint per Story				
Stories	1	2	3	4
2	100%	100%	-	-
3	100%	100%	80%	-
4	100%	100%	80%	80%

Primary Facades		
<ul style="list-style-type: none"> • Street-facing 		
Landscaping		
<ul style="list-style-type: none"> • Landscaping may not visually obstruct Shopfront or work space. • Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> • When provided open space shall be at least 10 feet in dimension. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Arcade/Gallery • Live/Work • Shopfront • Stoops 		
Primary Entry Location		
Street; except residential entries may be accessed through work space, through a paseo between units, or from the rear		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Concealed from street view behind bldg.
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

3. Mixed Use



Three-dimensional diagram

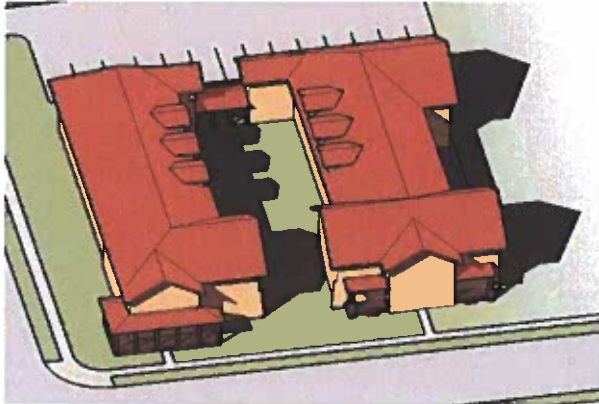


Example of Mixed-Use Building

Intent Statement			
A building designed for occupancy by retail, service, office, and/or residential uses on the ground floor, with upper floors also configured for office and/or residential uses; however two-story retail is permitted as long as the development complies with applicable mixed-use standards in Section 906.			
Location			
• Node		• Commercial Village	
Building Height			
Min.	2 stories		
Façade Width			
Max.	225 feet; >175 feet must have at least 1 facade break of at least 20 feet in length and 10 feet in depth		
Maximum Upper-Level Frontage Occupancy			
Portions of facades above 45 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).			
Maximum Allowed Footprint per Story			
Stories	1-3	4	5
2-3	100%	-	-
4-5	100%	85%	55%
Primary Facades			
Street-facing			

Landscaping		
<ul style="list-style-type: none"> Landscaping may not visually obstruct Shopfront or office or retail space. Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> When provided, open space shall be at least 10 feet in dimension. 		
Primary Frontage Types		
• Arcade/Gallery		• Shopfront
Primary Entry Location		
Street; Primary entrances to upper floors shall be accessed through an interior courtyard or a lobby which is accessed directly from the street		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Concealed from street view behind bldg.
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

4. Courtyard Complex



Three-dimensional diagram

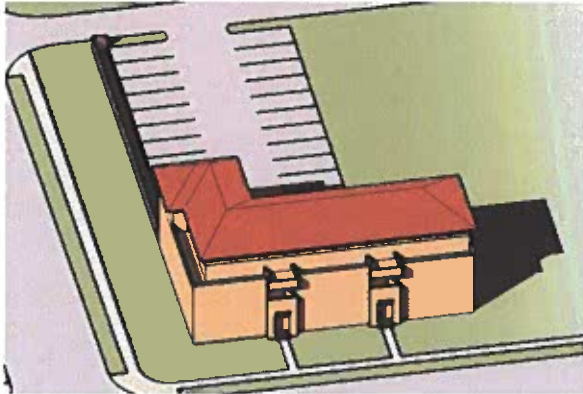


Example of Courtyard Complex

Intent Statement	
A grouping of townhouses or individual units arranged around a central courtyard or series of courtyards at grade. The uses may include residential, retail, office, or lodging.	
Location	
<ul style="list-style-type: none"> • Node • Avenue • Commercial Village 	
Building Height	
Min.	21 feet or 2 stories
Max.	40 feet or 3 stories
Façade Width	
Max.	Not Applicable
Maximum Upper-Level Frontage Occupancy	
Not Applicable	
Maximum Allowed Footprint per Story	
Not Applicable	
Primary Facades	
<ul style="list-style-type: none"> • Street-facing • Inner-Courtyard 	
Landscaping	
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 	

Open Space		
<ul style="list-style-type: none"> • Central courtyard shall average at least 20 feet in width, and at no time may be less than 10 feet in width. • Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 20-foot average width or 10-foot minimum width. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Shopfront • Dooryard 		
Primary Entry Location		
<ul style="list-style-type: none"> • Inner-Courtyard 		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Not permitted
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

5. Multi-Family Complex



Three-dimensional diagram

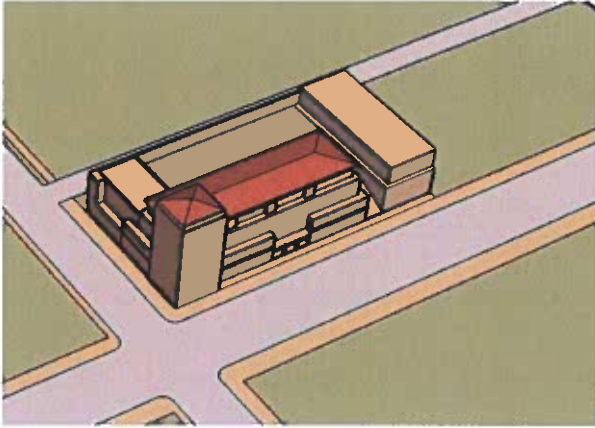


Example of Multi-Family Complex

Intent Statement		
Six units or more configured as one building, one over another, and exclusively residential.		
Location		
<ul style="list-style-type: none"> • Avenue 		
Building Height		
Min.	2 stories	
Maximum Facade Width		
Max.	Not Applicable	
Maximum Upper-Level Frontage Occupancy		
Portions of facades above 40 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).		
Maximum Allowed Footprint per Story		
Stories	1-3	4
2-3	100%	-
4	100%	85%
Primary Facades		
<ul style="list-style-type: none"> • Street-facing 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 		

Open Space		
<ul style="list-style-type: none"> • Where provided, courtyards shall average at least 25 feet in width, and at no time may be less than 10 feet in width. • Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 20-foot average width or the 10-foot minimum width. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Raised terrace • Stoop 		
Primary Entry Location		
<ul style="list-style-type: none"> • Street 		
Parking		
Type	Surface	At rear of lot
	Above-Grade	Integrated or detached
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

6. Podium



Three-dimensional diagram

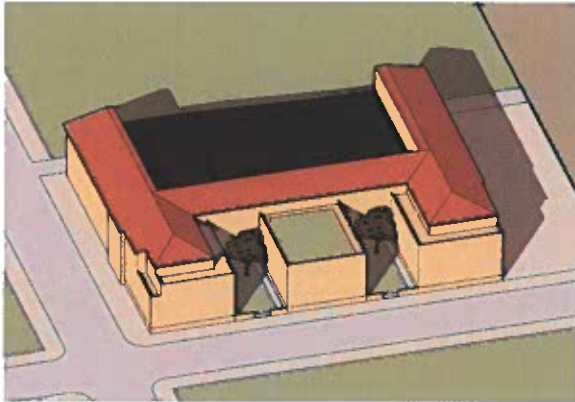


Example of Podium Building

Intent Statement			
Podium parking is typically two levels, one underground and one at grade. At-grade-level parking is wrapped by single-loaded flats or two-story units. Roof of garage is landscaped, providing usable courtyard for residents.			
Location			
• Node		• Avenue	
• Commercial Village			
Building Height			
Min.	3 stories		
Maximum Facade Width			
Max.	300 feet		
>175 feet must have at least one facade break of at least 20 feet in length and 10 feet in depth			
Maximum Upper-Level Frontage Occupancy			
Portions of facades above 45 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).			
Maximum Allowed Footprint per Story			
Stories	1-3	4	5
3	100%	-	-
4-5	100%	85%	50%

Primary Facades		
<ul style="list-style-type: none"> • Street-facing • Parking podiums at ground level and above shall be lined with habitable space along all street facing facades (alleys are excluded from this standard) 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> • Where provided, courtyards must average at least 25 feet in any dimension, and at no time may be less than 10 feet in any dimension • Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 25-foot minimum average dimension or the 10-foot minimum dimension 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Shopfront • Dooryard 		<ul style="list-style-type: none"> • Stoop • Raised terrace
Primary Entry Location		
• Street		• Podium-level Courtyard
Parking		
Type	Surface	Not Permitted
	Above-Grade	Screened by habitable spaces on all street-facing primary facades (with the exception of alleys)
Access	Alley	Permitted

7. “Wrap” Building



Three-dimensional diagram



Example of “Wrap” Building

Intent Statement			
The above-grade parking garage can be fully wrapped with building program or can be left open to a rear alley for access and ventilation. Building against garage is single-loaded corridor, while wings may be served by a double-loaded corridor.			
Location			
• Node		• Avenue	
• Commercial Village			
Building Height			
Min.	3 stories		
Maximum Facade Width			
Nodes	200 feet		
Avenue	No Maximum		
>175 feet must have at least one facade break of at least 20 feet in length and 10 feet in depth			
Maximum Upper-Level Frontage Occupancy			
Portions of facades above 45 feet in height and greater than 150 feet in length shall occupy no more than 80% of the primary facade plane established on the ground floor (see Figure 4).			
Maximum Allowed Footprint per Story			
Stories	1-3	4	5
3	100%	-	-
4-5	100%	90%	50%

Primary Facades		
• Street-facing		
Landscaping		
• Front yard landscaping shall primarily include hardscapes and potted plants.		
• Trees/vegetation is allowed in all other yards, except front yard.		
Open Space		
• Where provided, courtyards shall be at least 25 feet in any dimension, and at no time may be less than 10 feet in any dimension		
• Upper-floor balconies and patios may not project into minimum courtyard dimensions required to meet the 25-foot average minimum dimension or the 10-foot minimum dimension		
Primary Frontage Types		
• Shopfront		• Stoop
• Forecourt		
Primary Entry Location		
• Street		• Courtyard
Parking		
Type	Surface	Not Applicable
	Above-Grade	Concealed from street view behind building
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

8. Special Type Building – Urban “Large Format”



Three-dimensional diagram

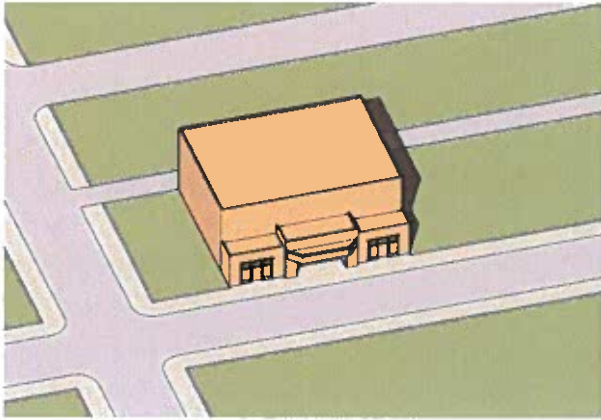


Example of Urban “Large Format” Building

Intent Statement	
<p>A multi-story building defined by its square, “L,” or “U” shape that allows for large-scale retail, office, or entertainment uses of at least 40,000 sf/floor for a single tenant, such as a grocery store or department store. Parking and loading should be designed to avoid vehicular/pedestrian conflict. Unlike “Big Boxes” in suburban areas, these typically have storefronts that create a pedestrian scaled environment and may have other uses such as office and/or residential above.</p>	
Location	
<ul style="list-style-type: none"> • Node • Avenue • Commercial Village 	
Building Height	
Min.	2 stories
Maximum Facade Width	
300 feet	
Primary Facades	
<ul style="list-style-type: none"> • Street-facing 	

Primary Frontage Types		
<ul style="list-style-type: none"> • Arcade/Gallery • Avenue-Commercial • Shopfront 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. • Parking lot landscaping shall be included to ensure that 50% of the total parking area is fully shaded within 5 years of the construction of the building. 		
Open Space		
<ul style="list-style-type: none"> • No minimum open space standard. 		
Primary Entry Location		
<ul style="list-style-type: none"> • Street 		
Parking		
Type	Surface	Not permitted
	Above-Grade	Screened by ground floor uses on all primary facades
Access	Alley	Permitted
	Secondary Street	Permitted

9. Special Type Building – Urban Theater



Three-dimensional diagram

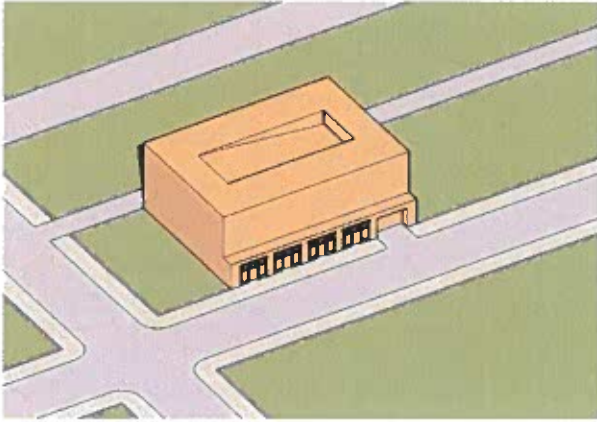


Example of Urban Theater

Intent Statement	
<p>A multi-story building that allows for auditorium spaces of different sizes, with a large floor-to-ceiling height, as well as common lobby areas. A distinguishing feature of the theater type is that shop windows and entrances are only utilized at the entry and occasionally in secondary lobbies above the first floor. Unlike suburban-style multiplexes, “Theaters” should be lined with storefronts accommodating other retailers so as not to present a blank wall to pedestrians. While this building type is defined by its internal volumes and primary uses, it is possible to have other uses connected to it or within it, including restaurants and cafes. Where height and FAR permit, additional retail uses may be accommodated below the theater, and/or office and residential may be accommodated above.</p>	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue 	
Building Height	
Min.	2 stories

Maximum Facade Width		
300 feet		
Primary Facades		
<ul style="list-style-type: none"> • Street-facing 		
Landscaping		
<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 		
Open Space		
<ul style="list-style-type: none"> • No minimum open space standard. 		
Primary Frontage Types		
<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront 		
Primary Entry Location		
<ul style="list-style-type: none"> • Street 		
Parking		
Type	Surface	Not permitted
	Above-Grade	Concealed from street view behind bldg.; Integrated
Access	Alley	Preferred
	Secondary Street	Permitted when alleys are not present

10. Special Type Building – Public Parking Structure



Three-dimensional diagram



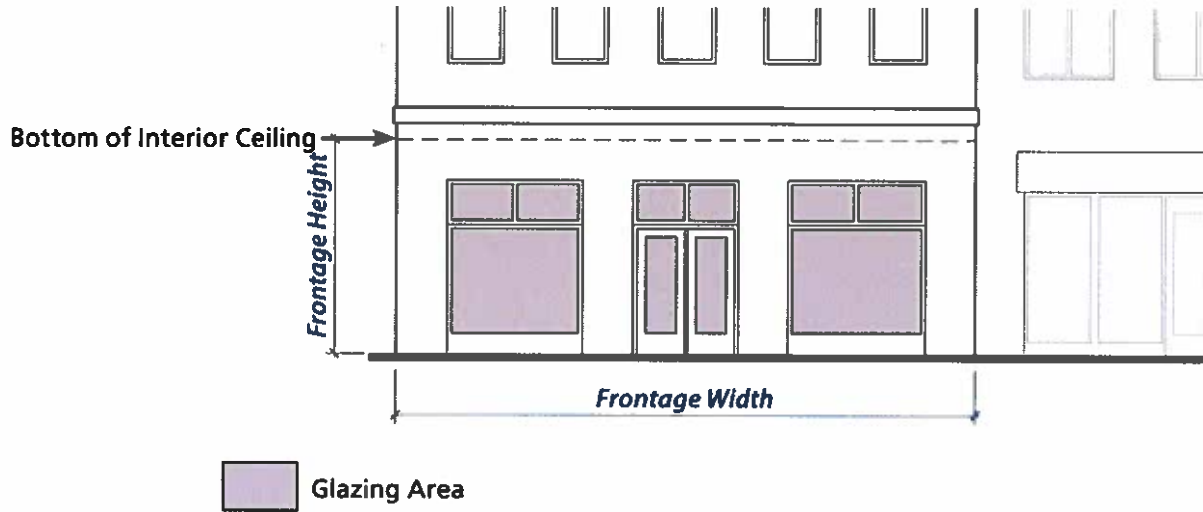
Example of Public Parking Structure

Intent Statement		Primary Facades	
An above-ground structure for vehicle public parking. This building type is designed to accommodate the need for shared public parking. Additional public uses should be considered for the top floor where views become available. Ground floor retail or office space along the edge of the building facing a public street and sidewalk are encouraged.		<ul style="list-style-type: none"> • Street-facing 	
Location		Primary Frontage Types	
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue 		<ul style="list-style-type: none"> • Arcade/Gallery • Shopfront 	
Building Height		Landscaping	
Min.	2 stories	<ul style="list-style-type: none"> • Front yard landscaping shall primarily include hardscapes and potted plants. 	
Max.	45 feet or 4 stories	Open Space	
Maximum Facade Width		<ul style="list-style-type: none"> • No minimum open space standard. 	
300 feet		Primary Entry Location	
Maximum Upper-Level Frontage Occupancy		Retail	Street
Portions of facades above the first floor shall be set back 10 feet		Parking	Street, or alley when possible
Maximum Allowed Footprint per Story		Required Parking	
Not Applicable		Not Applicable	
		Surface Parking	
		Not Applicable	
		Above-Grade Parking Garage	
		Not Applicable	
		Parking Access	
		<ul style="list-style-type: none"> • Entrances and exits may be no more than 3 lanes wide or 30 feet wide at sidewalk; • Ticket and payment machines should be recessed to allow significant stacking room inside structure. 	

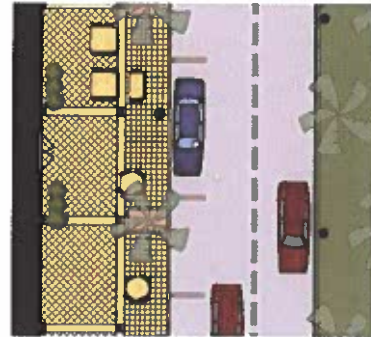
E. Frontage Type Standards

The frontage type standards and guidelines address the building-to-street relationship. Calculating the minimum frontage glazing is illustrated in Figure 5.

Figure 5. Minimum Frontage Glazing Diagram

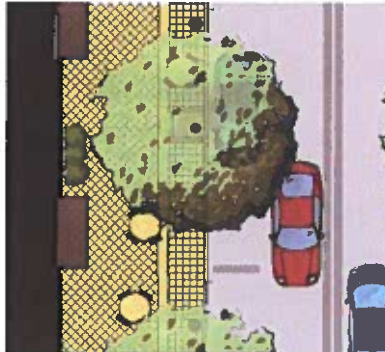


1. Arcade/Gallery



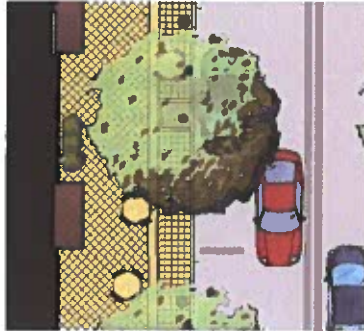
Intent Statement	
<p>A frontage wherein the facade is a colonnade that overlaps the sidewalk, while the facade at sidewalk level remains at the property line. This type is conventional for retail use. Arcades should be in alignment from building to building within a block.</p>	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
<ul style="list-style-type: none"> • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
Height to Underside of Arcade (H)	19 feet min. clear
Arcade Depth (D)	15 feet min.
Distance between Curb and face of Arcade (C)	2-4 feet
Paving and Landscaping	
<p>Area between the property line and building face shall be paved.</p>	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • Locate columns so they do not align with the entry doors of ground floor tenants. • Where an arcade does not wrap around a building's corner, open its end to facilitate the pedestrian path. • Arcades should not be used unless they encompass the primary pedestrian path of travel along the sidewalk. • Shading devices that obstruct views into the arcade from the street may not be used. • At least 60% of the arcade facade area at the ground floor shall be glazed, with at least 70% of the glazing to allow views into the store rather than being shallow window box displays (see Figure 5); Glazing shall be transparent and clear; Opaque, highly reflective, and dark tinting are not permitted. 	

2. Shopfront



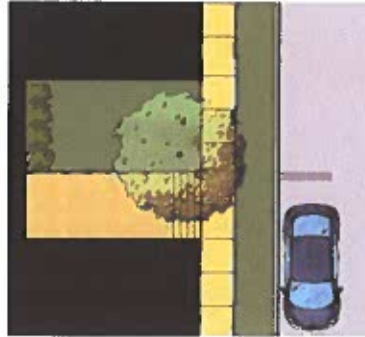
Intent Statement	
Shopfront frontages provide direct access to ground-floor spaces that are located adjacent to the sidewalk. Shopfronts are typically associated with retail uses but may accommodate other uses.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
<ul style="list-style-type: none"> • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
Shopfront Floor-to-Ceiling Height	15-21 feet
Paving & Landscaping	
Area between the property line and the building face shall be paved	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • At least 60% of the shopfront facade area at the ground floor shall be glazed, with at least 70% of the glazing to allow views into the store rather than being shallow window box displays (see Figure 5); Glazing shall be transparent and clear; Opaque, highly reflective, and dark tinting are not permitted. • Storefront window sill height may be no more than 30 inches high measured from the adjacent finished sidewalk • Unoccupied storefronts may be temporarily covered from the inside with white or light color paper, fabric or film, which may contain a graphic image or otherwise permitted sign • Maximum length of blank walls facing the street is limited to 15 horizontal feet for any one stretch 	

3. Live/Work



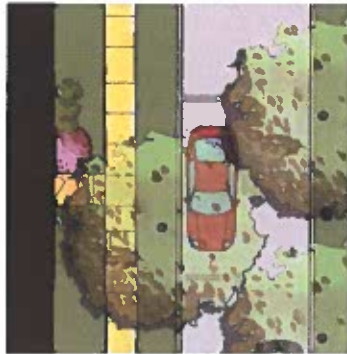
Intent Statement	
Live/Work frontages provide direct access to ground-floor spaces that are located adjacent to the sidewalk. Live/Work frontages may be associated with retail, offices or other work spaces.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village • Avenue 	
Entries	
<ul style="list-style-type: none"> • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
Live/Work – Floor-to-Ceiling Height	10 feet min.
Paving & Landscaping	
Area between the property line and the building face shall be paved	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • At least 35% of the Live/Work facade area at the ground floor shall be glazed (see Figure 5). Opaque, highly reflective, and dark tinting are not permitted. Glazed garage doors may satisfy this standard. • Live/Work window sill height may be no more than 30 inches high measured from the adjacent finished sidewalk. • Unoccupied Live/Work may be temporarily covered from the inside with white or light color paper, fabric, or film, which may contain a graphic image or otherwise permitted sign. • Maximum length of blank walls facing the street is limited to 15 horizontal feet for any one stretch. 	

4. Forecourt



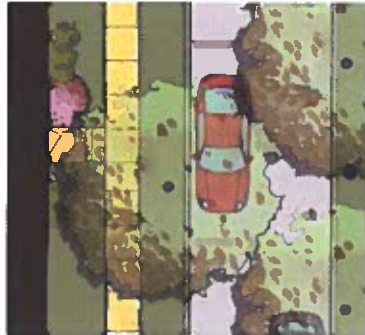
Intent Statement	
Forecourts are open areas located at primary building entrances. They may be designed as gardens or as paved courtyards. Frontages using a forecourt must comply with minimum frontage occupancy standards.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
Forecourt	Sidewalk
Building	3 feet max above adjacent forecourt
Dimensions	
Height	18 in. max above adjacent sidewalk
Depth	10-40 feet
Width	20-40 feet
Paving & Landscaping	
Forecourts may be planted with grass, shrubs, or other ground cover or be paved. All walks shall be paved.	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays • Planter pots • Water features 	
Additional Standards	
<ul style="list-style-type: none"> • Forecourts shall be open to the sky; porches are not permitted • Forecourts may be gated • Forecourts are not intended for access by automobiles • Minimum facade occupancy standards are not applicable where forecourts are used 	

5. Stoop



Intent Statement	
Stoops are small staircases leading to the entrance of a building. The stoop elevation provides some privacy between the sidewalk and ground-floor uses. Stoops may be covered.	
Location	
<ul style="list-style-type: none"> • Node • Commercial Village 	<ul style="list-style-type: none"> • Avenue
Entries	
<ul style="list-style-type: none"> • Sidewalk 	
Dimensions	
Height	3 feet max above adjacent sidewalk
Depth	4 feet min.
Width	4 feet min.
Paving & Landscaping	
Yards should be planted with grass, shrubs, or other ground cover. Walks shall be paved.	
Furnishing Zone	
Not Permitted	
Additional Standards	
<ul style="list-style-type: none"> • Awnings or canopies may cover stoops. • Elements from dooryard frontages and stoop frontages may be combined. 	

6. Dooryard



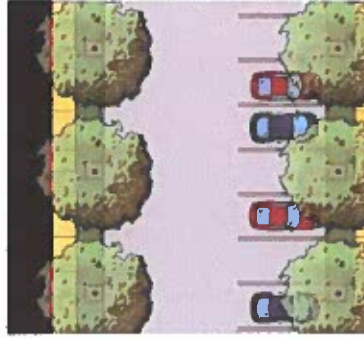
Intent Statement
Dooryard fronts are located in front setbacks and provide small landscaped and paved yards at buildings entrances. Dooryards are often enclosed by low walls, fences or hedges.
Location
<ul style="list-style-type: none"> • Node • Commercial Village <li style="text-align: right;">• Avenue
Entries
Ground-floor units in multi-family buildings with corridors may have the primary entry from a corridor accessible from a common building lobby, directly from the sidewalk via a dooryard, or both.
Dimensions
Not Applicable
Paving & Landscaping
Dooryards shall be planted with grass, shrubs, or other ground cover. Walks shall be paved. Low retaining walls, fences, or hedges may not exceed 3 feet in height measured from the sidewalk.
Furnishing Zone
<ul style="list-style-type: none"> • Loose Furniture
Additional Standards
Elements from dooryard frontages and stoop frontages may be combined.

7. Avenue – Residential



Intent Statement
A large front yard between a building face and sidewalk that provides a buffer and privacy for the building. A canopy or awning covering the walkway to the sidewalk provides a gracious entry to the building.
Location
<ul style="list-style-type: none"> • Avenue
Entries
Ground-floor units in multi-family buildings with corridors may have the primary entry from a corridor accessible from a common building lobby, directly from the sidewalk via a front yard, or both.
Dimensions
10-20 feet from property line to building face
Paving & Landscaping
Front yards shall be planted with grass, shrubs, or other ground cover. Walks shall be paved. Low retaining walls, fences, or hedges may enclose a front yard. Walls and hedges may not exceed 3 feet in height measured from the adjacent sidewalk.
Furnishing Zone
<ul style="list-style-type: none"> • Loose furniture
Additional Standards
<ul style="list-style-type: none"> • None

8. Avenue – Commercial



Intent Statement	
A street frontage facing a parking lot. Here, the parking lot shall be buffered from the public right of way with landscaping or wall separating the public realm from the private retail	
Location	
<ul style="list-style-type: none"> • Node 	<ul style="list-style-type: none"> • Commercial Village • Avenue
Entries	
<ul style="list-style-type: none"> • Avenue • Sidewalk • Alcove adjacent to sidewalk 	
Dimensions	
No greater than 75 feet from property line to building face	
Paving & Landscaping	
Area between the property line and the building face shall be paved	
Furnishing Zone	
<ul style="list-style-type: none"> • Outdoor seating • Product displays 	
Additional Standards	
<ul style="list-style-type: none"> • At least 60% of the shopfront facade area at the ground floor shall be glazed with at least 70% of the glazing to allow views into the store rather than being shallow window box displays (see Figure 5); Glazing shall be transparent and clear; Opaque, highly reflective, and dark tinting are not permitted. • Storefront window sill height may be no more than 30 inches high measured from the adjacent finished sidewalk. • Unoccupied storefronts may be temporarily covered from the inside with white or light color paper, fabric, or film, which may contain a graphic image or otherwise permitted sign. • Maximum length of blank walls facing the street is limited to 15 horizontal feet for any one stretch. 	

909 Architectural Standards

A. Purpose

The Architectural Standards regulate building orientation, access, projections, ceiling heights, and sustainability features.

B. Applicability

This section applies to all areas within the Incentive District.

C. General Architectural Standards

1. Building Orientation

- a. **Primary Facades.** All new or modified buildings shall orient the primary facade to the primary street. Secondary Facades front side and rear yards.
- b. **Corner Buildings.** Buildings at corners shall have primary facades fronting the Primary Street per Map 3 and confirmed by the City Planner. On some sites, corner buildings will front two Primary Streets. In these cases, both facades facing Primary Streets shall be considered primary.

2. Ground-Level Floor-to-Ceiling Heights

When not specified by the applicable frontage type in Section 908 (E), the minimum retail ground-floor-to-floor height shall be 15 feet. Minimum residential ground-floor-to-ceiling height shall be 10 feet.

3. Pedestrian Access

- a. **Primary Pedestrian Access.** Primary pedestrian access to building lobbies shall be provided along the sidewalk or permitted open spaces.
- b. **Mid-Block Paseos or Open Space.** Parcels longer than 300 feet shall be developed with a mid-block pedestrian paseo or open space. Where paseos are utilized, the placement shall emphasize connectivity to other paseos, alleys, or mid-block crossings.
- c. **Pedestrian paseos may be no less than 15 feet wide.**
- d. **If pedestrian paseos are covered, they shall have a floor-to-ceiling height of at least two times their width, but no greater than three times their width.**
- e. **Primary Entries.** Primary entries to ground floor tenant spaces or individual residential units shall be directly accessed from the sidewalk and occur at a maximum interval of 60 feet along primary streets.
- f. **Retail Frontages.** All frontages with retail uses shall provide tenant spaces with their own entry from the sidewalk, publically accessible open space or pedestrian paseo.

- g. Multiple entrances shall be incorporated for storefronts along a street front whose length spans the width of more than 90 feet. Where multiple entrances are required by this standard, they should be no more than 60 feet from the centerline of one entrance to the center-line of the other.
- h. Automobile drop-offs and porte cocheres may not be permitted along Primary Frontages where an alley is available. Exceptions may be made subject to the following findings and approval by the City Planner:
 - i. The project provides a community benefit in the form of public parking or public open space.
 - ii. Conflict points with pedestrians and vehicles are reduced to the extent feasible.
 - iii. All parking and service access areas are provided via the alley.
 - iv. Alternative surfacing materials and landscape areas clearly define the driveway entrance from the sidewalk and parking lanes.
 - v. Such street frontage elements do not compromise the viability of street-adjacent landscape areas.
- i. Parking is not permitted along any Primary Frontage.

4. Building Walls at the Ground Floor

When not specified by the applicable frontage type in Section 908 (E), along primary frontages, blank ground floor walls (defined as having no active use, glazing, or doorway) are limited to 20 percent of the linear building frontage, but in no case exceeding 40 feet of linear frontage.

5. Loading and Service

- a. Trash enclosures and retail loading areas shall be provided off of an alley where practical.
- b. Service, utility and mechanical equipment that is visible from the street should be screened from view with landscaping or enclosures.
- c. Back flow and standpipes, along with utility box transformers shall be screened.
- d. Chain link fencing with slats or mesh screen and unpainted wood are not permitted as screening materials.
- e. Mechanical equipment shall vent to an alley where practical.

6. Fenestration

- a. Ground Floor

- i. Provide large windows and window display boxes along ground floor commercial spaces to activate the street and allow pedestrians to view the merchandise for sale, restaurant interiors, dance classes, art galleries, artists at work, etc. At least 60 percent of Storefront Frontage shall be glazed.
 - ii. Use glazing that has limited UV tinted glazing so as to provide views into the building from the street. Glazing at ground-level storefronts and offices shall have a Visible Light Reflected Out value of 12 percent or less, and a Visible Transmittance value of 0.65 or more. Developers seeking relief from this standard based on Title 24 calculations shall provide those calculations to the City Planner as evidence that all other alternative measures to comply with Title 24 requirements, such as the use of shading devices, are insufficient.
 - iii. The ground floor window sill height may be no higher than 30 inches above the segment of sidewalk immediately adjacent to the window.
 - iv. Storefront windows shall be recessed at least 6 inches from building face.
 - v. Interior floor displays shall not obstruct views into the store.
 - vi. Exterior store displays against storefronts may not exceed 36 inches above adjacent sidewalk so as to not obstruct views into the store.
- b. Upper Level
- i. Upper-level glazing shall have a Visible Light Reflected Out value of 15 percent or less and a Visible Transmittance value of 50 percent or more.

7. Roofs

- a. Rooftop mechanical equipment, including roof vent penetrations, shall be screened from public view.

8. Projections

No projection may extend into the public right-of-way except as provided below. Projections into the public right-of-way shall be limited as follows:

- a. Awnings/Canopies. Awnings and canopies may project no more than 8 feet from the building face or 33 percent of the distance between the building face and the curb, whichever is less. The minimum vertical clearance for awnings and canopies shall be 8 feet if removable or retractable and 12 feet if fixed or permanent.

Awning and canopy heights on a building shall be designed to be consistent along the facade and/or frontage line so as to maintain a consistent street edge.

- b. Theater Marquees. Theater marquees may not project over the public sidewalk within 6 feet of the curb. Signage may not project more than 3 feet from the building face along block frontages where the distance from the curb to the building face is less than 18 feet in width and no more than 4 feet along block frontages where the distance between the curb and the building face is over 18 feet in width. If signage sits on the ledge of a canopy, then the guidelines for canopies shall apply (see above).
- c. Balconies. Balconies may not project into required setbacks by more than 7 feet from the building façade and may not extend beyond the property line.
- d. Projecting Habitable and Non-Habitable Space. The minimum vertical clearance below permitted Projecting Habitable and Non-Habitable Space shall be equivalent to the floor-to-floor ground floor height standard. The combined length of Projecting Habitable and Non-Habitable Spaces along the building face may not exceed 67 percent of the total length of the building face to which they are attached.

9. Encroachments

Encroachments shall be limited as follows:

- a. Stoops. Stoops are permitted for ground floor residences only, setbacks from the property line, and the grade change for the first floor residents as doors and windows are raised from the sidewalk grade.
 - 1. Stoops may not extend more than 8 feet from the building face and may not extend beyond the property line.
 - 2. The finished floor of the stoop may be a maximum of 42 inches above the adjacent sidewalk grade.
 - 3. Fences or walls defining the stoop may not exceed the height required by the building.
 - 4. Stoops may be parallel or perpendicular to the sidewalk from which they are accessed.
 - 5. Where they extend beyond the front setback line and run parallel to the sidewalk, the total length of the stoop or stoops may not exceed 40 percent of the total building frontage.
 - 6. Where they are perpendicular to the sidewalk, they may be combined with non-encroaching raised planters or terraces.
- b. Outdoor Seating, Dining and Temporary Display. These areas may encroach into the front setback, but may not encroach into the public right of way unless a clear area of travel is maintained of at least 6 feet in width not encroached upon by tree grates or street furniture.

- c. Raised Terraces. Raised terraces adjacent to storefronts with dining areas shall be a minimum of 7 inches in height. Raised terraces may be no taller than 42 inches as measured from the sidewalk grade. Raised terraces are permissible for restaurant and hotel uses located mid-block and behind the required minimum front yard setback line.
- d. Railings. Any railings that may be required by building code or desired windscreens shall be at least 80 percent transparent.
- e. Underground Parking. Underground parking may extend past the minimum front yard setback line, up to the property line.

10. Architectural Lighting

- a. Building Lighting and Fixtures
 - i. Frontages, entrances, arcades pathways, corner plazas, and adjacent public sidewalks shall be illuminated for pedestrian safety. Lighting may be freestanding or attached to the building. Lighting provided for pedestrian safety shall be mounted between 10 and 14 feet above the ground. Lighting along alleys shall be connected to a separate circuit.
 - ii. Lighting sources shall be shielded so that light is aimed downward or back at the building wall.
 - iii. Incandescent and high-pressure sodium exterior lights are prohibited.

910 Large Lot Standards

A. Purpose

- 1. The purpose of this section is to provide standards for the development and redevelopment of large parcels or assemblages.

B. Applicability

Any parcel or parcel assemblage with a contiguous area of 60,000 sf or more shall be developed according to these Large Lot Standards.

C. Submittal Standards

An application for a Development Plan review qualifying under this section shall include, at a minimum, a plan sheet with diagrams and text which identifies proposed individual building sites and their dimensions, existing adjacent streets, proposed new streets, proposed building types, proposed frontage types, and the relationship of the project site to its surroundings.

D. Relationship to Land Use Standards, Urban Standards, and Architectural Standards

- 1. Each building within a large lot development shall comply with the applicable standards in the Land Use Standards, Urban Standards, and Architectural Standards. However, standards shall be amended as follows:

- a. **Primary Frontage Line:** For buildings not abutting an existing street, the plan shall introduce a new street, alley or pedestrian paseo that is open to the sky for at least 50 percent of its length and accessible to the public at least during daylight hours and a proposed Primary Frontage Line along that street or paseo. Buildings proposed to front the new street or pedestrian paseo are not subject to any setback standards from the new street or paseo.
- b. Development on parcels or parcel assemblages exceeding 90,000 sf shall be subject to Section 910 (C) (1) a, above, and be composed of at least two distinct buildings (not connected via interior circulation, except as part of a subterranean parking structure) which may be the same or different building types as allowed by the applicable subarea.
- c. Frontage Types shall be permitted based upon the applicable subarea in which the project is located.

E. Access

1. In cases where parcel sizes exceed 90,000 sf, development shall provide for new streets and/or pedestrian paseos, according to the rules below and as shown in Figure 6.
 - a. Vehicular and pedestrian access shall include new internal streets, alleys, or driveways. Cul-de-sacs and dead-end streets are prohibited except where public utility constraints (e.g., railroad right-of-ways) prohibit through streets. Alleys or woonerfs (i.e., roads in which devices for reducing or slowing the flow of traffic have been installed) may be dead-end if they allow for future connection to adjacent parcels. Garage doors shall face alleys, woonerfs, or driveways. Flag lots are prohibited.
 - b. Parcels over 90,000 sf shall require at least one publicly accessible bisecting street, alley, or woonerf. Where such parcels border an existing public street and an existing alley sufficient to service the entire parcel, then the site may be partially bisected by a pedestrian paseo (minimum 15 feet wide between primary building walls) that is open to the sky for at least 50% of its length and accessible to the public at least during daylight hour in lieu of a street, alley, or woonerf.
 - c. Lots over 125,000 sf shall require at least one publicly accessible street. If the resulting subdivision includes parcels over 125,000 sf, they too shall require one publicly accessible bisecting street. If the resulting subdivision includes parcels over 90,000 sf, they too shall require one publicly accessible bisecting street, alley, or woonerf or, where conditions apply, may be partially bisected by a pedestrian paseo (see above for allowable conditions) as per Section 910 (D) (1) b, above.

- d. New streets shall be designed to emphasize the pedestrian experience. Roadway widths shall be the minimum deemed acceptable by the City Engineer and Fire Department.

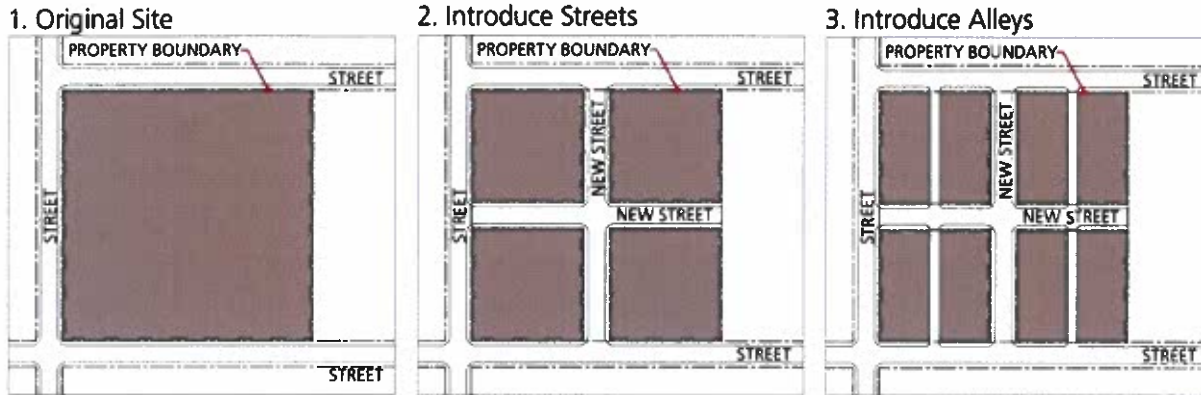


Figure 6. Large Lot Development

911 Parking Standards

A. Applicability

All development and redevelopment established under the Incentive District shall comply with the parking standards contained in Article 31 Off-Street Parking and Loading Regulations of the 1992 Ordinance, with the exception of the provisions contained in subsections (B) and (C) below.

B. Minimum Parking Standards

Table 4. Required Parking Spaces

Use	Parking Spaces Required
Nonresidential Uses ¹	1 for each 500 sf of gross floor area
Artisan Manufacturing	1 for each 800 sf of gross floor area
Eating and Drinking Establishment ²	1 for each 125 sf of seating area
Visitor Accommodations	1 per unit
Personal Services	1 for each 600 sf of gross floor area
Residential ⁴	1 parking space per 1,500 sf of habitable space ³
Notes:	
1) The nonresidential use category shall apply to all nonresidential uses within the Incentive District except those explicitly listed in Table 4 or in this footnote.	
2) Eating and Drinking Establishments shall include; Restaurant, Full Service; Restaurant, Fast Food; Bars and Cocktail lounges; Food and Beverage Sales; Breweries, Craft; Wineries, Craft as defined in Section 912.1.	
3) The minimum parking standard is calculated from a total aggregate square footage of habitable space for the residential project.	
4) No parking spaces shall be required for housing units dedicated to affordable housing units, senior housing units, or SROs.	

C. Additional Parking Provisions

1. No parking spaces shall be required to be individually accessible. However, parking spaces may not be more than two-deep (tandem), unless they are served by mechanical systems or robotic parking that allows for independent access.
2. Shared on-site parking among land uses with different periods of peak parking demand shall be allowed for all uses in the plan area. Shared on-site parking shall be allowed to satisfy 100 percent of the parking standard for each use, so long as documentation can be provided that the existing and anticipated land uses will have different periods of peak parking demand and the shared parking can accommodate the parking demand for both uses.
3. Off-site parking within 1,250 feet shall be allowed for all uses in the plan area. Off-site parking further than 1,250 feet should be allowed at the discretion of the City Planner as long as documentation that a shuttle bus service or valet parking service will be provided.
4. Upon establishment of an in-lieu parking fee, payment of the in-lieu fee may be used to 100 percent satisfy the required interim parking standard for each parking space that is not provided.

912 Definitions

912.1 Land Use Classifications

The use classifications defined in this section are listed in Section 907 (C) of this article. Definitions identified as “New” are unique to the Coast Highway Incentive District. Definitions identified as “Existing” are defined by Article 4 Use Classifications of the 1992 Ordinance. Definitions identified as “Existing/Modified” include slight modifications from the Article 4 Use Classifications of the 1992 Ordinance.

A. Animal Sales and Service (Existing)

1. Animal Boarding. Provision of shelter and care for animals on a commercial basis. This classification includes activities such as feeding, exercising, grooming, and incidental medical care. (Existing)
2. Animal Grooming. Provision of bathing and trimming services for animals on a commercial basis. This classification includes boarding of domestic animals for a maximum period of 48 hours. (Existing)
3. Animal Hospitals. Establishments where animals receive medical and surgical treatment. This classification includes only facilities that are entirely enclosed, soundproofed, and air-conditioned. Grooming and temporary (30 days) boarding of animals is included if incidental to the hospital use. (Existing)
4. Animals: Retail Sales. Retail sales and boarding of animals provided such activities take place within an entirely enclosed building. This classification

includes grooming if incidental to the retail use, and boarding of animals not offered for sale for a maximum period of 48 hours. (Existing)

- B. Artisan Manufacturing. A facility accommodating manufacturing processes involving and/or producing: apparel; food and beverage products; electronic, optical and instrumentation products; jewelry; and musical instruments. Artisan manufacturing also includes other establishments engaged in the assembly, fabrication, and conversion of already processed raw materials into products, as well as the incidental storage, sales and distribution of such products, where the operational characteristics of the manufacturing processes and the materials used are unlikely to cause impacts on surrounding land uses or the community. Typical artisan manufacturing uses include, but are not limited to; food and beverage product manufacturing such as breweries, wholesale bakeries, and coffee roasting; printing and publishing; art and jewelry making, clothing and fabric production; metal work; furniture making; glass or ceramic production; photo/film processing; and paper product manufacturing. (New)
- C. Assembly/Meeting Facility. Meeting, recreational, or social facilities of a private or nonprofit organization primarily for use by members or guests. Includes clubs, lodges, private meeting halls, religious assembly facilities (e.g., churches, mosques, synagogues, etc.), private auditoriums, union halls, etc. This use classification excludes private schools as defined in this section. (Existing/Modified – Clubs and Lodges and Religious Assembly)
- D. Bars and Cocktail Lounges. Any premises designed, used or intended to be used for the selling or serving of alcoholic beverages to the public for consumption on the premises, and in which food is not sold or served to the public as in a bona fide restaurant. (Existing)
- E. Child Care. Non-medical care and supervision on a less than 24-hour basis in any care facility of any capacity, and not within a licensee’s home for persons under the age of 18. (Existing)
- F. Commercial Recreation and Entertainment – Indoor. Provision of participant or spectator recreation or entertainment. This classification includes bowling alleys, billiard parlors, pool rooms, dance halls, ice/roller skating rinks, scale-model courses, shooting galleries, arcades and game centers having five or more coin-operated game machines and card rooms. This does not include outdoor facilities such as amusement parks, batting cages, go-cart tracks, golf-driving ranges, and miniature golf courses. (Existing/Modified).
1. Dance Establishment. Any premises wherein a public dance, as defined in the Oceanside City Code, is held. (Existing)
 2. Pool Rooms, Billiard Rooms, and Shooting Galleries. Pool rooms, billiard rooms, and shooting galleries as defined in the Oceanside City Code. (Existing)

3. Arcades and Game Centers. Any place having five or more coin-operated, slug-operated, or any type of amusement or entertainment machines for which payment is necessary for operation. These include, but are not limited to, pinball machines and video games, but do not include merchandise vending machines or mini-jukeboxes (see Article 36 Separation of Regulated Uses of the 1992 Ordinance). (Existing)
- G. Brewery, Craft. A small-scale beer manufacturing facility that includes designated floor area (comprising no less than 20 percent and no more than 40 percent of the total floor area) for product sampling and/or retail sales of beer conforming to state requirements. Retail sales of craft brewery merchandise including mugs, pint glasses, growlers, tap handles, coasters, apparel, signs, bottle openers, and books are also permitted. A craft brewery shall produce a minimum of 250 barrels annually and not exceed a maximum output of 15,000 barrels annually (with a barrel of beer being equal to 31 U.S. gallons). Facilities housing a craft brewery shall not exceed 15,000 square feet of floor area. The category of a craft brewery (based upon floor area and scale of operation) shall determine the appropriate review and approval process, as specified below. Such facilities shall be subject to the following limitations:
1. Other than business name and/or logo, such uses shall not display exterior signage (including advertising directed to the exterior from interior spaces) that promotes the availability of alcoholic beverages.
 2. There shall be no amusement or video machines maintained on the premises.
 3. There shall be no “happy hour” or regular periods of reduced-priced alcoholic beverages.
 4. Amplified live entertainment shall require issuance of an Administrative Use Permit.
 5. Ancillary food service involving outdoor equipment (e.g., food trucks) shall require issuance of an Outdoor Facilities Permit.
 6. Delivery/distribution activities involving Class 4 or higher vehicles shall not occur between the hours of 10 p.m. and 7 a.m.
 7. Hours of operations shall not extend beyond 10 p.m., unless extended hours of operation are approved through a Conditional Use Permit.

Craft breweries shall be regulated under the following tiered review and approval process:

1. Tier 1: Produces at least 250 and no more than 10,000 barrels annually and does not exceed 10,000 square feet of floor area. Tier 1 facilities are subject to approval of an Administrative Conditional Use Permit (ACUP) when not abutting residential zoning districts, schools, and/or churches. Tier 1 facilities abutting

residential zoning districts, schools, and/or churches are subject to approval of a Conditional Use Permit (CUP).

2. Tier 2: Produces at least 750 and no more than 15,000 barrels annually and does not exceed 15,000 square feet of floor area. Tier 2 facilities are subject to approval of a Conditional Use Permit (CUP).
- H. Cultural Institutions. Nonprofit institutions displaying or preserving objects of interest in one or more of the arts or sciences. This classification includes libraries, museums, and art galleries. (Existing)
- I. Financial Services. Financial institutions that provide retail banking services to individuals and businesses. This classification is limited to institutions engaged in the on-site circulation of cash money including businesses offering check-cashing facilities. (Existing/Modified)
1. Drive-Through/Drive-Up Service. Institutions providing self-service banking facilities that are not associated with a primary banking or savings and loan building located on the same site. (Existing)
 2. Self-Service Facilities (ATMs). Institutions providing self-service banking facilities that are not associated with a primary banking or savings and loan building located on the same site. (Existing)
- J. Food and Beverage Sales. Retail sales of food and beverages for off-site preparation and consumption. Typical uses include groceries, liquor stores, or delicatessens. Establishments at which 20 percent or more of the transactions are sales of prepared food for on-site or take consumption shall be classified as Catering Services. (Existing/Modified)
- K. Hospitals. Facilities providing medical, surgical, psychiatric, or emergency medical services to sick or injured persons, primarily on an inpatient basis. This classification includes incidental facilities for outpatient treatment, as well as training, research, and administrative services for patients and employees. (Existing)
- L. Offices, Business and Professional. Offices of firms or organizations providing professional, executive, management, or administrative services, such as architectural, engineering, real estate, insurance, investment, legal, and medical/dental offices. This classification includes medical/dental laboratories incidental to an office use, but excludes banks and savings and loan associations. (Existing)
- M. Parking as a Primary Use Land Use. Any lot or space where vehicles may be kept, sometimes for a fee, and includes public parking areas. (New)

- N. Park and Recreation Facilities. Noncommercial parks, playgrounds, recreation facilities and open spaces. Includes squares, plazas, outdoor theaters, skateparks, dog parks, trails, beaches, playgrounds, sports fields, greenbelts and other park, recreation and open space areas, including buildings and facilities that comprise the parks and recreation system of the city under the management and control of the Neighborhood Services Department. (Existing/Modified)
- O. Personal Improvement Services. Provision of instructional services or facilities, including; photography, fine arts, crafts, dance or music studios, driving, business or trade schools, diet centers or reducing salons, health and fitness studios, spas or clubs. (Existing)
- P. Health/Fitness Studios, Spas or Clubs. A fitness center, gymnasium, health and athletic club or studio, which may include any of the following: sauna, spa or hot tub facilities; indoor tennis, handball, racquetball, archery and shooting ranges, rock climbing and other indoor sports activities. Outdoor areas may be used as accessory to the primary use held indoors. (New)
1. Massage Establishments. Establishments providing massage service. (Existing)
- Q. Personal Services. Any premises primarily engaged in the provision of non-medical services for the enhancement of personal appearance, cleaning, alteration or reconditioning of garments and accessories, and similar non-business-related or nonprofessional services. These uses may also include accessory retail sales of products related to the services provided. Personal services include barber shops or beauty parlors, dry cleaning, self-service laundromats, locksmiths, tailors and shoe repair shops, tattoo parlors, and similar establishments catering directly to consumers. (Existing/Modified)
1. Tattooing Establishment. Any establishment or business engaged in “tattooing” as defined in the Oceanside City Code, excluding “micropigmentation” or “permanent cosmetic makeup” typically provided at beauty salon or day spas.
 2. Body Piercing Establishment. Any establishment or business engaged in “body piercing.” Body piercing means the creation of an opening in the body of a human being for the purpose of inserting jewelry or other decoration. This includes, but is not limited to, piercing of an ear, lip, tongue, nose, eyebrow, naval, genitals, breasts. “Body piercing” does not include piercing an ear with a disposable, single-use stud or solid needle that is applied using a mechanical device to force the needle or stud through the ear.
- R. Residential. Any building or portion thereof which is used for residential purposes, including multi-family dwelling units, live/work units, group houses, rest homes, residential care facilities, and rest homes, and accessory buildings and structures. (New)

1. Group Residential. Shared living quarters without separate kitchen or bathroom facilities for each room or unit. This classification includes roomhouse/boardingshouse, dormitories, fraternities, sororities, and private residential clubs, but excludes residential hotels (see Single-Room Occupancy (SRO) Residential Hotels). (Existing)
 2. Live/work. A mixed-use unit consisting of an office land use or retail land use with a residential land use. The habitable area may be anywhere in the unit and is intended to be occupied by a business operator who lives in the same structure that contains the office or retail activity. (New)
 3. Multifamily Residential. Two or more dwelling units on a site. This includes two-family dwelling units, three-family dwelling units, four-family dwelling units, zero lot line development, townhomes, and apartments. (New)
 4. Convalescent Facilities. Establishments providing care on a 24-hour basis for persons requiring regular medical attention, but excluding facilities providing surgical or emergency medical services. (Existing)
- S. Restaurants, Fast Food. A bona fide restaurant establishment where the principal business is the sale of prepared or rapidly prepared food and beverages to guests via counter, walk up, or window service for consumption on or off the premises. The sale of beer and wine for on-site consumption is permitted. As used in this definition, a “bona fide” restaurant shall have suitable kitchen facilities for cooking and/or preparation of meals. The word “meals” means the assortment of food commonly ordered at various hours of the day. (Existing)
1. Restaurants, Fast Food with Drive-thru or Drive-up. A restaurant establishment providing service from a building to patrons in vehicles through an outdoor service window (Drive-thru) or delivery service to vehicles parked in designated parking spaces (Drive-up). The sale and consumption of alcoholic beverages at a restaurant with a Drive-thru or Drive-up window is prohibited.
- T. Restaurant Full Service. A bona fide restaurant establishment where the principal business is the sale of food and beverages to guests via table service for consumption on the premises. The sale of beer and wine for on-site consumption shall be considered incidental to the full service restaurant. Delivery service to vehicles parked in designated parking spaces (i.e. drive-up) is allowed as an ancillary service to the Restaurant Full Service. As used in this definition, a “bona fide” full service restaurant shall have suitable kitchen facilities for cooking of complete meals. The word “meals” means the assortment of foods commonly ordered at various hours of the day; the service of only such foods as sandwiches or salads does not meet the bona fide restaurant definition.
1. Restaurants Full Alcohol. A bona fide restaurant establishment authorized to sell distilled spirits for consumption on licensed premises. The sale of liquor is

included as an appurtenant use to full service restaurants having table seating and service for more than 50 guests.

2. Restaurants Full Service with Live Entertainment – (Small Scale). Restaurant establishments providing live entertainment to patrons with 5 or fewer performers at restaurant facilities with no dance floor during typical lunch and dinner hours (11:00 a.m. – 11:00 p.m.) and having 75 percent food sales compared to alcohol sales.
- U. Retail. Premises available for the sale of many lines of merchandise and food service. These include, but are not limited to: art galleries, art supplies, bakeries, book or stationary shops, cameras and photographic supplies, collectibles, dress or millinery shops, drug stores, dry goods, florist shops, gifts, sundries and souvenir shops, grocery stores, hardware stores, jewelry stores, meat markets or delicatessen stores, recreation equipment rental and sales, shoe stores convenience food store, liquor store, and secondhand store. (New)
1. Convenience Market. Retail sales of food, beverage and small convenience items typically found in establishments with long or late hours of operation. This definition excludes delicatessens and other specialty food shops having a sizeable assortment of fresh fruits and vegetables, and fresh-cut meat or fish. (Existing)
 2. Drive-in facilities. Any place of business, excluding gasoline service stations, which transact any part or all of its business directly with customers within a vehicle. (New)
 3. Liquor store. A place or business engaged in the primary business of off-sale alcoholic beverages. For the purposes of article, primary business shall mean 25 percent or more of the shelf area of a business. (Existing)
 4. Pawnshops. Establishments engaged in the buying or selling of new or secondhand merchandise and offering loans secured by personal property and subject to Chapter 22 of the Municipal Code. (Existing)
 5. Recreational equipment rental and sales. Shall mean any recreational facilities operated as a business and open to the general public for a fee. This includes bicycles, commercial fishing, diving, and sportfishing establishments including supplies and services. (Existing/Modified)
 6. Secondhand Furniture, Appliances, “Collectibles,” and Clothing Sales. The retail sale of used furniture, appliances, “collectibles,” and clothing, and secondhand dealers who are subject to Chapter 22 of the Municipal Code. This classification excludes antique shops primarily engaged in the sale of antique furniture and accessories. (Existing)

- V. Schools. A public or private academic institution including school providing specialized education/training. Example include: Elementary, middle and high school, art schools, computer and electronic school, professional school (law, medicine, etc.) Also includes facilities, institutions and conference centers that offer specialized programs for personal growth and development. Does not include child care facilities (see Child Care) or Studios used for smaller-scale facilities offering specialized instruction. (New)
- W. Studios. Small-scale facilities, typically accommodating one group of students at a time, in no more than one instructional space. Larger facilities are included under the definition of Schools. Examples of these facilities include: individual and group instruction and training in the arts; production rehearsal; photography, and the processing of photographs produced only by users of the studio facilities; martial arts training studios; gymnastics instruction, and aerobics and gymnastics studios with no other fitness facilities or equipment. Also includes production studios for individual musicians, painters, sculptors, photographers, and other artists. (New)
- X. Theaters. An indoor facility for group entertainment. Examples include movie theaters and facilities for live theater and concerts. (New)
- Y. Vehicle/Equipment Sales and Services. This does not include commercial parking facilities (see Parking as a primary use). (Existing/Modified)
1. Automobile Washing. Washing, waxing, or cleaning of automobiles or similar light vehicles. (Existing)
 2. Service Stations. Establishments engaged in the retail sale of gas, diesel fuel, lubricants, parts, and accessories. This classification includes incidental maintenance and repair of automobiles and light trucks, but excludes body and fender work or repair of heavy trucks or vehicles. (Existing)
 3. Vehicle/Equipment Repair. Repair of automobiles, trucks, motorcycles, mobile homes, recreational vehicles, or boats, including the sale, installation, and servicing of related equipment and parts. This classification includes auto repair shops, body and fender shops, wheel and brake shops, and tire sales and installation, but excludes vehicle dismantling or salvage and tire retreading or recapping. (Existing)
 4. Vehicle/Equipment Sales and Rentals. Sale or rental of automobiles, motorcycles, trucks, tractors, construction or agricultural equipment, mobile homes, and similar equipment, including storage and incidental maintenance. (Existing)
 5. Vehicle Storage. Storage of operative or inoperative vehicles. This classification includes storage of parking tow-aways, impound yards, and storage lots for automobiles, trucks, buses, and recreational vehicles, but does not include vehicle dismantling. (Existing)

- Z. Visitor Accommodations. A facility that offers transient lodging accommodations to the general public, and includes campgrounds and recreational vehicle parks, hotels, motels, tourist cottages, and accessory uses. (Existing/Modified)
1. Bed and Breakfast Inns. Establishments offering lodging on a less than weekly basis in a converted single-family or multi-family dwelling, with incidental eating and drinking service for lodgers only provided from a single kitchen. (Existing)
 2. Hotels, Motels and Time-Share Facilities. Establishments offering commercial lodging on a less than monthly basis. This classification includes incidental eating, drinking, and banquet services intended for the convenience of guests. (Existing)
 3. Single-Room Occupancy (SRO) Residential Hotels. Buildings with six or more guest rooms without kitchen facilities in individual rooms, or kitchen facilities for the exclusive use of guests, and which are also the primary residences of the hotel guests. (Existing)
 4. Vacation Club. Prepaid or credit based establishments offering lodging on a less than weekly basis and having kitchens. This classification includes eating, drinking and banquet services. (Existing)
- AA. Winery, Craft. A small-scale winemaking facility that includes designated floor area (comprising no less than 20 percent and no more than 40 percent of the total floor area) for product sampling and/or retail sales of wine conforming to state requirements. Retail sales of craft winery merchandise including wine glasses, wine bottles, decanters, corkscrews, home décor, apparel, signs, books, and other wine paraphernalia are also permitted. A craft winery shall produce a minimum of 300 cases annually and not exceed a maximum output of 10,000 standard cases annually (with a standard case of wine containing twelve 750 ml bottles or nine liters of wine). Facilities housing a craft winery shall not exceed 15,000 square feet of floor area. The category of a craft winery (based upon floor area and scale of operation) shall determine the appropriate review and approval process, as specified below. Such facilities shall be subject to the following limitations:
1. Other than business name and/or logo, such uses shall not display exterior signage (including advertising directed to the exterior from interior spaces) that promotes the availability of alcoholic beverages.
 2. There shall be no amusement or video machines maintained on the premises.
 3. There shall be no “happy hour” or regular periods of reduced-priced alcoholic beverages.
 4. Amplified live entertainment shall require issuance of an Administrative Use Permit (ACUP).

5. Ancillary food service involving outdoor equipment (e.g., food trucks) shall require issuance of an Outdoor Facilities Permit.
6. Delivery/distribution activities involving Class 4 or higher vehicles shall not occur between the hours of 10 p.m. and 7 a.m.
7. Hours of operations shall not extend beyond 10 p.m., unless extended hours of operation are approved through a Conditional Use Permit.

Craft wineries shall be regulated under the following tiered review and approval process:

1. Tier 1: Produces at least 300 and no more than 7,000 standard cases annually and does not exceed 10,000 square feet of floor area. Tier 1 facilities are subject to approval of an Administrative Conditional Use Permit (ACUP) when not abutting residential zoning districts, schools, and/or churches. Tier 1 facilities abutting residential zoning districts, schools, and/or churches are subject to a Conditional Use Permit (CUP).
2. Tier 2: Produces at least 900 and no more than 10,000 standard cases annually and does not exceed 15,000 square feet of floor area. Tier 2 facilities shall be subject to approval of a Conditional Use Permit (CUP).

912.2 Architectural and Development Standards

- A. Arcade. A frontage type typical for retail uses wherein the facade is a colonnade supporting habitable space that overlaps the sidewalk, while the facade at sidewalk level remains at the primary frontage line.
- B. Bay System. The spaces between posts, columns, or buttresses in the length of a building.
- C. Blockfronts. The aggregate of private lots, passages, alleys, circumscribed by streets.
- D. Colonnade. A row of columns supporting a roof or arcade.
- E. Encroachments. An encroachment is defined as either a fixed or moveable architectural or furnishing element that projects beyond the building face at the ground level or below ground.
- F. Facade Compositional Strategy. The combining of distinct parts or elements of building facades to form a unified whole.
- G. Habitable Projecting Space. Habitable Projecting Space is a portion of a building enclosed by walls and a roof that extends beyond the building face (i.e., bay windows and other architectural projections).
- H. Massing. The perception of the general shape or form as well as the size of a building.

- I. Mixed-Use Development. Development that includes two or more land uses.
- J. Non-Habitable Projecting Space. Non-habitable Projecting Space is a space used by occupants that are not enclosed by walls and a roof, such as balconies. They should not extend more than 6 feet from the building face and should not extend beyond the property line.
- K. Porte Cochere. A vehicular passageway leading through a building or screen wall into an interior courtyard. A porte cochere can also refer to a roofed structure extending from the entrance of a building over the adjacent driveway and sheltering those getting in or out of vehicles.
- L. Pedestrian Paseo. A pathway, open or roofed, that serves as an extension of the sidewalk as it passes from the primary frontage line onto private property, often between buildings, to courtyards, parking areas, or civic spaces.
- M. Primary Frontage Line. A setback, parallel to the primary street, which marks the location from which the principle vertical plane of the building façade must be erected.
- N. Primary Street. Streets with key circulation, mix of intensities, more pedestrian and vehicular accommodation than secondary streets, most developed street, with a mix of uses and access to transit.
- O. Secondary Street. Street with single use development as opposed to mixed-use development that are fed from primary streets, have less circulation than primary street, have less mix of intensities than primary streets, and have less of pedestrian and vehicular traffic than the main streets.
- P. Standalone Residential. A residential development where residential is the primary use and other land uses such as retail or office are not included as a primary or secondary use. A standalone residential development may include open space and other uses considered incidental in a residential development project.
- Q. Stoops. Stoops are elevated entry porches with stairs paced close to the front setback line.
- R. Transoms. A transverse horizontal structural beam or bar, or a crosspiece separating a door from a window above it.
- S. Underlying Zoning. The underlying zoning district per the 1986 zoning ordinance.
- T. Visible Light Reflected Out. A value describing the percentage of total visible light that is reflected by a window glass and film system.
- U. Visible Transmittance. A value describing the percentage of visible light that can pass through a window glass and film system.
- V. Woonerfs. A road in which devices for reducing or slowing the flow of traffic have been installed.

912.3 Frontage and Building Type Terms

- A. Allowable Building Types. Defines which building types are permitted within each subarea. Allowable building types are provided in 908 (C) and (D) and the following describes the standards provided for each building type.
1. Building Intent Statement. This statement describes the intent of each building type.
 2. Locations. List of subareas where specified building types are permissible.
 3. Maximum Building Height. The maximum building height for the building type.
 4. Maximum Façade Width. The maximum dimension along a frontage of any single façade.
 5. Maximum Upper-Level Frontage Occupancy. The maximum linear dimension of building face above a specified height that may be set along the building frontage.
 6. Maximum Allowed Footprint Per Story. The maximum percentage of the site's buildable area (after excluding required setbacks) that may be constructed per story.
 7. Primary Facades. The primary orientation of the building facades.
 8. Landscaping. Landscaping standards by building type.
 9. Open Space. Any open space standards applicable to the building type.
 10. Primary Frontage Types. The primary frontage types that may be used for each building type.
 11. Primary Entry Locations. The location of the building's front door.
 12. Parking. Parking orientation standards by building type.
- B. Allowable Frontage Types. Defines which frontage types are permitted within each subarea. In some cases, the permitted frontage types may also be regulated according to the street on which they face and must also comply with the permitted frontage types of the selected building type. Allowable frontage types are provided in Section 908 (E) and the following describes the standards provided for each frontage type.
1. Frontage Intent Statement. This statement describes the building-to-street relationship that each frontage type is meant to achieve.
 2. Locations. List of subareas where specified frontage types are permissible.
 3. Entries. These standards address entries at the blockfronts, not those that are internal to the site.

4. Dimensions. Specific dimensions of features like massing, entry height, openings, and setbacks are delineated here.
 5. Paving and Landscaping. This standard addresses the area between the property line and building face.
 6. Furnishing Zone. This standard addresses furnishing within front setbacks.
 7. Additional Standards and Guidelines. These standards and guidelines provide additional direction in shaping the appropriate building-to-street relationship. They address glazing at the ground floor, frontages, and entries (see Figure 5).
- C. Maximum Building Height. Identifies the maximum height buildings in each subarea are not allowed to exceed.
- D. Maximum Average Building Height. Requires buildings to create a varied “street wall” by requiring portions of a development to have different heights to meet the average standard. In no case can any portion of the development exceed the Maximum Building Height standard. Figures 1 and 2 identify how average building height shall be calculated to determine compliance with the standard.
- E. Minimum Frontage Occupancy. Is the minimum percentage of a blockfront at which a building frontage is set either at or within the zone established by the minimum and maximum Setback Lines, as shown in the Setback Regulating Plan, Map 3. The Minimum Frontage Occupancy is measured as a linear distance parallel to the property line. The remaining frontage length may be set behind the Maximum Setback Line or may be left unoccupied.



SOURCE: ESA, 2016; City of Oceanside, 2016; NARP, 2014

City of Oceanside Coast Highway Corridor Stud. 130217

Map 2
Coast Highway Incentive District
 Sub-Area Plan



SOURCE: ESA, 2018; City of Oceanside 2018; NAIP 2014

City of Oceanside Coast Highway Corridor Stud. 130217

Map 4
Coast Highway Incentive District
Residential Incentive Program Area

ATTACHMENT 5

ACTION ITEMS

The table of Action Items outlines specific steps to achieve the goals of the Coast Highway Vision plan. Every action item has a corresponding dollar sign - signifying the level of cost to the City - as well as a corresponding list of agencies or departments that will be responsible for overseeing the action.

Planwide Efforts				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
PW-1: Engage consultant to prepare a parking reform study and create a parking district plan to include: recommendations for TOD parking standards, shared parking, location and peak pricing for park-once garages and surface lots.	S	\$\$	City	TSG, RBF
PW-2: Engage consultant to prepare a traffic infrastructure and capacity study to enable urban circulation system for Coast Highway, including road diet, roundabouts, landscaped median, and mid-street pedestrian crossings.	S	\$\$	City	TSG, RBF
PW-3: Direct staff to prepare and implement a Transferable Development Rights (TDR) height policy.	S	\$	City	TSG, RBF
PW-4: Direct staff to review General Plan and zoning code for inconsistencies between Vision Plan and GP, revise code and General Plan to implement Vision Plan.	S	\$	City	TSG, RBF
PW-5: Direct staff to prepare and implement Development Incentives Policy, to include, among other things: green tape zone, expedited permitting, and 'zero fee' green design incentives.	M	\$	City	TSG, RBF
PW-6: Direct staff to work with developers on land assembly and site-specific incentives for catalytic projects.	S	\$	City	TSG, RBF
PW-7: Implement applicable Coastal Commission task force "Climate Change" policies.	S	\$	City	TSG, RBF

Coast Highway Street Improvements				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
CH-1: Pending traffic study, re-configure Coast Highway to include: reduced traffic lanes (road diet), roundabouts, landscaped median, parking lane, and widened sidewalk.	M	\$\$\$	City	PROP 1B, RBF, Other
CH-2: Apply urban design streetscaping treatment to include: shade trees, landscaped parkway, street furniture, trash/recycling, crosswalks, bulb-outs, pedestrian lighting, signage, and public art.	S-M	\$\$\$	City, PPO	PROP 1B, RBF, Other
CH-3: Improve and install alternative transit infrastructure to include: bike lane, bike parking, transit shelters, and way finding signage.	S-M	\$-\$\$\$	City	PROP 1B, RBF, Other
CH-4: Pending parking study, adopt and apply parking strategies for nodes, district, and neighborhood areas.	M	\$\$	City	TSG, RBF, GF

Action	Timeframe	Cost	Responsibility	Potential Funding Source
PW = Planwide	Short (S) = 0-3 years	\$ = - \$100,000	CCC = California Coastal Commission	TSG = TransNet Smart Growth Incentive Program (SANDAG)
CH = Coast Highway Corridor	Medium (M) = 3-5 years	\$\$ = \$100,000 - \$500,000	City = Appropriate City	TLP = TransMet Local Programs (SANDAG)
NC = North Coast Node	Long (L) = 5+ years	\$\$\$ = - \$500,000	Department(s)	PROP 1B = Proposition 1B State/ Local
OT = Oceanside Transit Center Node			PPO = Private Property Owner	RBF = Redevelopment Bond Funds (City)
SH = Seaside Neighborhood			NCTD = North County Transit District	ND = New Development
SS = Sprinter Station Node ATE = Arts, Technology & Environment District				FBF = Federal Bridge Funds
SO = South O' Village Node				GF = General Funds
				Other = Other Federal/ State Grants

ACTION ITEMS, CONTINUED

Las Ramblas North 'O' Area				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
NC-1: Plan and construct new North Coast Highway bridge over San Luis Rey River.	L	\$\$\$	City	RBF,ND,FBF
NC-2: Design and apply urban design and arts treatment to new Arts Bridge.	L	\$\$\$	City	FBF
NC-3: Realign Coast Highway, pull back (west) from Interstate 5.	S	\$	City	RBF,ND
NC-4: Plan and construct new pedestrian bridge over I-5 linking North Coast catalytic site with east neighborhood.	L	\$\$\$	City, Caltrans	TSG,PROP 1B
NC-5: Pending completion of traffic infrastructure and capacity study, construct roundabout gateway and landscaping at North Coast Highways north of Neptune Way.	L	\$\$\$	City	TLP,PROP 1B,RBF
NC-6: Require new development along North Coast Highway to plan and construct new streets as indicated on plan and as per the Design Guidelines.	S-L	\$\$\$	City	ND
NC-7: Revitalize San Luis Rey River area, including bike and walking trails.	M-L	\$\$\$	City	ND, Other
NC-8: Relocate Lift Station.	L	\$\$\$	City	RBF,ND
NC-9: Plan and construct pedestrian and bike bridges over and under railroad tracks.	L	\$\$\$	City, NCTD	TLP,PROP1B,ND,Other

Oceanside Transit Center Transit Oriented Development Area				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
OTC-1: Work with NCTD to further develop TOD plan for site consistent with Vision Plan and fund necessary infrastructure improvements.	S - M	\$	NCTD, City	TSG
OTC-2: Design and build new transit station and adjoining development.	M	\$\$\$	NCTD, City, PPO	TLP, PROP 1B, ND
OTC-3: Add a floor of parking to existing parking garage.	L	\$\$\$	City	TLP,RBF
OTC-4: Design and install solar panels on existing parking garage.	L	\$\$	City	Other

Seaside Neighborhood				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
SN-1: Amend zoning regulations to exclude rowhouses in Seaside Neighborhood.	S	\$	City	GF
SN-2: Pending results of parking reform study, apply parking strategy such as residential permit parking district to prevent spillover parking from Coast Highway businesses.	S-M	\$\$	City	GF

Action	Timeframe	Cost	Responsibility	Potential Funding Source
PW = Planwide CH = Coast Highway Corridor NC = North Coast Node OT = Oceanside Transit Center Node SN = Seaside Neighborhood SS = Sprinter Station Node ATE = Arts, Technology & Environment District SO = South O' Village Node	Short (S) = 0-3 years Medium (M) = 3-5 years Long (L) = 5+ years	\$ = \$100,000 \$\$ = \$100,000 - \$500,000 \$\$\$ = \$500,000	CCC = California Coastal Commission City = Appropriate City Department(s) PPO = Private Property Owner NCTD = North County Transit District	TSG = TransNet Smart Growth Incentive Program (SANDAG) TLP = TransNet Local Programs (SANDAG) PROP 1B = Proposition 1B State/ Local RBF = Redevelopment Bond Funds (City) ND = New Development FBF = Federal Bridge Funds GF = General Funds Other = Other Federal/ State Grants

ACTION ITEMS, CONTINUED

The table of Action Items below outlines specific steps to achieve the goals of the Coast Highway Vision plan. Every action item has a corresponding dollar sign - signifying the level of cost to the City - as well as a corresponding list of agencies or departments that will be responsible for overseeing the action.

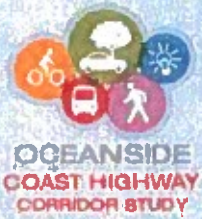
Sprinter Station Area				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
SS-1: Pending results of traffic study, re-stripe three to four block area as part of road-diet demonstration project.	S	\$	City	TSG, TLP, PROP 1B, RBF, ND
SS-2: Pending results of traffic study, install one traffic circle as part of road diet demo project.	S	\$	City	TSG, TLP, PROP 1B, RBF, ND
SS-3: Pending result of parking plan, implement "park-once" garage at transit station plaza.	M	\$\$\$	City	TSG, RBF, ND
SS-4: Work with developer to restore and revitalize Loma Alta Creek area.	M-L	\$\$\$	City, CCC	RBF, ND, Other
SS-5: Re-configure Cleveland Street to connect to Godfrey Street.	M-L	\$\$\$	City	TLP, PROP 1B

Arts, Technology & Environment District				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
ATE-1: As part of PW-5, develop package of incentives to attract infill development, renovation to existing structures, and adaptive re-use that appeals to creative industries, emerging green technologies, and environment.	S	\$\$	City	GF
ATE-2: Pending completion of parking study, implement parking strategy for the area such as shared parking district.	S - M	\$	City	GF
ATE-3: Introduce specialized lighting, signage, and public art program to distinguish area as unique district, as well as act as gateway.	S - M	\$\$-\$\$\$	City	GF
ATE-4: Adopt and apply Design Guidelines to direct new development.	S - M	\$-\$\$	City	GF

South "O" Village				
Action	Timeframe	Cost	Responsibility	Potential Funding Source
SOV-1: Pending result of parking study, adopt shared parking district to include: re-striping and minor curb work to accommodate parking along Freeman Street.	S	\$	City	ND
SOV-2: Provide artistic gateway signage into the South "O" Village at corners of Cassidy Street and Vista Way.	M	\$	City	ND

Action	Timeframe	Cost	Responsibility	Potential Funding Source
PW = Planwide CH = Coast Highway Corridor NC = North Coast Node OT = Oceanside Transit Center Node SN = Seaside Neighborhood SS = Sprinter Station Node SO = South O' Village Node	Short (S) = 0-3 years Medium (M) = 3-5 years Long (L) = 5+ years	\$ = < \$100,000 \$\$ = \$100,000 - \$500,000 \$\$\$ = > \$500,000	CCC = California Coastal Commission City = Appropriate City Department(s) PPO = Private Property Owner NCTD = North County Transit District	TSG = TransMet Smart Growth Incentive Program (SANDAG) TLP = TransMet Local Programs (SANDAG) PROP 1B = Proposition 1B State/ Local RBF = Redevelopment Bond Funds (City) ND = New Development FBF = Federal Bridge Funds GF = General Funds Other = Other Federal/ State Grants

COAST HIGHWAY INCENTIVE DISTRICT



As part of the implementation of the Coast Highway Vision and Strategic Plan, City staff proposes the establishment of an Incentive District along the Coast Highway corridor from Seagaze Drive to the City's southern boundary at Buena Vista Lagoon.

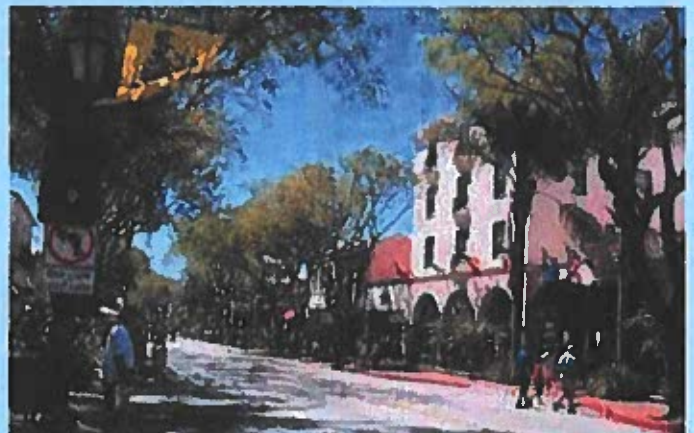
The Incentive District would provide an alternative to (not a replacement of) existing zoning standards, allowing additional building height and density, standalone residential use, and expedited permit processing in exchange for public benefits and adherence to form-based development standards. This fact sheet addresses the following questions regarding the proposed Incentive District:

- How do the land use and development standards of the proposed Incentive District compare with those in place under current zoning within the Coast Highway corridor?
- Which properties within the Coast Highway corridor would be eligible for the proposed incentives?
- How would the permit review and approval process be streamlined under the proposed Incentive District?

Current Zoning Standards

Virtually all property on Coast Highway south of Seagaze Drive is currently zoned General Commercial (C-2). In place since the late 1950s, the C-2 designation allows for building height up to **45 feet/four stories** and residential density up to **43 dwelling units per acre** in conjunction with mixed-use development. Additional building height, with no prescribed maximum, can be approved through a conditional use permit ("High-rise structures" per Zoning Ordinance Section 1130C). Thus, while this segment of Coast Highway is currently characterized

by relatively low-profile development and minimal residential use, existing zoning standards allow for significantly taller buildings and extensive high-density housing.





Sub-Districts and Corresponding Incentives

The proposed Incentive District would create **three sub-districts** within the Coast Highway corridor, each of which would provide a different range of standards and incentives beyond what is allowed today. Node sub-districts would promote higher density, transit-oriented development. Avenue sub-districts would provide for standalone residential use. Village Commercial sub-districts would essentially maintain the existing underlying C-2 zoning standards. All three would allow for streamlined permit review for projects conforming to form-based development standards.



Nodes

Properties in the vicinity of the Oceanside Transit Center and the Coast Highway Sprinter Station, as well as those on the inland side of Coast Highway between Whalay Street and Cassidy Street, would constitute nodal areas where building height up to **65 feet** and **residential densities up to 63 dwelling units per acre** would be allowed in exchange for one or more of the following **public benefits**: public open space, public parking, or commercial floor area exceeding a floor area ratio (FAR) of 25 percent. This is intended to be a catalyst for revitalization and to provide housing and jobs near public transit. Any additional height and/or density requested would be scaled in proportion to the extent of public benefit provided. Under the Incentive District, additional height and density above current zoning allowances would only be permitted in nodal areas.

Avenue Segments

Stretches of the Coast Hwy corridor between the nodal areas would be designated as "avenues," where standalone residential uses would be permitted. This is intended to encourage a varied skyline along the corridor and support commercial activity with additional consumers. Proposed standalone residential projects would be subject to more restrictive height and setback standards. For example, such projects would be limited to an average building height of **35 feet**, in contrast to the 45-foot allowance for standalone commercial or mixed-use projects under current C-2 zoning. Commercial uses could still be pursued in avenue segments, per current C-2 land use standards.

Commercial Villages

The Incentive District calls for two "commercial village" areas, one located along Wisconsin Avenue between Coast Highway and Pacific Street and another extending along Coast Highway south of Cassidy Street. This is intended to help revitalize these areas without adding density or height. These sub-districts would **not provide for any additional building height or residential density, nor would they allow for standalone residential**. Projects conforming to form-based development standards would be eligible for streamlined permit review.

Form-Based Development Standards and Streamlined Permit Review

Form-based development standards are a means of regulating development to achieve a specific urban form or design. Such standards foster predictable results and high-quality public areas by using physical form as the organizing principle. The form-based development standards proposed under the Incentive District would require pedestrian-oriented building design with articulated facades, variable height, landscaping, and a variety of architectural features intended to create an attractive and engaging streetscape.

Under current C-2 zoning standards, most proposed development within the Coast Highway corridor is subject to a public hearing with the Planning Commission. Under the Incentive District, projects conforming to the form-based development standards would be subject to an administrative permit review process, with approval authority delegated to the City Planner. Such projects would be subject to current public notification requirements, including notice of application and notice of pending administrative decision. Consistent with current zoning standards, administrative decisions would be appealable to the Planning Commission, and Planning Commission decisions would be appealable to the City Council.

For more information on the Coast Highway Incentive District, please contact:

Russ Cunningham, Principal Planner
760-435-3525
RCunningham@ci.oceanside.ca.us

Additional information regarding the Coast Highway Corridor Study can be found on the website:
https://www.ci.oceanside.ca.us/gov/dev/coast_highway_corridor_study/

ATTACHMENT 7

**PLANNING COMMISSION
RESOLUTION NO. 2019-P23**

**A RESOLUTION OF THE PLANNING COMMISSION
RECOMMENDING CITY COUNCIL CERTIFICATION OF THE
FINAL ENVIRONMENTAL IMPACT REPORT AND ADOPTION
OF THE MITIGATION, MONITORING, AND REPORTING
PROGRAM, FINDINGS OF FACT, AND STATEMENT OF
OVERRIDING CONSIDERATIONS FOR THE COAST
HIGHWAY CORRIDOR STUDY, INCLUDING SUNDRY
ROADWAY IMPROVEMENTS AND THE COAST HIGHWAY
INCENTIVE DISTRICT**

**APPLICATION NO: GPA16-00001, ZA19-00004, LCPA19-00005
APPLICANT: City of Oceanside
LOCATION: Citywide**

**THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA DOES
RESOLVE AS FOLLOWS:**

**WHEREAS, a Final Environmental Impact Report for the Coast Highway Corridor Study
was prepared and proper notification was given in accordance with the California Environmental
Quality Act; and**

**WHEREAS, the originally scheduled Planning Commission public hearing on the proposed
project was continued from May 20, 2019 to June 10, 2019; and**

**WHEREAS, the Planning Commission, after giving the required notice, did on the 10th day
of June 2019 conduct a duly advertised public hearing on the content of the Final Environment
Impact Report and the Mitigation Monitoring and Reporting Program; and**

**WHEREAS, studies and investigations made by the Commission and in its behalf reveal
the following facts:**

For the Environmental Impact Report:

- 1. The Final Environmental Impact Report was completed in compliance with the provisions
of the California Environmental Quality Act (CEQA).**
- 2. There are certain significant environmental effects detailed in the Final Environmental
Impact Report which have been avoided or mitigated to a less than significant level by
mitigation measures detailed in Exhibit "A" The Mitigation Monitoring and Reporting
Program (MMRP).**

1 3. The Final Environmental Impact Report, MMRP, Findings of Fact, and Statement of
2 Overriding Considerations were presented to the Planning Commission, and the Planning
3 Commission reviewed and considered the information contained in these documents prior
4 to making a decision on the project. The Final Environmental Impact Report, MMRP,
5 Findings of Fact, and Statement of Overriding Considerations have been determined to be
6 accurate and adequate documents which reflect the independent judgement of the City.

7 NOW, THEREFORE, BE IT RESOLVED as follows:

- 8 1. The Planning Commission does hereby recommend City Council certification of the Final
9 Environmental Impact Report and certification of the MMRP, and adoption of the Findings
10 of Fact and Statement of Overriding Considerations for the Coast Highway Corridor Study.
- 11 2. The Planning Commission does hereby further recommend City Council adoption of
12 Alternative 3, as described in the Final Environmental Impact Report, which extends Coast
13 Highway complete street improvements and the Coast Highway Incentive District
14 southerly to Morse Street.

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1 3. Notice is **HEREBY GIVEN** that the time within which judicial review must be sought on
2 this decision is governed by the provisions of the California Environmental Quality Act.
3 **PASSED AND ADOPTED** Resolution No. 2019-P23 on June 10, 2019 by the following
4 vote, to wit:

5 **AYES:** KraheJ, Balma, Busk, Rosales and Custer

6 **NAYS:** None

7 **ABSENT:** Morrissey

8 **ABSTAIN:** Goodkind

9
10 

11 _____
12 Kyle Krahel, Chairperson
13 Oceanside Planning Commission

13 **ATTEST:**

14 
15 _____
16 Jeffrey Hunt, Secretary

17
18 I, JEFFREY HUNT, Secretary of the Oceanside Planning Commission, hereby certify that this is
19 a true and correct copy of Resolution No. 2019-P23.

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21 Dated: June 10, 2019
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1 PLANNING COMMISSION
2 RESOLUTION NO. 2019-P21

3 A RESOLUTION OF THE PLANNING COMMISSION OF
4 THE CITY OF OCEANSIDE RECOMMENDING CITY
5 COUNCIL ADOPTION OF AMENDMENTS TO THE
6 LAND USE ELEMENT AND CIRCULATION ELEMENT

6 APPLICATION NO: GPA16-00001
7 APPLICANT: City of Oceanside
8 LOCATION: Citywide

9 THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA DOES
10 RESOLVE AS FOLLOWS:

11 WHEREAS, there was filed with this Commission a verified petition on the forms
12 prescribed by the Commission requesting a General Plan Amendment under the provisions of
13 Article 45 of the Zoning Ordinance of the City of Oceanside for the following:

14 amendment of the Land Use Element and Circulation Element to incorporate provisions
15 that enable the implementation of the Coast Highway Vision and Strategic Plan, including
16 the establishment of a Special Management Area within the Coast Highway corridor and
17 the re-designation of Coast Highway from a four-lane collector to a two-lane collector with
18 raised medians, roundabouts, and bicycle lanes; and

19 WHEREAS, Coast Highway is a key mixed-use corridor that provides access to the City's
20 coastal resources, downtown district, and adjacent neighborhoods; and

21 WHEREAS, there are opportunities to improve pedestrian and bicycle facilities within the
22 Coast Highway corridor; and

23 WHEREAS, the Coast Highway corridor includes vacant and underutilized property that
24 can support additional commercial activity, housing, recreational uses, and gathering spaces; and

25 WHEREAS, the City's Circulation Element includes policies and protocols that promote
26 the implementation of "complete streets" throughout Oceanside; and

27 WHEREAS, on April 15, 2009, the City Council adopted the Coast Highway Vision and
28 Strategic Plan, which calls for the revitalization of the Coast Highway corridor through right-of-
29 way improvements and updated zoning standards; and

1 WHEREAS, on August 21, 2013, the City Council approved a professional services
2 agreement with IBI and ESA to prepare a Programmatic Environmental Impact Report for
3 proposed roadway improvements and updated zoning standards intended to implement the Coast
4 Highway Vision and Strategic Plan; and

5 WHEREAS, on April 13, 2016, the City Council directed staff to analyze 1) the
6 reconfiguration of Coast Highway from a four-lane collector to a two-lane collector with new
7 raised medians, roundabouts, and bicycle lanes, and 2) the implementation of zoning incentives to
8 promote new mixed-use and residential development within the Coast Highway corridor; and

9 WHEREAS, a Programmatic Environmental Impact Report analyzing the above-noted
10 actions was circulated for public review between July 13, 2017 and August 28, 2017; and

11 WHEREAS, a revised Programmatic Environmental Impact Report with additional
12 analysis of potential traffic, noise, and air quality/GHG impacts was circulated for public review
13 between November 14, 2018 and January 14, 2019; and

14 WHEREAS, the proposed project requires amendment of the City's Land Use Element and
15 Circulation Element; and

16 WHEREAS, the originally scheduled Planning Commission public hearing on the
17 proposed project was continued from May 20, 2019 to June 10, 2019; and

18 WHEREAS, the Planning Commission, after giving the required notice, did on the 10th
19 day of June, 2019 conduct a duly advertised public hearing as prescribed by law to consider the
20 proposed project; and

21 WHEREAS, pursuant to the California Environmental Act of 1970, the Planning
22 Commission finds that a Environmental Impact Report has been prepared in conformance with the
23 California Environmental Quality Act (CEQA); and

24 WHEREAS, the Planning Commission reviewed and considered information in this
25 document prior to making a decision on the project; and

26 WHEREAS, the documents and other material constituting the record of proceedings
27 upon which the decision is based will be maintained by the City of Oceanside Planning
28 Division, 300 North Coast Highway, Oceanside, California 92054; and

29

1 WHEREAS, studies and investigations made by this Commission and in its behalf reveal
2 the following facts:

3 **FINDINGS:**

4 **For the General Plan Amendment:**

- 5 1. The proposed amendments to the Circulation Element would allow for the installation of
6 enhanced traffic safety features on Coast Highway, making the corridor safer for motorists,
7 pedestrians, and bicyclists.
- 8 2. The proposed amendments to the Circulation Element would facilitate visual enhancement
9 of the Coast Highway corridor, providing for upgraded paving, additional landscaping,
10 wayfinding elements, historical markers, and other aesthetic improvements.
- 11 3. The proposed amendments to the Land Use Element would provide for updated zoning
12 standards that incentivize the revitalization of the Coast Highway corridor as promulgated
13 by the Coast Highway Vision and Strategic Plan.
- 14 4. The proposed amendments to the Land Use Element would facilitate the expansion of the
15 City's housing stock, thereby contributing to the availability of housing in the San Diego
16 region.
- 17 5. The proposed amendments to the Land Use Element would promote synergies between
18 commercial and residential land uses and thereby contribute to the expansion of
19 commercial activity within the Coast Highway corridor.
- 20 6. Together, the proposed amendments to the Circulation Element and Land Use Element
21 reflect a coordinated planning effort to align mobility improvements with appropriate land
22 use and development patterns.

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1 NOW, THEREFORE, BE IT RESOLVED that the Planning Commission does hereby
2 recommend City Council adoption of the proposed amendments to the Circulation Element and
3 Land Use Element (GPA16-00001), as modified to be consistent with Alternative 3 of the Final
4 Environmental Impact Report, with authority granted to the City Manager to approve subsequent
5 technical revisions of the proposed roadway improvements and zoning implementation measures.

6 PASSED AND ADOPTED Resolution No. 2019-P21 on June 10, 2019 by the
7 following vote, to wit:

8 AYES: Krahel, Balma, Busk, Rosales and Custer

9 NAYS: None

10 ABSENT: Morrissey

11 ABSTAIN: Goodkind

12
13 
14 _____
15 Kyle Krahel, Chair
16 Oceanside Planning Commission

17 ATTEST:

18 
19 _____
20 Jeff Hunt, Secretary

21 I, JEFF HUNT, Secretary of the Oceanside Planning Commission, hereby certify that this is a
22 true and correct copy of Resolution No. 2019-P21.

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24 Dated: June 10, 2019
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**PLANNING COMMISSION
RESOLUTION NO. 2019-P22**

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA RECOMMENDING THE CITY COUNCIL ADOPTION OF ZONING TEXT AMENDMENT (ZA19-0004) TO INCORPORATE THE COAST HIGHWAY INCENTIVE DISTRICT INTO THE CITY'S COMPREHENSIVE ZONING ORDINANCE AS ARTICLE 9 AND ESTABLISH THE AMENDED TEXT AS PART OF THE CITY'S LOCAL COASTAL PROGRAM

APPLICATION NO: ZA19-00004, LCPA19-00005

APPLICANT: City of Oceanside

LOCATION: Citywide

THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA DOES RESOLVE AS FOLLOWS:

WHEREAS, the Comprehensive Zoning Ordinance sets forth the purpose and intent, and zoning regulations for properties within the City of Oceanside; and

WHEREAS, certain Articles of said ordinance contain zoning regulations applicable citywide and others are limited to inland or coastal zones; and

WHEREAS, Comprehensive Zoning Ordinance Articles applicable citywide and within the coastal zone have been certified by the California Coastal Commission and constitute part of the Implementation Plan of the Local Coastal Program; and

WHEREAS, amendments to citywide and coastal zone Article regulations require processing of Local Coastal Program amendments and certification by the California Coastal Commission; and

WHEREAS, the originally scheduled Planning Commission public hearing on the proposed project was continued from May 20, 2019 to June 10, 2019; and

WHEREAS, the Planning Commission did, on the 10th day of June, 2019, conduct a duly advertised public hearing as prescribed by law to consider staff recommendations for a zoning text amendment to incorporate the Coast Highway Incentive District into the Comprehensive Zoning Ordinance as Article 9 and include said amendment as part of the Local Coastal Program Implementation Plan; and

WHEREAS, in accordance with the provisions of the California Environmental Quality Act (CEQA), an Environmental Impact Report was prepared for the proposed project; and

1 WHEREAS, studies and investigations made by this Commission and on its behalf reveal the
2 following facts pertaining to the proposed Zoning Text Amendment and Local Coastal Program
3 Amendment:

- 4 1. The Zoning Text Amendment conforms to the General Plan goal and objective of ensuring
5 the enhancement of community and neighborhood values through land use planning by setting
6 forth standards and incentives designed to facilitate revitalization of the Coast Highway
7 corridor.
- 8 2. The Zoning Text Amendment is consistent with the Zoning Ordinance purpose of fostering
9 harmonious and workable relationships among land uses, as the Incentive District promotes
10 synergies between commercial and residential uses as well as visually-appealing, pedestrian-
11 friendly environments that support commerce, recreation, and social interaction.
- 12 3. Pursuant to Public Resources Code §30510(a), the Planning Commission hereby certifies that
13 the Local Coastal Program Amendment (LCPA19-00005) is intended to be carried out in
14 conformance with the Coastal Act of 1976.
- 15 4. The Zoning Text Amendment conforms to the Local Coastal Program, in that it does not
16 adversely impact public coastal access, water or marine resources, sensitive habitat, visual
17 resources, visitor serving uses, or public facilities. The Coast Highway Incentive District
18 promotes pedestrian and bicycle access to the coastline, redevelopment and infill within
19 already urbanized areas, expansion of visitor-serving commercial uses, and visual
20 enhancement of the Coast Highway corridor. New development facilitated by the Incentive
21 District would be subject to development standards and guidelines designed to avoid adverse
22 impacts on existing coastal view corridors
- 23 5. Pursuant to Coastal Commission Local Coastal Program Regulations §13551(b), this
24 amendment shall take effect upon Coastal Commission approval, following City Council
25 approval and adoption.

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1 NOW, THEREFORE, BE IT RESOLVED that the Planning Commission does hereby
2 recommend that the City Council approve Zone Amendment (ZA19-00004) and Local Coastal
3 Program Amendment (LCPA19-00005), incorporating the Coast Highway Incentive District into
4 the City's Comprehensive Zoning Ordinance as Article 9 and establishing the amended text as part
5 of the Local Coastal Program (Exhibit - A), with the following modifications:

- 6 I. In Table 2 (Page 11) of Exhibit A, indicate that standalone residential development in
7 Avenue sub-districts be limited to a maximum average height of 35 feet.
- 8 II. In Table 3 (Page 13) of Exhibit A, indicate that auto sales operations are prohibited in Node
9 and Avenue sub-districts.
- 10 III. On Maps 1-4 of Exhibit A, show the Coast Highway Incentive District extending southerly
11 to Morse Street, consistent with Alternative 3 of the Final Environmental Impact Report.

12 PASSED AND ADOPTED Resolution No. 2019-P22 on the 10th day of June, 2019 by the
13 following vote, to wit:

14
15 AYES: Krahel, Balma, Busk, Rosales and Custer
16 NAYS: None
17 ABSENT: Morrissey
18 ABSTAIN: Goodkind

19
20 
21 _____
22 Kyle Krahel, Chairperson
23 Oceanside Planning Commission

24 ATTEST:

25 
26 _____
27 Jeff Hunt, Secretary

28 I, JEFFREY HUNT, Secretary of the Oceanside Planning Commission, hereby certify that this is
29 a true and correct copy of Resolution No. 2019-P22.

30 Applicant accepts and agrees with all conditions of approval and acknowledges impact fees may
31 be required as stated herein:

32 Date: June 10, 2019

Coast Highway Corridor Study



BANK OF ITALY AND NEWTON BLDGS.
OCEANSIDE CALIF.

August 14, 2019

City Council

Synopsis

- *Implement Alternative 3 which includes complete street improvements from Harbor Drive to Morse Street and the Incentive District from Seagaze Drive to Morse Street*
- *Adopt a resolution certifying the Final Environmental Impact Report (FEIR) and adopting the associated Mitigation, Monitoring, and Reporting Program (MMRP), Findings of Fact, and Statement of Overriding Considerations.*
- *Adopt a resolution adopting General Plan Amendment (GPA16-00001) to amend the Land Use Element and the Circulation Element.*
- *Introduce an ordinance adopting Zone Amendment (ZA19-00004).*
- *Adopt a resolution approving Local Coastal Plan Amendment (LCPA 19-00005).*

Project Background/Goals

- Builds on Coast Highway Vision & Strategic Plan
- Study potential transformation of Coast Highway into a Complete Street
- Study of development incentive policies and zoning
- Complete environmental review and preliminary design of preferred street concept

Coast Highway Vision and Strategic Plan





- Coast Highway Vision and Strategic Plan (CHVSP) adopted April 2009
- CHVSP Table of Action Items basis for Coast Highway Corridor Study scope
 - CHVSP based on a set of Livable Communities and Smart Growth principles
 - CHVSP recommends establishing activity centers (nodes) connected by landscaped avenue segments
- Coast Highway Corridor Study Steering Committee

Coast Highway Corridor Study: Goals


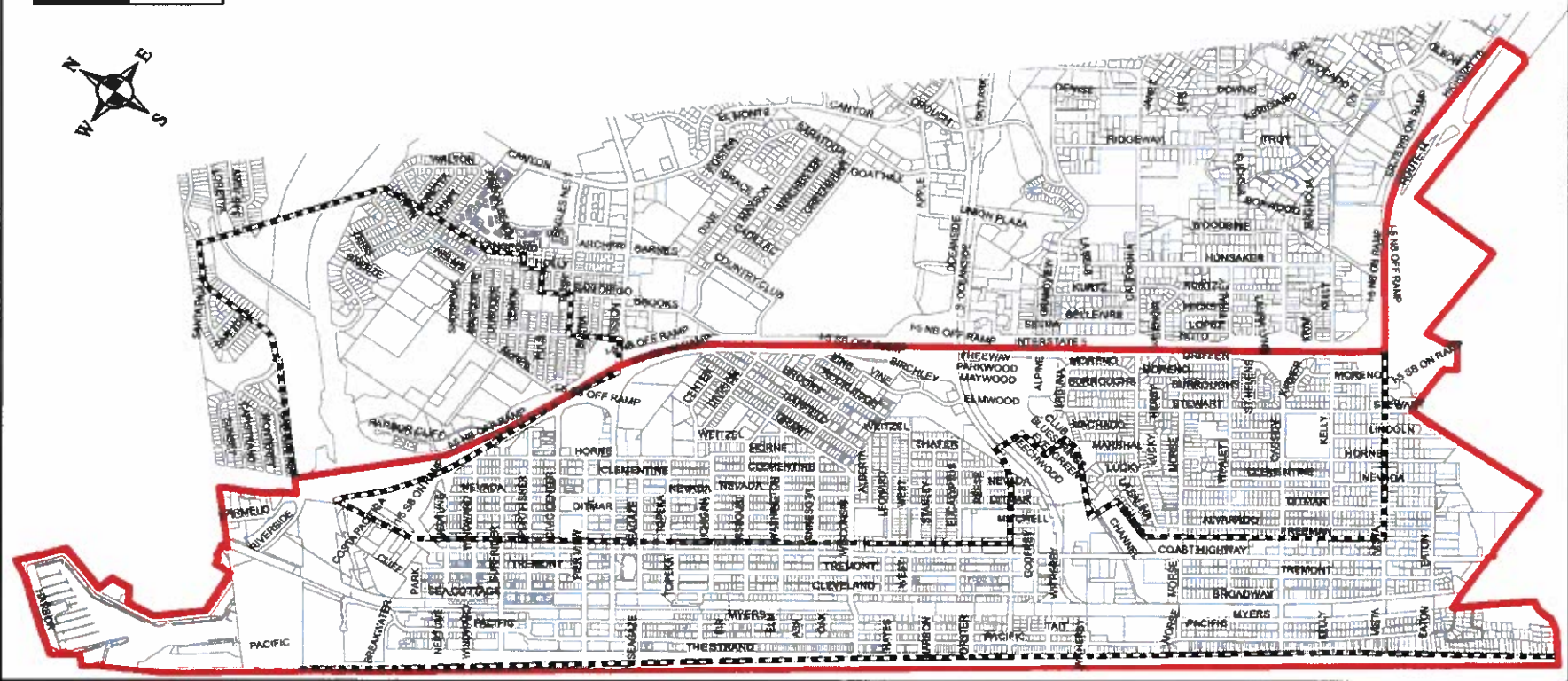
- Goal 1: Transform Coast Highway into a “Complete Street” that accommodates all roadway users (pedestrians, bicyclists, automobiles, and transit).
- Goal 2: Improve safety for all roadway users.
- Goal 3: Facilitate implementation of the Coast Highway Vision and Strategic Plan.

Coast Highway Corridor Study Area

Legend

-  Study Area Boundary
-  Parcel Lines
-  Centerline
-  Coastal Zone areas

1,300 650 0 1,300 Feet

Transportation: Project Alternatives

- Preferred Project – Complete Streets improvements with roundabouts from north City limit to south City limit. Incentive District from Seagaze Drive to south City limit.
- No Project – Keep Coast Highway as it currently exists.
- Alternative 1 – Complete Streets improvements with roundabouts from north City limit to Oceanside Blvd. Incentive District from Seagaze Drive to south City limit.
- Alternative 2 – Complete Streets improvements with roundabouts from north City limit to Morse Street. Incentive District from north City limit to south City limit.
- Alternative 3 – **Complete Streets improvements with roundabouts from north City limit to Morse Street. Incentive District from Seagaze Drive to Morse Street.**
- Alternative 4 – Complete Streets improvements with roundabouts from north City limits to south City limits. The Incentive District would not be adopted.

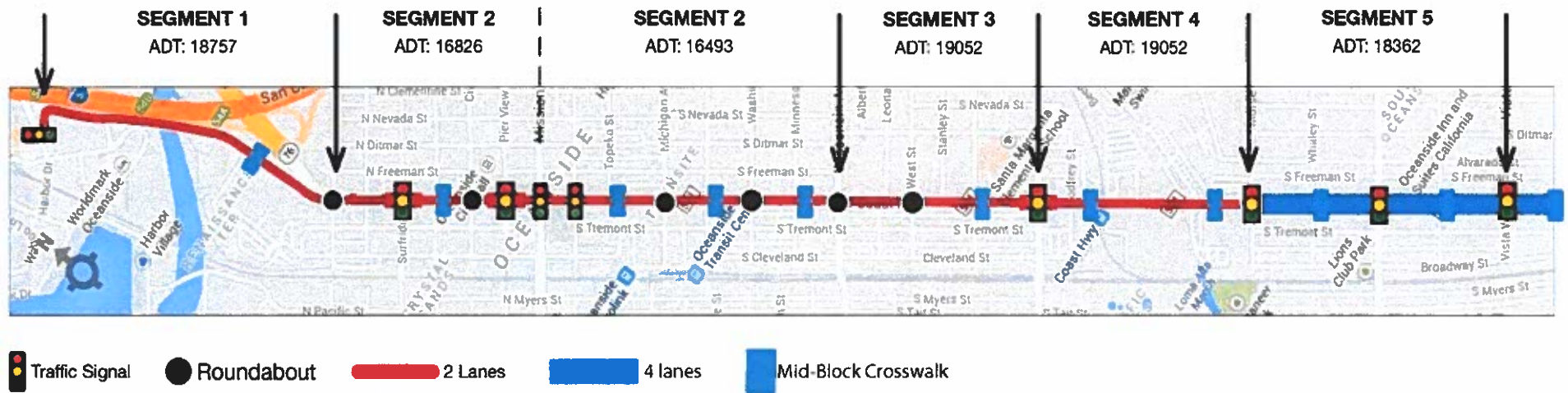
Alternative 3: Study Intersections

- 56 study intersections
- 7 existing intersections studied with roundabouts
- 6 intersections recommended for roundabouts with Alternative 3
- 11 mid-block crosswalk locations identified
- Adds approximately 17 new on-street parking stalls to existing on-street parking supply

Alternative 3

Alternative 3

Complete Streets Improvements from Harbor Drive to Morse Street

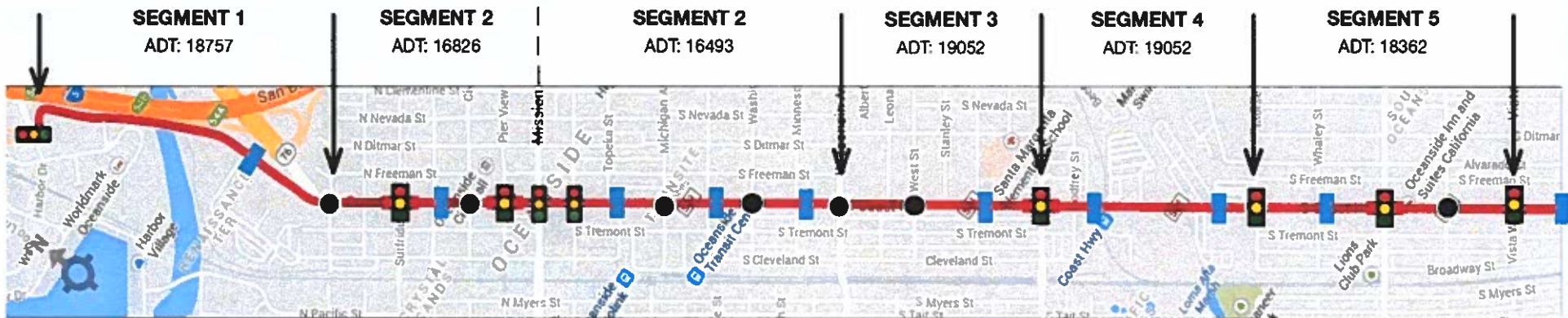


Key Attributes: Converts Coast Highway from 4 lanes to 2 lanes between Harbor Drive and Morse Street.
Roundabouts to optimize traffic flow at intersections currently controlled by signals.
Creates space in public right-of-way for bicycle facilities, improved sidewalks, additional landscaping

Preferred Project

Preferred Project

Complete Streets Improvements from Harbor Drive to South City Limits

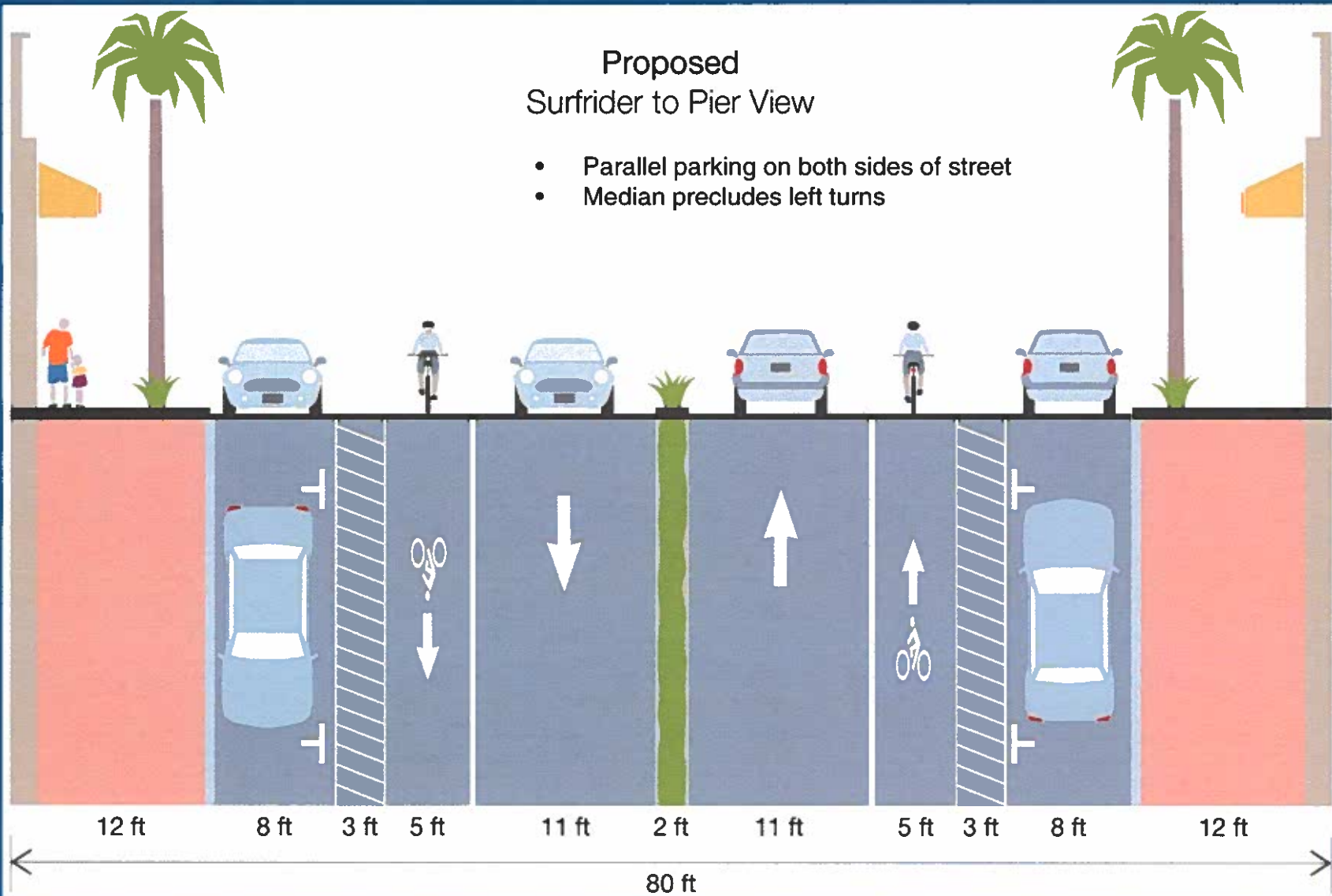


Key Attributes: Converts Coast Highway from 4 lanes to 2 lanes between Harbor Drive and South City Limits.
Roundabouts to optimize traffic flow at intersections currently controlled by signals.
Creates space in public right-of-way for bicycle facilities, improved sidewalks, additional landscaping

Segment 2: SR-76 to Wisconsin

Proposed Surfrider to Pier View

- Parallel parking on both sides of street
- Median precludes left turns



Oceanside Boulevard to Morse Street “The Dip”

Key Elements per Alternative 3:

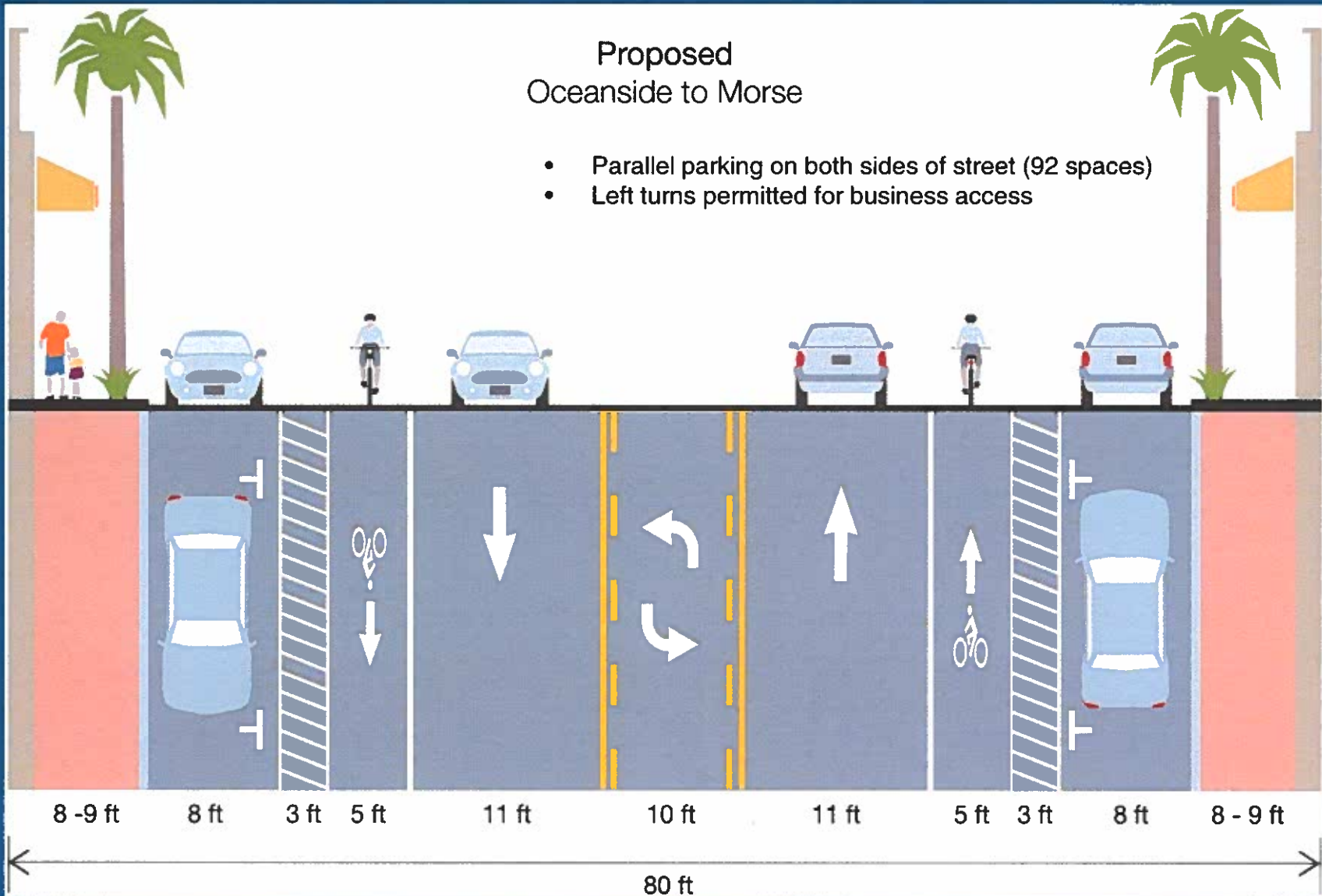
- High potential for redevelopment due to large parcel sizes and proximity to Sprinter Station
- Addition of on-street parking (92 spaces)
- Loma Alta Creek connection to Buccaneer Beach
- Preservation of left-turning access to RV parks
- Includes two mid-block crossings
- No cut-through traffic potential to parallel streets

Staff recommends maintaining Incentive District and 2-lane roadway design.

Segment 4: O'side Blvd. to Morse St. ("Dip")

Proposed Oceanside to Morse

- Parallel parking on both sides of street (92 spaces)
- Left turns permitted for business access



Visual Simulation: Coast Highway at Minnesota Avenue (Today)



Visual Simulation: Coast Highway at Minnesota Avenue (After)



Visual Simulation: Roundabout at Intersection (Today)



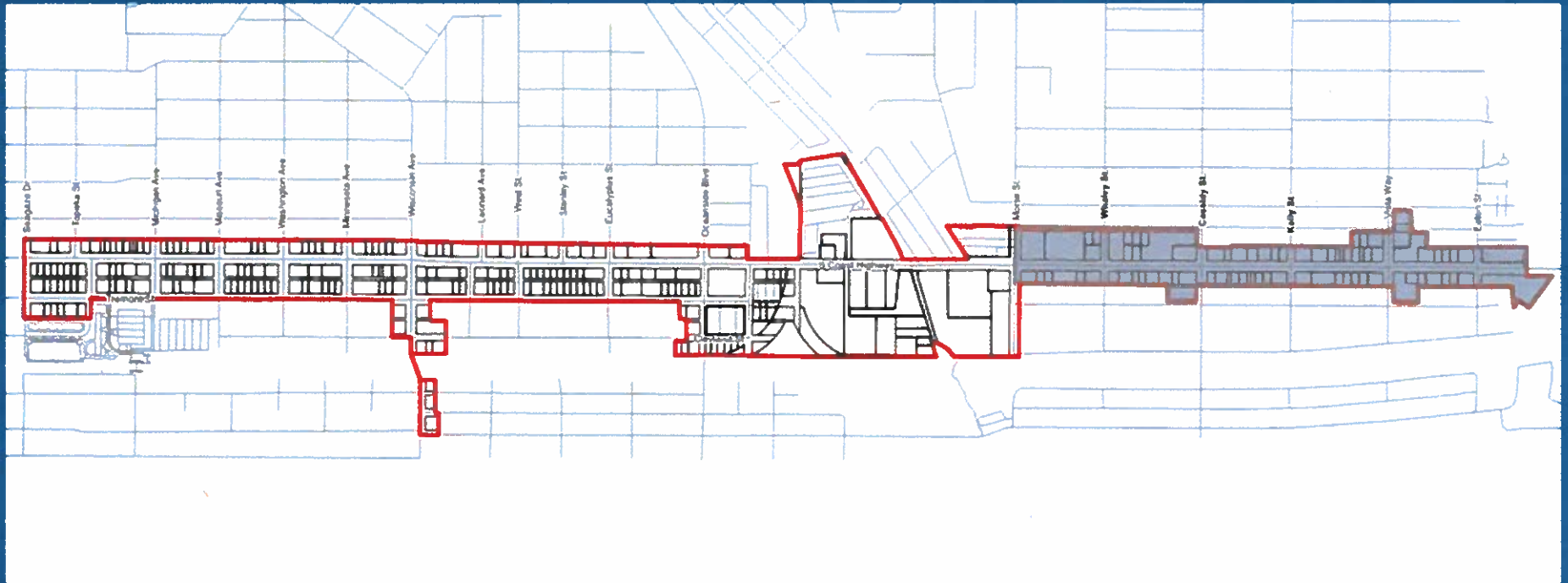
Visual Simulation: Roundabout at Intersection (After)



Incentive District (ID)



Incentive District Boundaries



Coast Hwy Vision and Strategic Plan Objectives

- **Economic enhancement and revitalization**
 - New housing that expands the market for commercial goods/services
 - Synergies between commercial uses (particularly in nodal areas)
 - Sense of place/destination
- **Smart growth**
 - More efficient land use
 - Enhanced walkability and transit access
- **Updated parking standards & programs**
 - Efficient use of parking resources
 - Programs responsive to existing conditions and emerging trends
- **High-quality site & architectural design**
 - Pedestrian orientation (outdoor room)
 - Horizontal and vertical articulation, transparency,
 - Focus on gateways, key intersections, and public spaces
- **Regulatory flexibility**

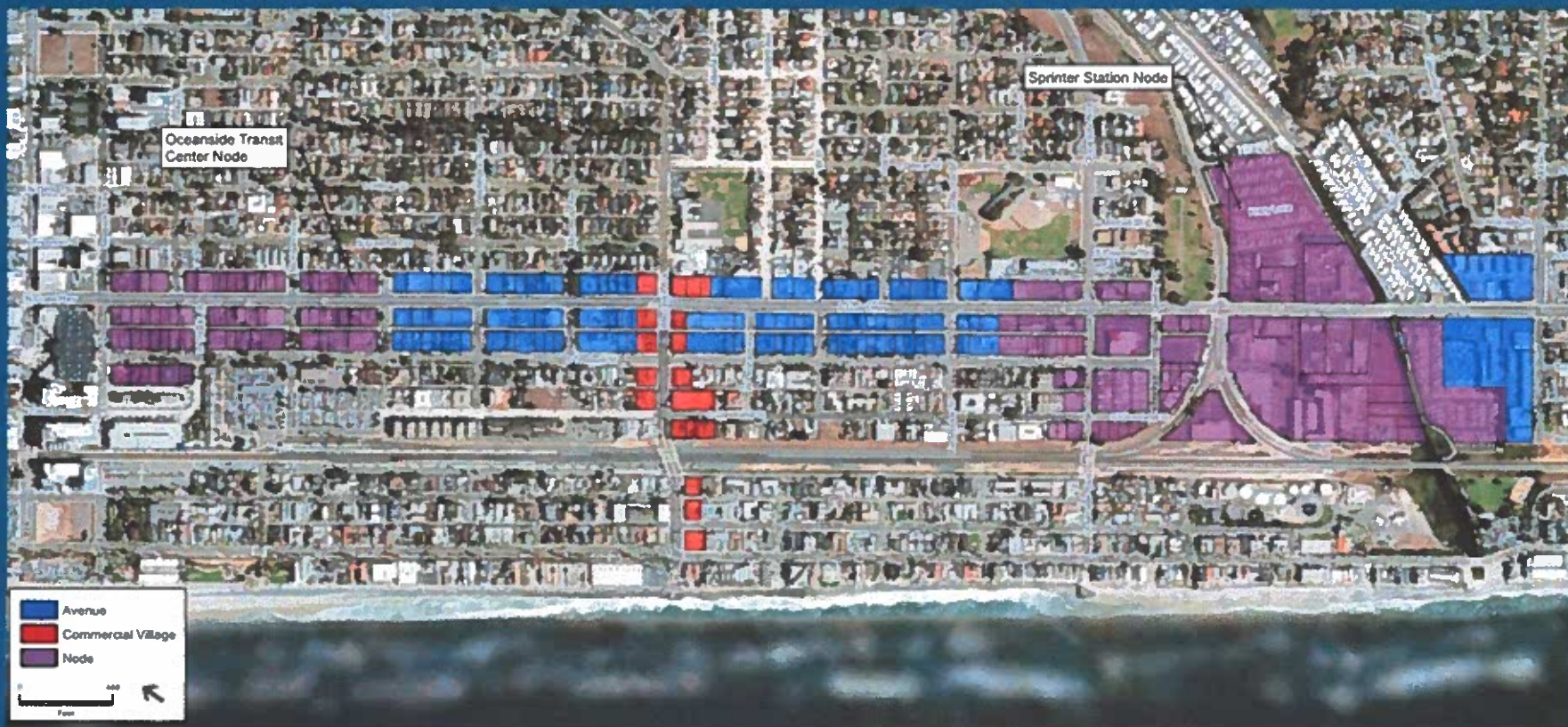
Market Analyses

- Robert Charles Lesser & Co. (Vision Plan)
 - More households and visitors needed to support additional commercial development
 - Significant demand for housing (both for-sale and rental)
- Keyser Marston Associates (Corridor Study)
 - Demand for up to 3,600 dwelling units through 2030
 - New housing can spur additional demand for commercial goods and services
 - Land aggregation and parking requirements present the most significant challenges to redevelopment
 - Transfer of Development Rights (TDM) not economically viable

ID Sub-Districts

- Nodes
 - OTC, Coast Hwy Sprinter Station, Whaley/Cassidy (removed in Alt. 3)
 - Higher intensity of land use, greater scale of development
 - Density and height incentives in exchange for public benefits
- Avenues
 - Segments between nodes providing visual relief
 - Allowance for standalone residential, subject to enhanced development standards
- Commercial Villages
 - Wisconsin Avenue; Segment s/o Cassidy removed under Alt. 3
 - No additional building height or density
 - No standalone residential

ID Sub-Districts



Incentives

- Nodal Areas
 - Additional density and building height in exchange for public benefits
 - Public parking
 - Public open space
 - Additional commercial floor area
 - In-lieu fee option
- Avenue Segments
 - Allowance for standalone residential (subject to more restrictive development standards)
- All Sub-Districts
 - Streamlined project review (form-based standards, PEIR)
 - Flexible parking provisions
 - Fewer uses subject to conditional use permits

Incentives (Nodes)

- **Additional Density: Up to 63 du/acre**
 - Maximum density under current standards is 43 du/acre
 - Addresses increasing market demand for smaller dwelling units
- **Additional Building Height: Up to 65 feet**
 - 55-foot average height maximum
 - Maximum height under current standards is 45 feet/four stories
 - Existing high-rise provision allows for additional building height through a CUP (no maximum established)

Development Review Process

- Administrative Review
 - Conformance to base density and height limits
 - Conformance to form-based development standards
 - Appealable to Planning Commission, City Council
- Planning Commission Review
 - Exceedance of base density and/or height limits
 - Contiguous lot area of 60K square feet or more
- Required findings/grounds for appeal
- Most projects should be able to tier off the PEIR

Parking

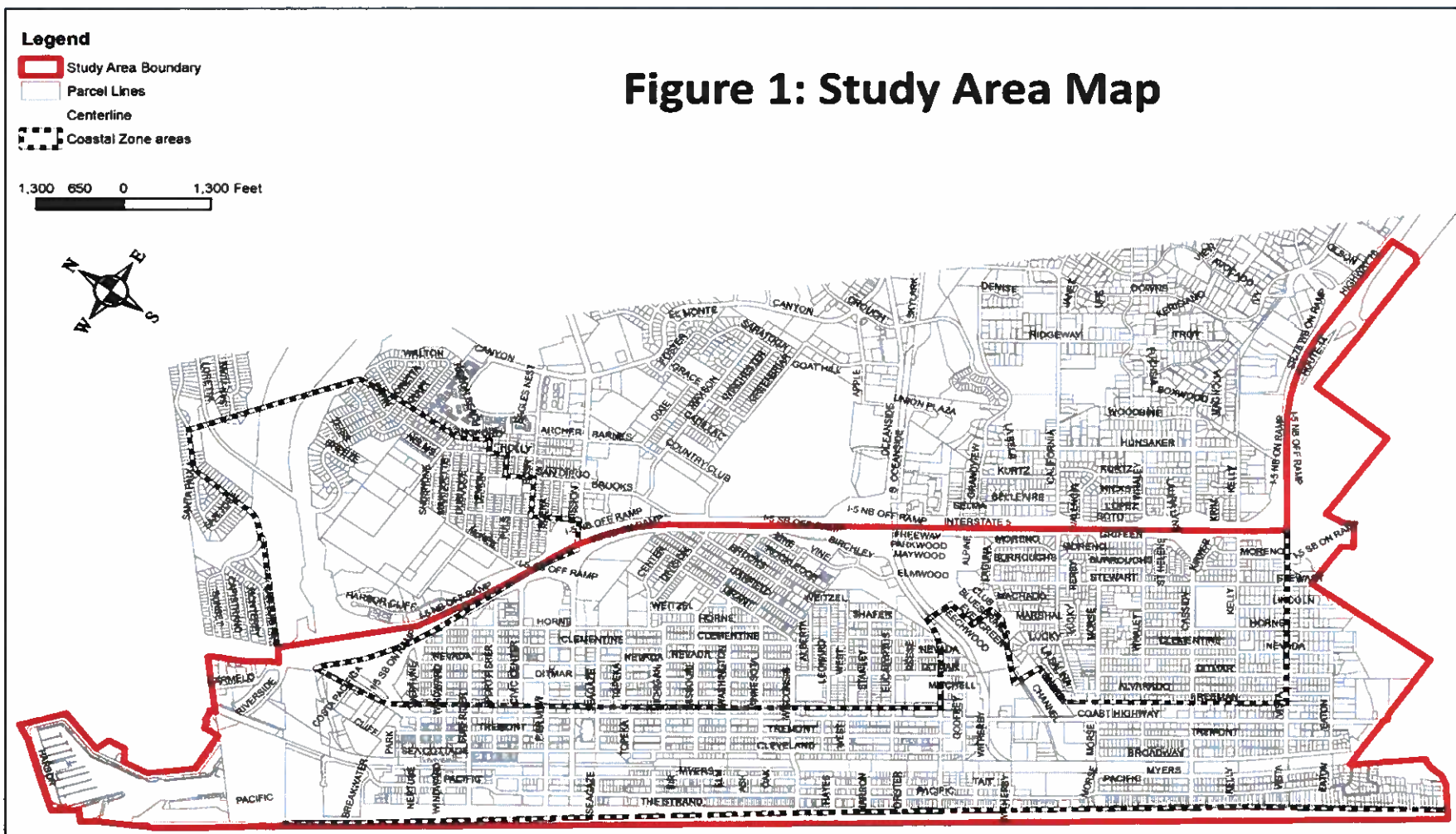
- Current parking requirements have impeded revitalization of the corridor.
- Adaptive reuse has been enabled by flexibility in the application of current standards.
- The Vision Plan outlines several strategies that promote efficient use of parking resources.

Parking

- A memorandum prepared by the City's traffic consultant (IBI) reiterates Vision Plan parking concepts and offers additional parking strategies.
- PDM strategies should be responsive to existing conditions and evolving trends.
 - Ride-hailing services
 - Micro-transit options
 - Ped/bike facilities

Traffic Study

Figure 1: Study Area Map



Highlights

- 56 study intersections
- 36 study roadway segments
- Analysis included:
 - Proposed roundabouts
 - Existing & Year 2035 conditions
 - Preferred project and Alternatives 1, 2, & 3
 - Coordination with Caltrans, NCTD, Coastal Commission

Traffic Analysis Summary

Scenario	Impacted Intersections	Mitigated Intersections	Unmitigated Intersections
Preferred Project	11	8	3
Alternative 1	8	5	3
Alternative 2	8	5	3
Alternative 3	6	3	3

Where Impacts Remain:

- Coast/Wisconsin – Roundabout is integral to the streetscape plan (All Alts)
- Ditmar/Wisconsin – Traffic signal not warranted, explore traffic calming (Alt 3)
- I-5 Southbound Ramps/Oceanside – Sprinter tracks & I-5 limit widening (All Alts)
- I-5 Southbound Ramps/Vista – Future interchange project would address impact (Preferred Project, Alt 1, Alt 2)

Multi-Modal Project Elements

- Vehicle Miles Traveled
 - No project results in 8% per capita VMT increase
 - Preferred Project & Alt 1/Alt 2 reduce per capita VMT from existing levels
 - Alt 3 maintains status quo for per capita VMT
- Bicycle connections
 - New striped Class II bike lanes and sharrows
 - Connections to 9 existing and planned bikeways
- Pedestrian crossings
 - 11 new mid-block crossing locations

Mobility Goals

- Goal 1: Transform Coast Highway into a Complete Street that accommodates all roadway users (pedestrians, bicyclists, transit, and autos)
- Goal 2: Improve safety for all roadway users

Goal 1:

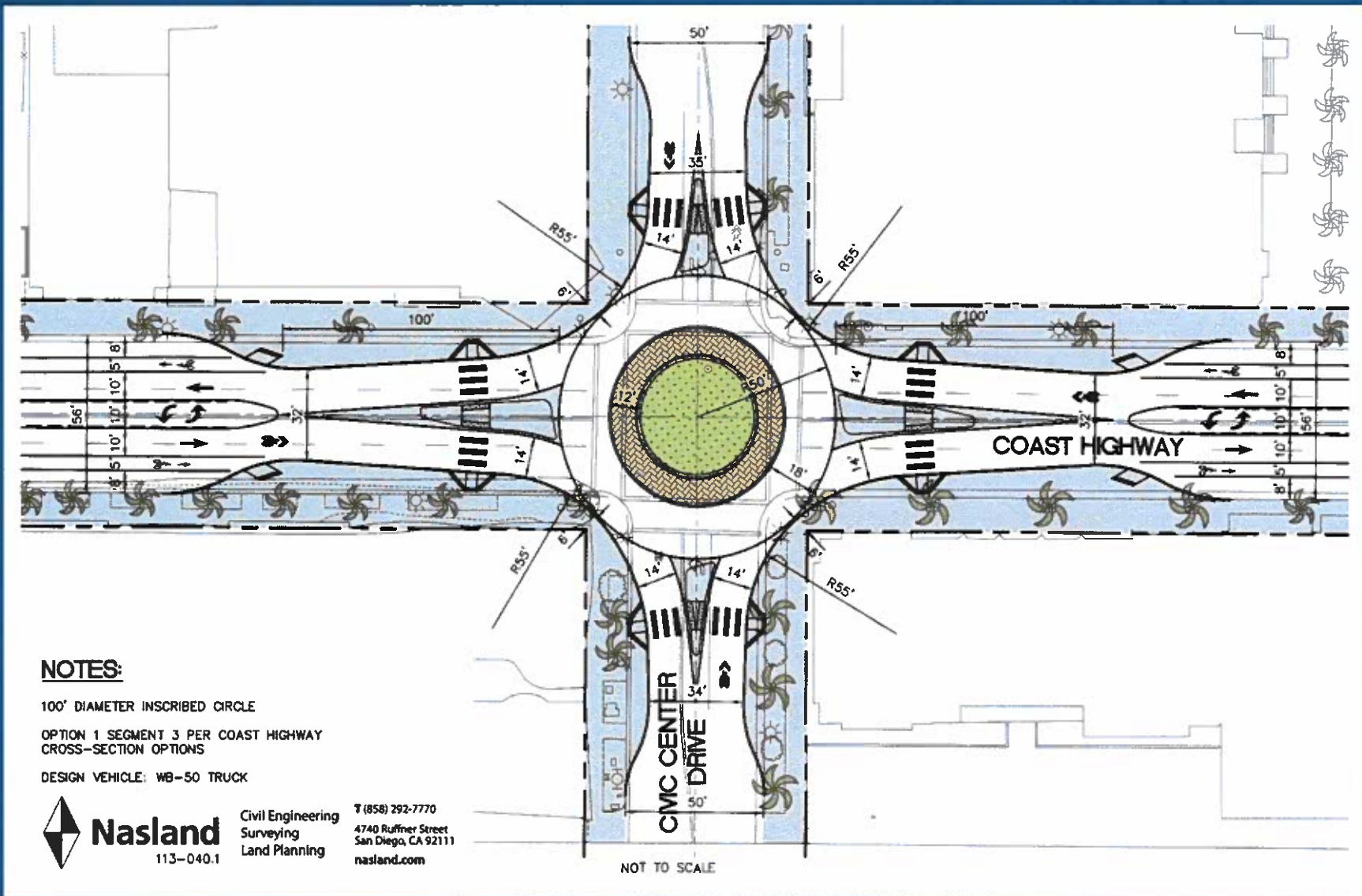
Create a Complete Street

- Roundabouts
- Bicycle lanes
- Mid-block pedestrian crosswalks
- Landscaping
- Increase on-street parking where possible

Complete Street Objectives

- Roundabouts:
 - Collision reductions – number, type, and severity
 - Delay reductions – vehicles, bikes, and peds
 - Speed reductions
- Lane Reductions:
 - Allow implementation of bicycle lanes
 - Reduce auto traffic speeds
 - Create opportunities for mid-block crosswalks
- Medians
 - Reduces left turn conflicts and improves traffic flow

Roundabout Concept Design



Mid-Block Crosswalk Example

Pedestrian Safety Enhancements
National City Blvd & W. 14th St near Public Library



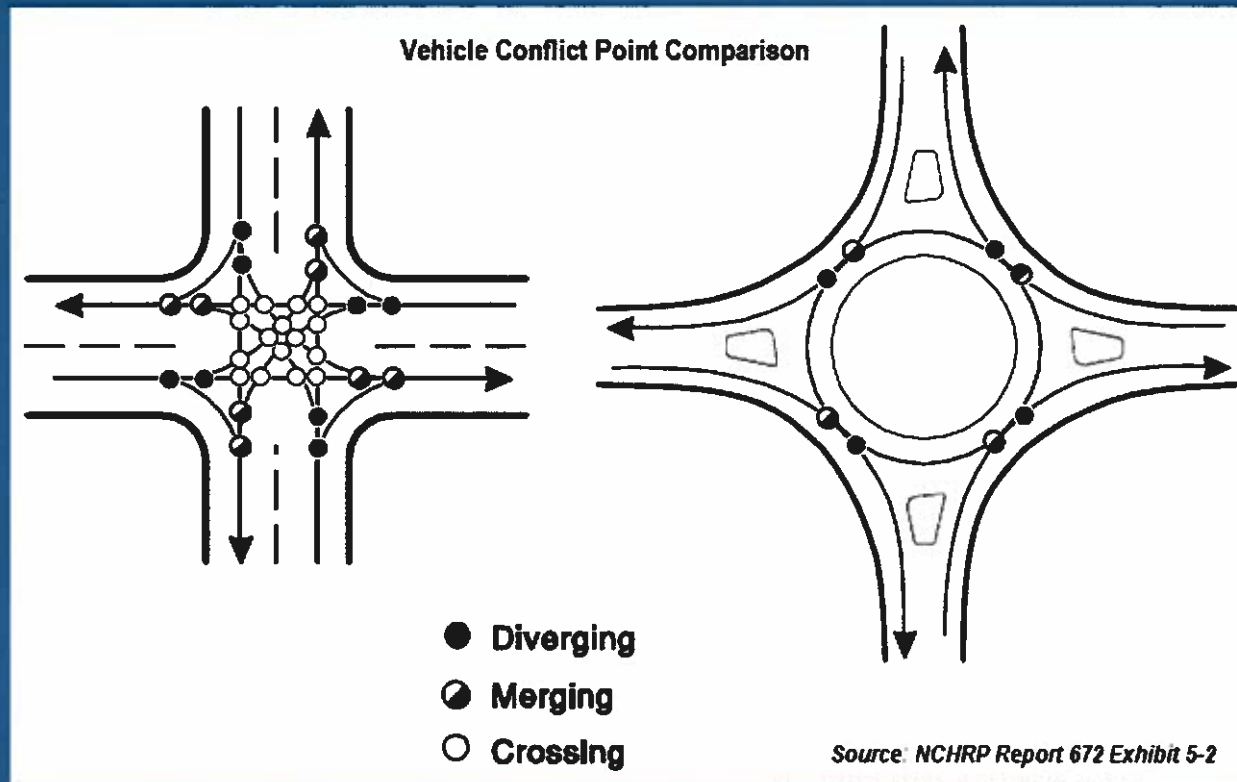
Goal 2:

Improve Safety for All Users

- Reduce traffic speeds
- Remove barriers/conflicts to walking and bicycling
- Promote safe travel across and along Coast Hwy

Roundabout Safety

- Reduce collisions with serious injuries & fatalities by 78-82% compared to stop signs and signals
- Reduce potential conflict points from 32 to 8



Roadway Safety

- Risk of pedestrian fatality is <10% at speeds below 30 mph
- Above 40 mph pedestrian fatality risk is >50%
- Pedestrian fatality risk ~4-5 times higher at 40 mph than 30 mph

Summary

- Complete Street improvements:
 - Reduce traffic speeds
 - Allow room for bicycle and pedestrian improvements
 - Minimize impact to traffic operations
 - Create safer environment for mobility
 - Reduce future vehicle miles traveled (VMT) per capita
- Traffic study identifies mitigations for all but 3 study locations

Potentially Significant Environmental Effects that can be Reduced to Less Than Significant with Mitigation

- Air Quality
- Biology
- Cultural
- Hazmat
- Noise
- Traffic

Significant Unavoidable Impacts

- Air Quality
- Greenhouse Gas
- Noise
- Traffic

Next Steps

- Coastal Commission Certification
- Identify funding source for final design
- Complete final design
 - Continued coordination with public safety
- Identify and pursue funding sources for construction (e.g., SANDAG and CALTRANS grants)
- Phased Project Construction

Recommendation

- *Implement Alternative 3 which includes complete street improvements from Harbor Drive to Morse Street and the Incentive District from Seagaze Drive to Morse Street*
- *Adopt a resolution certifying the Final Environmental Impact Report (FEIR) and adopting the associated Mitigation, Monitoring, and Reporting Program (MMRP), Findings of Fact, and Statement of Overriding Considerations.*
- *Adopt a resolution adopting General Plan Amendment (GPA16-00001) to amend the Land Use Element and the Circulation Element.*
- *Introduce an ordinance adopting Zone Amendment (ZA19-00004).*
- *Adopt a resolution approving Local Coastal Plan Amendment (LCPA 19-00005).*